

**SUBDIVISION,  
PLANNED UNIT DEVELOPMENT &  
PLANNING APPROVAL STAFF REPORT**

**Date: July 21, 2005**

<b><u>NAME</u></b>	Cottage Hill Baptist Church Subdivision
<b><u>LOCATION</u></b>	4255 Cottage Hill Road (Southeast corner of Cottage Hill Road and North Demetropolis Road, extending to the north side of Thigpen Drive South, and the southeast corner of North Demetropolis Road and Thigpen Drive South, extending to the north side of Troy Lane)
<b><u>CITY COUNCIL DISTRICT</u></b>	District 4
<b><u>PRESENT ZONING</u></b>	R-1, Single-Family Residential
<b><u>AREA OF PROPERTY</u></b>	14.52 acres $\pm$
<b><u>CONTEMPLATED USE</u></b>	Planning Approval to allow the expansion of an existing church; Planned Unit Development Approval to allow multiple buildings on a single building site, with shared access and parking between sites; Subdivision Approval to create two legal lots of record.
<b><u>TIME SCHEDULE FOR DEVELOPMENT</u></b>	Immediate
<b><u>ENGINEERING COMMENTS</u></b>	Must comply with all stormwater and flood control ordinances. Any work performed in the right of way will require a right of way permit.
<b><u>TRAFFIC ENGINEERING COMMENTS</u></b>	Driveway number, size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards.  The multiple pedestrian crosswalks on Thigpen Drive must be omitted. The current traffic calming measures and a single crosswalk may be retained.
<b><u>URBAN FORESTRY COMMENTS</u></b>	Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

Preservation status is to be given to the Two 48" Live Oak Trees and 54" Live Oak Tree located on the North East side of Lot 1. Any work on or under these trees are to be permitted and coordinated with Urban Forestry; removal to be permitted only in the case of disease or impending danger.

Full compliance with landscaping and tree requirements of the Zoning Ordinance to be coordinated with Urban Forestry.

## **FIRE DEPARTMENT**

### **COMMENTS**

All commercial projects shall comply with the 2003 International Fire Code and Appendices B through G, as adopted by the City of Mobile on July 6, 2004. All One- or Two-Family residential developments shall comply with Appendices B, C, and D of the 2003 International Fire Code, as adopted by the City of Mobile on July 6, 2004.

### **REMARKS**

The applicant is seeking Subdivision Approval to create two legal lots of record, Planning Approval to allow expansion of an existing church complex in an R-1, Single-Family Residential district, and is also seeking Planned Unit Development Approval to allow multiple buildings on a single site with shared access and parking between sites. The proposed improvements consist of constructing a new building and removing/remodeling an existing building.

Subdivision review examines the site with regard to promoting orderly development, protecting general health, safety and welfare, and ensuring that development is correlated with adjacent developments and public utilities and services, and to ensure that the subdivision meets the minimum standards set forth in the Subdivision Regulations for lot size, road frontage, lot configuration, etc.

Planning Approval review examines the applicant's location and site plan with regard to transportation, parking and access, public utilities and facilities, traffic congestion and hazard, and to determine if the proposal is in harmony with the orderly and appropriate development of the district.

Planned Unit Development (PUD) review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating excess traffic along minor residential streets in residential districts outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD.

It is very important to note that both the Planning Approval and the Planned Unit Development reviews are site plan specific; therefore any future changes to the site plan, as approved, by current or future applicants must be submitted for Planning Approval and Planned Unit

Development review. Thus any future proposed changes to parking, structure expansion, etc., must be reviewed and approved through the planning process.

As the reviews for Planning Approval and Planned Unit Development approval are similar, the analysis/remarks for both applications will be consolidated. The review, however, will begin with the proposed two-lot subdivision.

The site abuts single-family residential uses to the east and south, commercial uses to the west, and commercial and residential uses to the north. Cottage Hill Baptist Church has been at this location for over 50 years, and has seen numerous expansions of both worship and educational facilities.

Cottage Hill Baptist Church's proposed subdivision is a consolidation of a combined total of 16 lots and parcels, as well as a vacated street right-of-way, into two lots. The site fronts onto Cottage Hill Road, a major street with an existing 140-foot wide right-of-way, Demetropolis Road, a minor street with an existing 80-foot wide right-of-way, is bisected by South Thigpen Drive, a minor street with an existing 50-foot wide right-of-way, and has as its southern boundary Troy Lane, a minor street with an existing 50 foot right-of-way. The proposed Lot 1 will have frontage onto Cottage Hill, Demetropolis and South Thigpen, while the proposed Lot 2 will have frontage onto South Thigpen, Demetropolis and Troy.

As Cottage Hill Road is a major street, access management is a concern. The church currently has three access points onto Cottage Hill Road. Due to the 793 foot  $\pm$  frontage along Cottage Hill Road, no additional curb-cuts should be permitted. Along Demetropolis Road, the proposed Lots 1 and 2 have one curb-cut each; Lots 1 and 2 should be limited to one curb-cut each onto Demetropolis. Lot 1 currently has two parking-lot access points to South Thigpen Drive, and one drop-off access drive, while Lot 2 has three access points to South Thigpen Drive; as South Thigpen Drive is a minor street serving a residential area to the east of the church site, no additional curb-cuts should be permitted onto Thigpen, for either Lot 1 or Lot 2. Finally, Lot 2 has one access to Troy Lane, a minor street that primarily serves a small residential area; no additional curb-cuts from Lot 2 should be permitted. The design, size and location of any relocated curb-cuts or revised curb-cuts must be approved by Traffic Engineering and conform to AASHTO standards.

It should be noted that Cottage Hill Road was originally intended to have service roads on the north and south sides. A service road was constructed on the north side, extending from University Boulevard east to Oakfield Drive. Right-of-way for service roads on the south side of Cottage Hill Road is more intermittent, and as such there are no service roads on the south side within one mile east or west of the site, nor is there right-of-way on the south side within approximately  $\frac{1}{4}$  mile east or west of the site. Furthermore, the practice of requiring services roads has been revised to generally not require service roads along Cottage Hill Road where there are no existing rights-of-way or service roads.

The Planning Approval and Planned Unit Development reviews are required because 1) the church is located in an R-1, Single Family residential district; 2) multiple buildings occur on one

lot; and 3) parking required for the church is located on a separate lot, rather than on the same lot.

With regard to the general compatibility, the church and educational uses already occur on the site, and this application is for the construction of a new “Family Life Center” on the site – partially replacing an existing building, while increasing the overall building site coverage to approximately 27% on the proposed Lot 1, which is within the 35% building site coverage normally permitted within an R-1 district. No information was provided regarding the height of the proposed new construction, however, the Zoning Ordinance states that the maximum height within an R-1 district is 35 feet. Religious facilities and their associated uses are generally compatible with residential areas, although this church’s success (as evidenced by the size of the facilities, new construction and extensive parking areas) is a matter of concern due to the proximity of the adjacent residential area. The site also provides parking for twice the number of cars that might have been associated with the site if it had instead been developed solely for single-family residential use. Thus, should future expansions be anticipated by the church at this location, consideration should be given to the potential neighborhood impacts of church-associated parking, traffic and site development.

The existing parking area exceeds the number required by the Zoning Ordinance for the worship facility and associated uses. However, a portion of Lot 2 is unpaved, yet is apparently used for overflow parking; the overflow parking area should be paved with asphalt or an approved alternative surface, landscaped/buffered, and illuminated to City standards.

Regarding the presence of multiple buildings on the site, a PUD is required to permit more than one non-accessory structure to be located on one lot. As the church provides a wide array of activities beyond traditional worship, including school facilities, additional buildings are to be expected.

Religious facilities are allowed “by right” in commercial districts, where normally a 10-foot wide protection buffer would be required where a site is adjacent to residential uses, specifically either a 6 foot high wooden fence with a 10 foot setback, or a 10 foot wide landscape buffer. Information provided by the applicant does not clearly indicate if these buffers exist on the site, but they should be required where Lots 1 and 2 abut residential uses.

Sidewalks are normally required for residential and commercial developments. Lot 2 has sidewalks on its road frontages. Lot 1 appears to lack sidewalks along Cottage Hill and Demetropolis Roads, and South Thigpen Drive. Furthermore, the church’s large congregation walks from church-owned parking facilities on Lot 2, and apparently from parking located across Demetropolis (as indicated by the crosswalks on Demetropolis). As previously mentioned, the site also has school facilities, and thus children may need sidewalks to facilitate access to and from the site, and minimize safety concerns due to the proximity of a major street. Therefore, sidewalks should be required for Lot 1 along all of its road frontages.

Pedestrian and vehicular circulation is a concern for this site, as a school is operated on site, in addition to the worship facilities. School-related pick-up and drop-off in the mornings and afternoons may potentially increase traffic through the residential neighborhoods to the east,

along South Thigpen Drive. Worship activities also generate a high volume of vehicular traffic and on-site/near site pedestrian traffic due to the large size of the congregation.

The site plan does not indicate the location of a stormwater detention basin, which may be required for the site due to the new construction. If a detention basin is required by the City's Storm Water Drainage Ordinance, the site plan should be revised to depict the location of the basin. Revisions of the site plan, if they are significant, may require new PUD and Planning Approval applications.

Finally, there is no indication on the site plan as to the proposed location of any dumpster or other waste storage facility. If the applicant utilizes a dumpster, the location of the storage area for the dumpster must be indicated on the site plan, and the location and required screening must comply Section IV.D.9. of the Zoning Ordinance, as well as with all other applicable regulations.

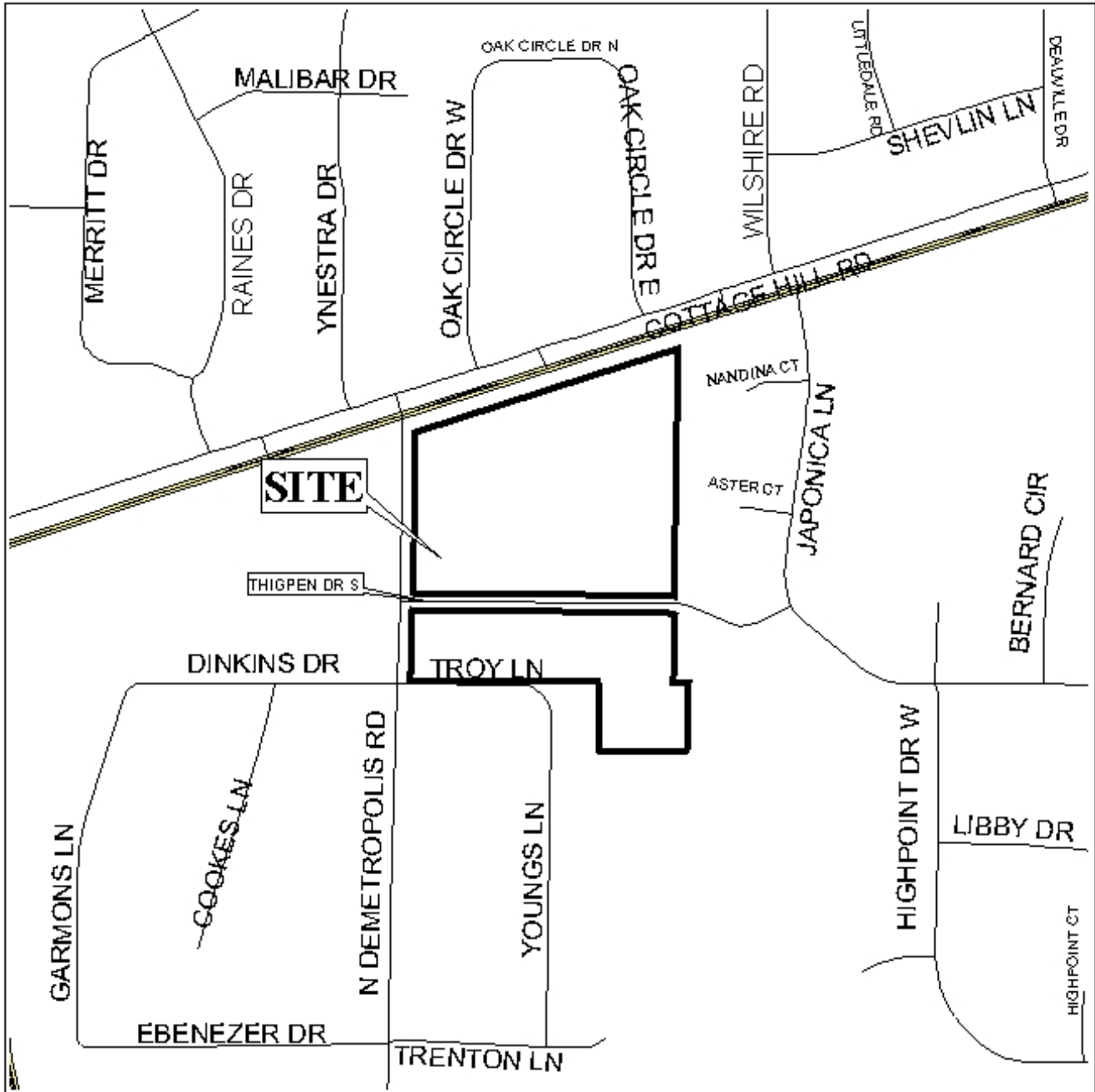
### **RECOMMENDATION**

**Subdivision Approval:** based on the preceding, it is recommended that the proposed subdivision be approved, with the following conditions: 1) placement of a note on the Final Plat stating that Lot 1 is limited to a maximum of three curb-cuts onto Cottage Hill Road, two curb-cuts and the existing drop-off along South Thigpen Drive, and one curb-cut onto Demetropolis Road, and that Lot 2 is limited to one curb-cut onto Troy Lane, one curb-cut onto Demetropolis Road, and three curb-cuts onto South Thigpen Drive, with the size, location and design to be approved by Traffic Engineering and conform to AASHTO standards; and 2) placement of a note stating that *"preservation status is to be given to the Two 48" Live Oak Trees and 54" Live Oak Tree located on the North East side of Lot 1. Any work on or under these trees are to be permitted and coordinated with Urban Forestry; removal to be permitted only in the case of disease or impending danger."*

**Planning Approval:** based on the preceding, it is recommended that the proposed building expansion be approved, subject to the following conditions: 1) placement of a note on the Site Plan stating that *"preservation status is to be given to the Two 48" Live Oak Trees and 54" Live Oak Tree located on the North East side of Lot 1. Any work on or under these trees are to be permitted and coordinated with Urban Forestry; removal to be permitted only in the case of disease or impending danger"*; 2) full compliance with landscaping and tree requirements of the Zoning Ordinance, to be coordinated with Urban Forestry; 3) full compliance with Section VI.A. (Off-Street Parking Requirements) of the Zoning Ordinance, as amended; 4) provision of protection buffers, in compliance with Section IV.D.1. of the Zoning Ordinance; 5) Lot 1 is limited to a maximum of three curb-cuts onto Cottage Hill Road, two curb-cuts and the existing drop-off along South Thigpen Drive, and one curb-cut onto Demetropolis Road, and that Lot 2 is limited to one curb-cut onto Troy Lane, one curb-cut onto Demetropolis Road, and three curb-cuts onto South Thigpen Drive, with the size, location and design to be approved by Traffic Engineering and conform to AASHTO standards; 6) full compliance with the Storm Water Drainage Ordinance, if required, including the provision of a storm water detention basin to be indicated on the final site plan; 7) provision of sidewalks for Lot 1 along Demetropolis and Cottage Hill Roads, and South Thigpen Drive; 8) depiction on the Site Plan of the location of any dumpster or waste storage facilities; and 9) full compliance with all other applicable municipal codes and ordinances.

**Planned Unit Development:** based on the preceding, it is recommended that the proposed building expansion be approved, subject to the following conditions: 1) placement of a note on the Site Plan stating that “*preservation status is to be given to the Two 48” Live Oak Trees and 54” Live Oak Tree located on the North East side of Lot 1. Any work on or under these trees are to be permitted and coordinated with Urban Forestry; removal to be permitted only in the case of disease or impending danger*”; 2) full compliance with landscaping and tree requirements of the Zoning Ordinance, to be coordinated with Urban Forestry; 3) full compliance with Section VI.A. (Off-Street Parking Requirements) of the Zoning Ordinance, as amended; 4) provision of protection buffers, in compliance with Section IV.D.1. of the Zoning Ordinance; 5) Lot 1 is limited to a maximum of three curb-cuts onto Cottage Hill Road, two curb-cuts and the existing drop-off along South Thigpen Drive, and one curb-cut onto Demetropolis Road, and that Lot 2 is limited to one curb-cut onto Troy Lane, one curb-cut onto Demetropolis Road, and three curb-cuts onto South Thigpen Drive, with the size, location and design to be approved by Traffic Engineering and conform to AASHTO standards; 6) full compliance with the Storm Water Drainage Ordinance, if required, including the provision of a storm water detention basin to be indicated on the final site plan; 7) provision of sidewalks for Lot 1 along Demetropolis and Cottage Hill Roads, and South Thigpen Drive; 8) depiction on the Site Plan of the location of any dumpster or waste storage facilities; and 9) full compliance with all other applicable municipal codes and ordinances.

# LOCATOR MAP



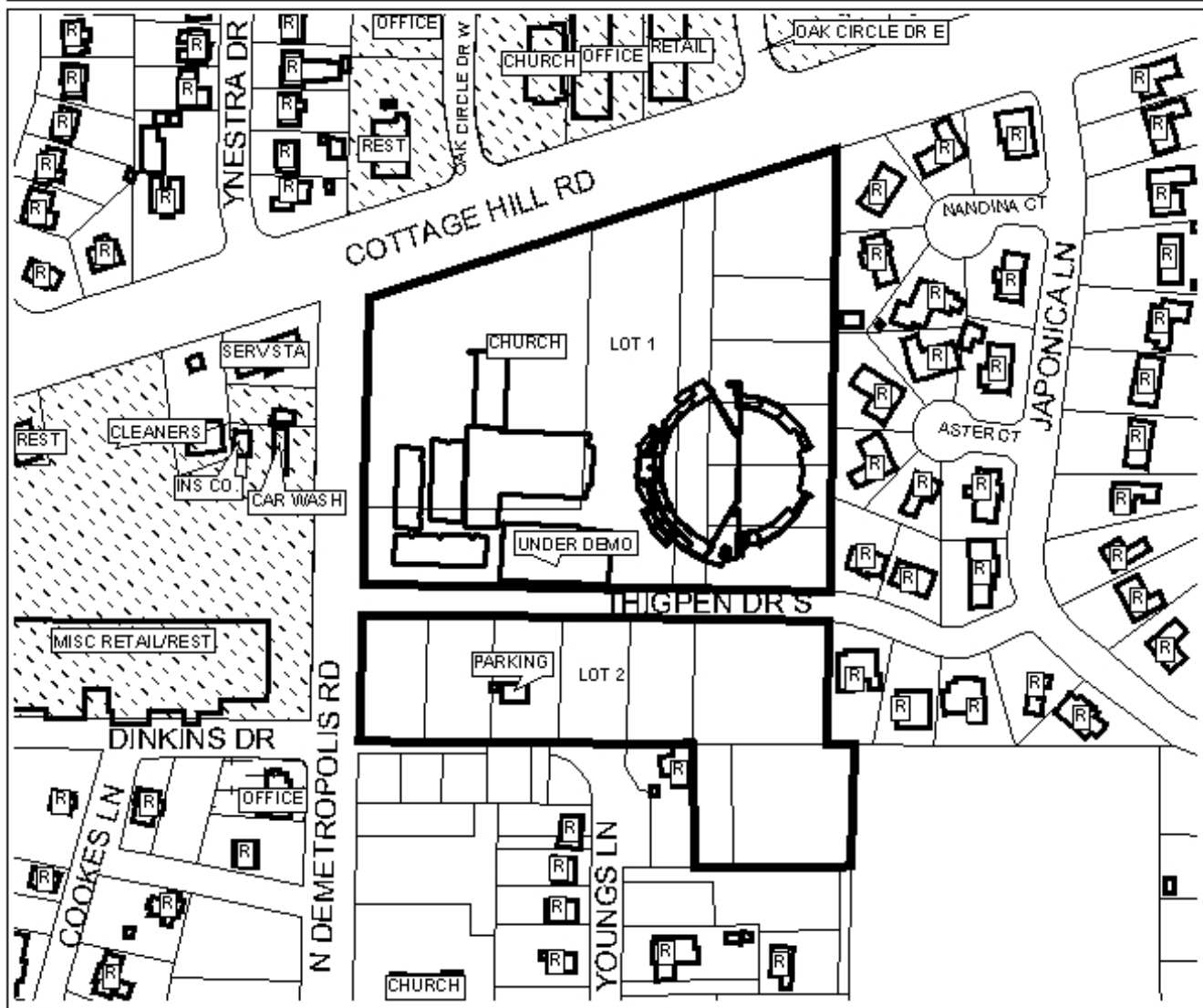
APPLICATION NUMBER 1 & 2 & 3 DATE July 21, 2005

APPLICANT Cottage Hill Baptist Church

REQUEST Planning Approval, Planned Unit Development, Subdivision

N  
NTS

# PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The site is surrounded by mixed land use

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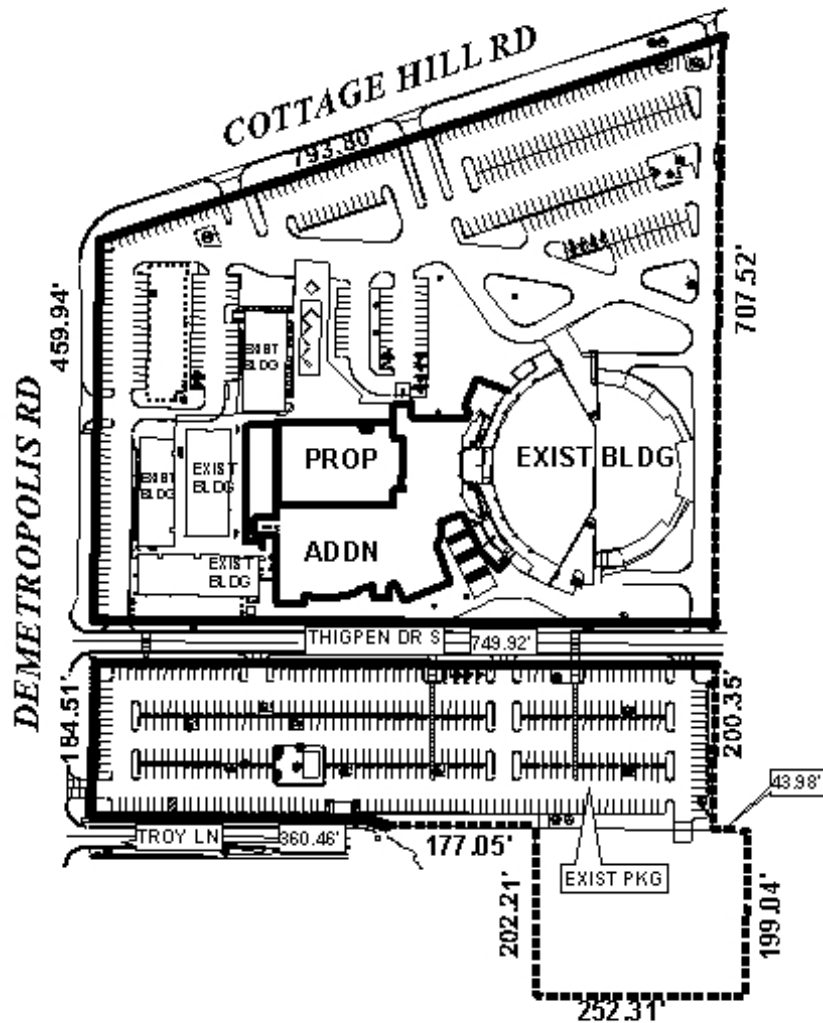
LEGEND

R-1	R-2	R-3	R-A	R-B	H-B	B-1	LB-2	B-2	B-3	B-4	B-5	I-1	I-2

N  
NTS



# SITE PLAN



The site plan illustrates the existing buildings, parking, drives, and landscaping along with the proposed buildings.

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