

LOV-A-LOTS SUBDIVISION

Engineering Comments: Must comply with all stormwater and flood control ordinances. Any work performed in the right-of-way will require a right-of-way permit.

Traffic Engineering Comments: Driveway numbers, size, location and design to be approved by Traffic Engineering and conform to AASHTO standards.

Urban Forestry Comments: Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

The plat illustrates the proposed 2 lot, .6 ± acre subdivision which is located on the North side of Fairway Drive, 225' ± West of Courtney Street. The subdivision is served by public water and sanitary sewer.

The purpose of this application is to subdivide one lot into two lots of records.

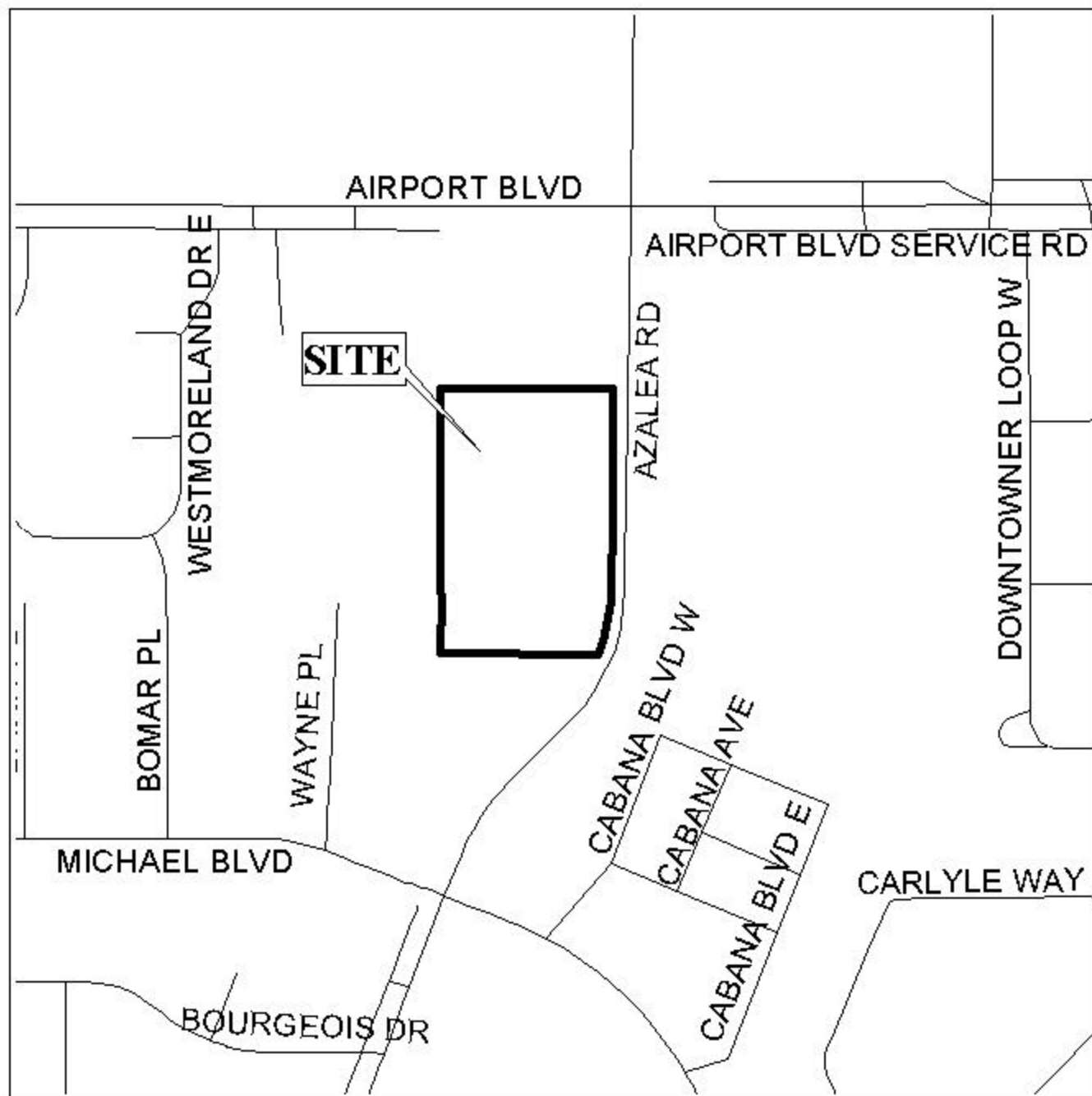
The site fronts Fairway Drive, a planned major street, which has an existing right-of-way of 50 feet; the Major Street Plan requires a 100-foot right-of-way. Therefore, the setback of sufficient right-of-way from the centerline of Fairway Drive to provide for the right-of-way of the planned major street, should be required. Additionally, as a means of access management, the placement of a note on the final plat stating that both lots are limited to one curb cut each to Fairway Drive, with the size, location and design to be approved by Traffic Engineering, should be required.

As illustrated on the Vicinity Map, a structure seems to be constructed near the proposed property line, possibly creating a zoning violation. The Zoning Ordinance requires a minimum 8-foot side yard setback; and accessory structures are not allowed on a residential lot without a primary structure. Therefore, it should be illustrated on the final plat that the existing structure provides the minimum setback requirement of 8-feet, or the submission of documentation that the structure has been moved, should be required. Additionally, accessory structures appears to be on Lot A; therefore, removal of the accessory structures will be also required prior to the recording of the final plat.

The 25-foot minimum building setback lines are not shown but would be required on the final plat.

The plat meets the minimum requirements of the Subdivision Regulations and is recommended for Tentative Approval subject to the following conditions: 1) the setback of sufficient right-of-way from the centerline of Fairway Drive; 2) the placement of a note on the final plat stating that both lots are limited to one curb cut each, with the design and location to be approved by Traffic Engineering; 3) the provision of an 8-foot setback for the structure on Lot A or the removal of the structure prior to the recording of the final plat; and 4) the placement of the 25-foot minimum building setback lines on the final plat.

LOCATOR MAP



APPLICATION NUMBER 16 DATE February 5, 2004

APPLICANT Azalea Center Subdivision

REQUEST Subdivision



LOV-A-LOTS SUBDIVISION



APPLICATION NUMBER 21 DATE February 5, 2004

LEGEND



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