

HIGH POINT WEST

Engineering Comments: Although this property does not lie within a floodzone it is a SIGNIFICANT drainage area for the surrounding watershed. Storm water runoff from Thigpen Drive, Japonica Lane, Highpoint Drive West, and Trenton Lane all drain through this site. Therefore, the drainage system will have to be designed to accommodate all existing flows onto the property. In addition, increased or concentrated storm water cannot be discharged onto an adjacent property owner without a hold harmless agreement, which appears to be necessary for this site, and there is no common area for stormwater detention shown on the preliminary plat, which will be necessary. Must comply with all stormwater and flood control ordinances. Any work performed in the right-of-way will require a right-of-way permit.

Traffic Engineering Comments: Driveway number, sizes, location and design to be approved by Traffic Engineering and conform to AASHTO standards.

Urban Forestry Comments: Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

Fire Department Comments: May require the addition of fire hydrants. All single-family or two-family residential developments shall comply with Appendices B, C, and D of the 2003 International Fire Code, as adopted by the City of Mobile on July 6, 2004.

The plat illustrates the proposed 15 ± acre, 26 lot subdivision which is located at the east terminus of Trenton Lane, extending to the west terminus of an unnamed street stub, 200 feet ± west of Highpoint Drive South and Highpoint Drive West, and is in Council District 4. The subdivision is served by public water and sanitary sewer.

The purpose of this application is to create a twenty-six lot subdivision from one existing parcel.

Two public rights-of-way for minor streets stub into the site. Trenton Lane, on the western side of the site, has a 37 ½ foot wide right-of-way where it stubs into the property, and is paved to within 173 feet ± of the site. On the eastern side of the site, an unnamed street stub has a 50-foot right-of-way from High Point Drive to the site, and appears to be paved to the edge of the site. The applicant proposes to connect to this second, unnamed street-stub. Section V.B.1. of the Subdivision Regulations states *“The street layout shall be devised for the most advantageous development of the entire neighborhood or neighborhoods in which the land to be subdivided is located. Existing streets that abut the subdivision shall be continued, and the continuations shall be at least as wide as the existing streets and in alignment with them.”* Due to the substandard right-of-way width of the Trenton Lane street stub, however, waiver of Section V.B.1. is recommended.

The proposed subdivision includes two cul-de-sacs. The longer of the two cul-de-sacs appears to be approximately 820 feet long, which exceeds the recommended length of 600 feet specified in Section V.D.6. of the Subdivision Regulations. As the land around the site is already developed,

there are no opportunities for additional street stub connections to the east or west; thus waiver of Section V.D.6. is recommended.

The 25-foot minimum building setback line from the right-of-way edge is not shown (though it is noted on the plat) but would be required on the Final Plat, if approved.

It should be noted that the site has areas of steep topography. These areas are generally located along the eastern and western edges of the site, and in some cases the steep slopes appear to reach a 1 to 1 ratio – 1 foot rise for every 1 foot of distance. The design of the subdivision appears to provide enough area on each lot to allow the construction of a single-family home, while avoiding the steepest topography.

Five parcels located outside of the proposed subdivision, but adjacent to its western boundary, are currently land-locked: they do not have frontage onto a maintained public street. However, existing conditions show that some of the land-locked parcels are owned by Cottage Hill Baptist Church, and are adjacent to other parcels owned by the Church which front public rights-of-way, or are land-locked parcels owned by related families and that abut other family-owned parcels which front public rights-of-way.

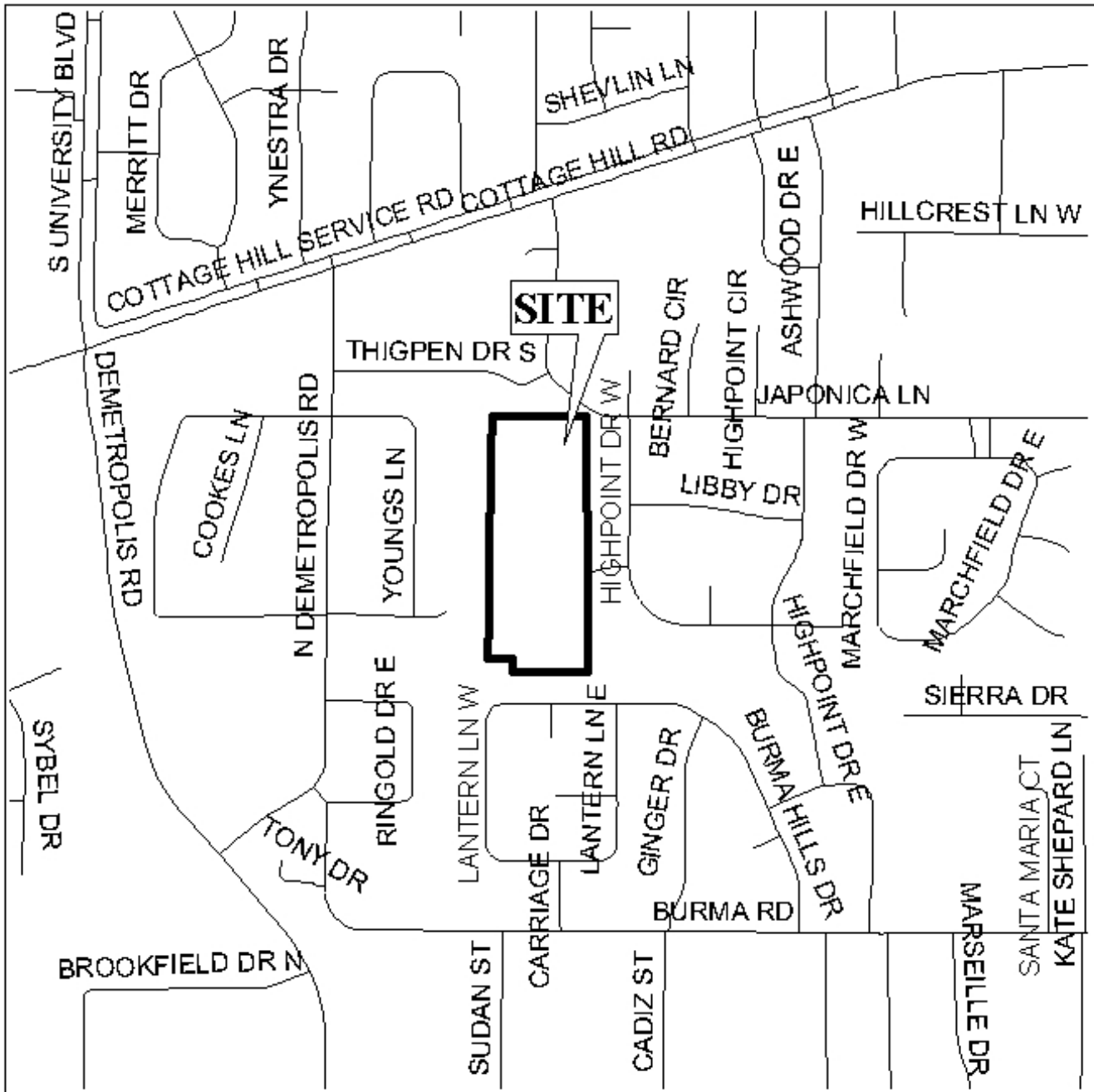
The site includes the upper reaches of Moore Creek, a tributary of Dog River. The site is the low point for a large drainage area that extends as far west as University Boulevard / Demetropolis Road, as far north as Wesley Lane South (approximately 4 blocks north of Cottage Hill Road), and as far east as Ashwood Drive East, containing about 3/4 square miles. Due to the presence of the creek drainage channel, wetlands or hydric soils may occur on the site. Thus, the potential presence of wetlands, in addition to the creek, indicate that the area may be environmentally sensitive; therefore, the approval of all applicable federal, state and local agencies would be required prior to the issuance of any permits.

As mentioned in the Engineering Department's comments, no stormwater detention basin is indicated on the subdivision site plan. A stormwater detention basin, sized to meet the requirements of the City's Storm Water Drainage Ordinance, must be indicated on the final plat. Additionally, a note should be placed on the final plat stating that maintenance of the detention basin common area is the responsibility of the subdivision's property owners.

With a waiver of Section V.B.1. for the existing substandard street stub on the west side of the site, and a waiver of Section V.D.6. for cul-de-sac length, the plat is recommended for Tentative Approval subject to the following conditions: 1) the depiction of the 25-foot minimum building setback line on the Final Plat; 2) compliance with all applicable federal, state and local regulations regarding the wetlands and floodplain issues; 3) full compliance with the Engineering comments (*Although this property does not lie within a floodzone it is a SIGNIFICANT drainage area for the surrounding watershed. Storm water runoff from Thigpen Drive, Japonica Lane, Highpoint Drive West, and Trenton Lane all drain through this site. Therefore, the drainage system will have to be designed to accommodate all existing flows onto the property. In addition, increased or concentrated storm water cannot be discharged onto an adjacent property owner without a hold harmless agreement, which appears to be necessary for this site, and there is no common area for stormwater detention shown on the preliminary plat,*

which will be necessary. Must comply with all stormwater and flood control ordinances. Any work performed in the right-of-way will require a right-of-way permit.); and 4) placement of a note on the final plat stating that the maintenance of the detention basin common area is the responsibility of the subdivision's property owners.

LOCATOR MAP

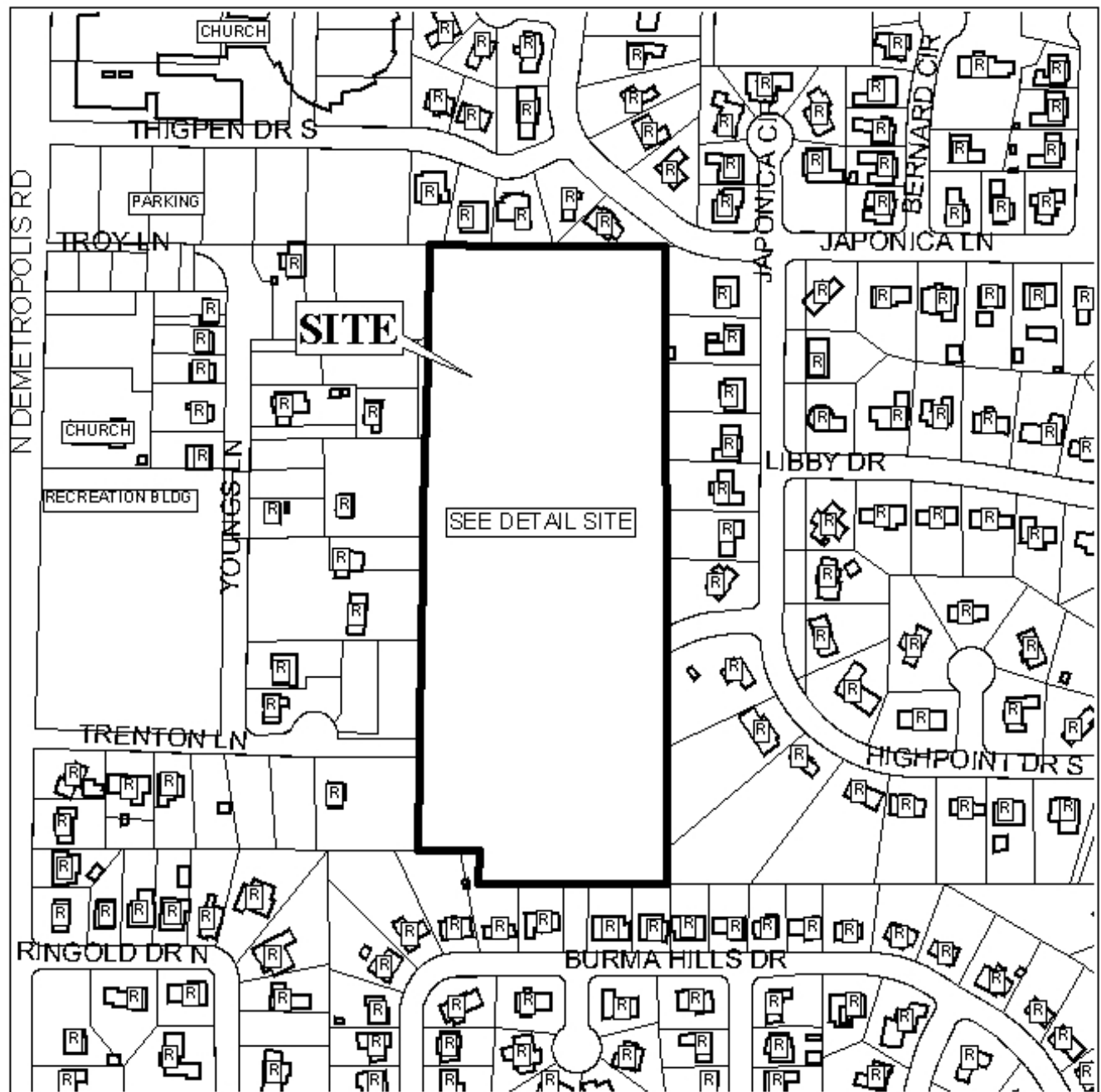


APPLICATION NUMBER 19 DATE April 7, 2005
APPLICANT High Point West Subdivision
REQUEST Subdivision



NTS

HIGH POINT WEST SUBDIVISION



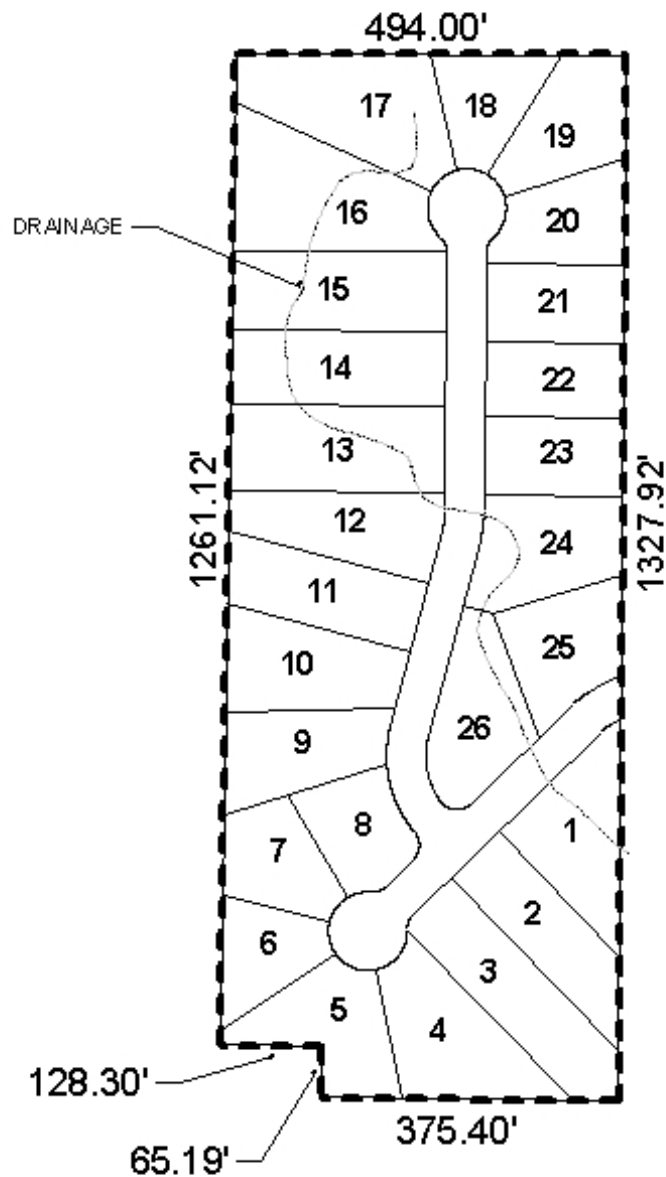
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LEGEND

R-1	R-2	R-3	R-A	R-B	H-B	B-1	LB-2	B-2	B-3	B-4	B-5	I-1	I-2

N
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DETAIL SITE PLAN



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