

## **CENTRAL BAPTIST SUBDIVISION**

Engineering Comments: Must comply with all stormwater and flood control ordinances. Any work performed in the right-of-way will require a right-of-way permit.

Traffic Engineering Comments: Driveway number, size, location, and design to be approved by Traffic Engineering and ALDOT and conform to AASHTO standards.

Urban Forestry Comments: Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64). Preservation status is to be given to the 75" Live Oak Tree located on the North West side of Lot 1. Any work on or under this tree is to be permitted and coordinated with Urban Forestry; removal to be permitted only in the case of disease or impending danger.

Fire Department Comments: All commercial projects shall comply with the 2003 International Fire Code and Appendices B through G, as adopted by the City of Mobile on July 6, 2004. All One- or Two-Family residential developments shall comply with Appendices B, C, and D of the 2003 International Fire Code, as adopted by the City of Mobile on July 6, 2004.

The plat illustrates the proposed 7.8 acre  $\pm$ , 2 lot subdivision which is located on the west side of Dauphin Island Parkway at the west terminus of Hurtel Street, and is in Council District 3. The subdivision is served by public water and sanitary sewer.

The purpose of this application is to create two lots from five existing parcels. The site is currently developed with a church, warehouse, businesses, and a communications tower. These uses appear to share access to Dauphin Island Parkway, as well as parking on the site.

Dauphin Island Parkway is a proposed major street, and as such should have a right-of-way width of 100 feet, according to the Major Street Plan. The Alabama Department of Transportation (ALDOT) states that the existing right-of-way in this area ranges from 85 to 90 feet wide, which conflicts with the width depicted on the preliminary plat. It is therefore recommended that the applicant confirm the right-of-way width with ALDOT, and if found to be less than 100 feet, dedicate an amount sufficient to provide a right-of-way width of 50 feet as measured from the centerline of Dauphin Island Parkway.

Access management is a concern along Dauphin Island Parkway because of its status as a major street. As previously mentioned, the site currently shares access via two curb-cuts onto Dauphin Island Parkway. It is recommended that the shared-use of curb-cuts be continued, with no additional curb-cuts provided. Should the existing curb-cuts need modification, or if new curb-cuts are proposed, the number, design, location and size of all curb-cuts must be approved by Traffic Engineering and ALDOT, and conform to AASHTO standards.

An administrative PUD will be required for this site to allow multiple buildings on the proposed lots, shared access to Dauphin Island Parkway, and if necessary, shared parking facilities. The

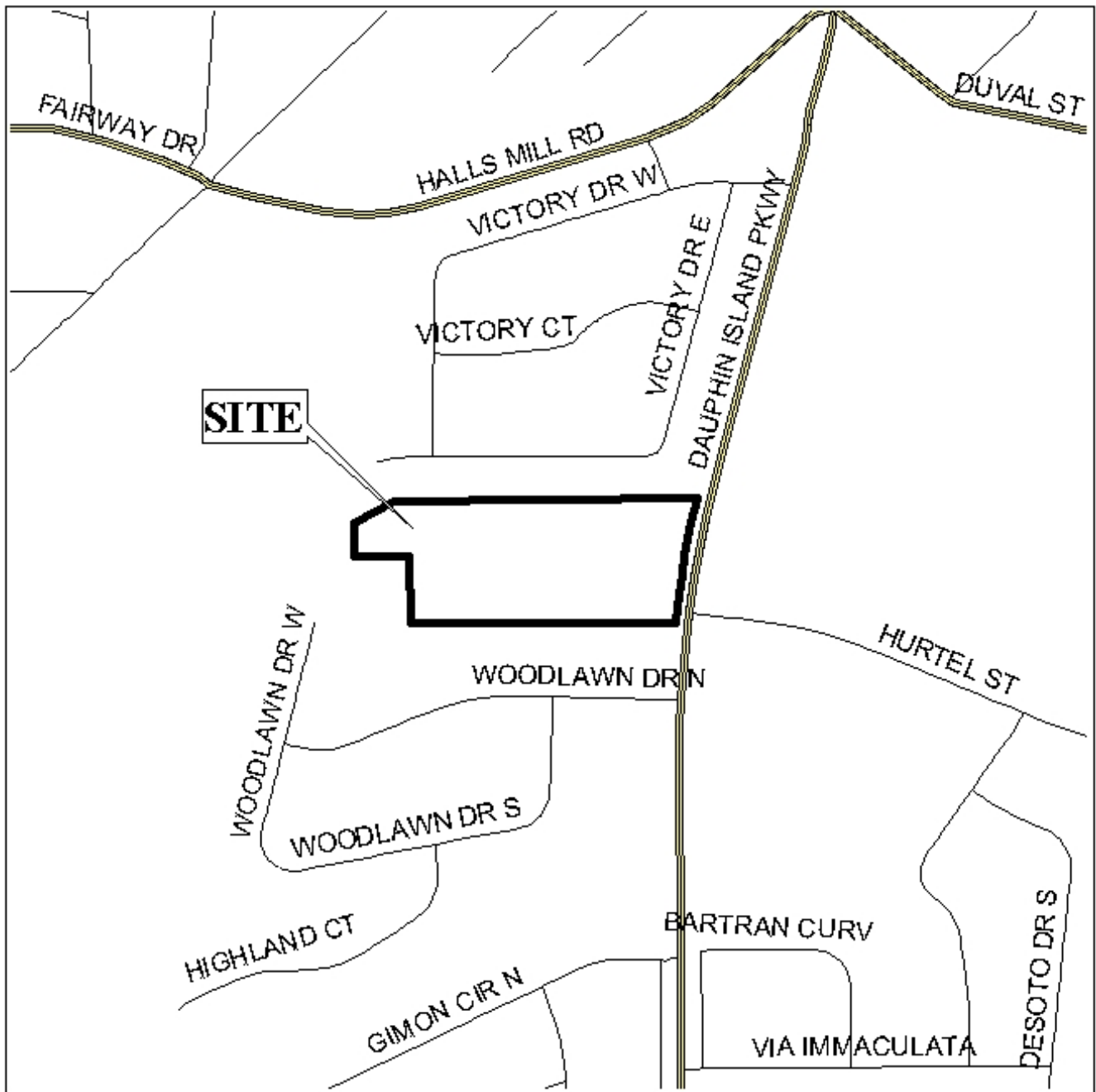
shared parking component of the PUD will only be necessary if the individual uses lack sufficient on-lot parking. Documentation of sufficient parking should be provided by the applicant; otherwise, shared parking will be included within the administrative PUD.

The 25-foot minimum building setback line is not depicted on the plat, but would be required on the final plat, if approved.

The proposed depth to width ratios for the two lots exceed the 3.5 depth to width ratio recommended in Section V.D.3. of the Subdivision Regulations; Lot 1's ratio is 3.65, while Lot 2's is approximately 8.25, due to the flag-shaped nature of the lot. Existing development adjacent to the north, south and west precludes other subdivision options, therefore a waiver of Section V.D.3 could be considered appropriate.

With a waiver of Section V.D.3., the plat is recommended for Tentative Approval, subject to the following conditions: 1) completion of an administrative PUD; 2) depiction of the 25-foot minimum building setback line; 2) placement of a note on the Final Plat stating that the lots will continue to share existing curb-cuts, and that any new or revised curb-cuts must be approved by Traffic Engineering and ALDOT, and conform to AASHTO standards; 3) placement of a note on the Final Plat stating that subdivision of the site to more than two lots is not permitted unless a public street is provided; and 4) if necessary, dedication of right-of-way sufficient to provide 50-feet of right-of-way width from the centerline of Dauphin Island Parkway.

## LOCATOR MAP



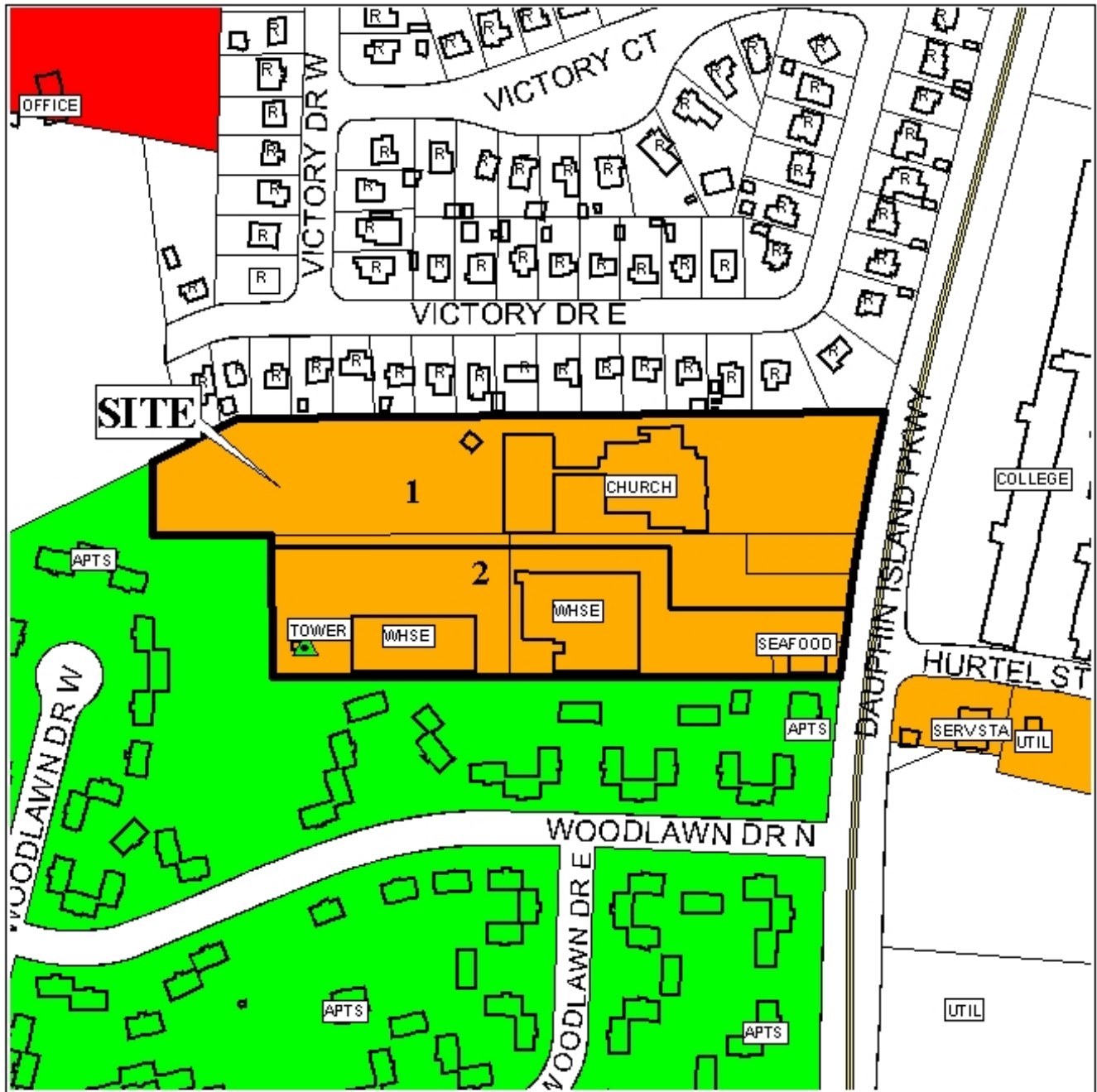
APPLICATION NUMBER 6 DATE June 16, 2005

APPLICANT Central Baptist Subdivision

REQUEST Subdivision



# CENTRAL BAPTIST SUBDIVISION



APPLICATION NUMBER 6 DATE June 16, 2005

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