

## **KEY STREET COMMERCIAL PARK SUBDIVISION**

Engineering Comments: Must comply with all stormwater and flood control ordinances. Any work performed in the right-of-way will require a right-of-way permit.

Traffic Engineering Comments: Driveway numbers, size, location and design to be approved by Traffic Engineering and conform to AASHTO standards.

Urban Forestry Comments: Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

The plat illustrates the proposed 4 lot, 3.4  $\pm$  acres subdivision which is located on the West side of West I-65 Service Road South, 90'  $\pm$  South of Key Street, extending to the South side of Key Street, 390'  $\pm$  West of West I-65 Service Road South and is in City Council District 5. The subdivision is served by public water and sanitary sewer.

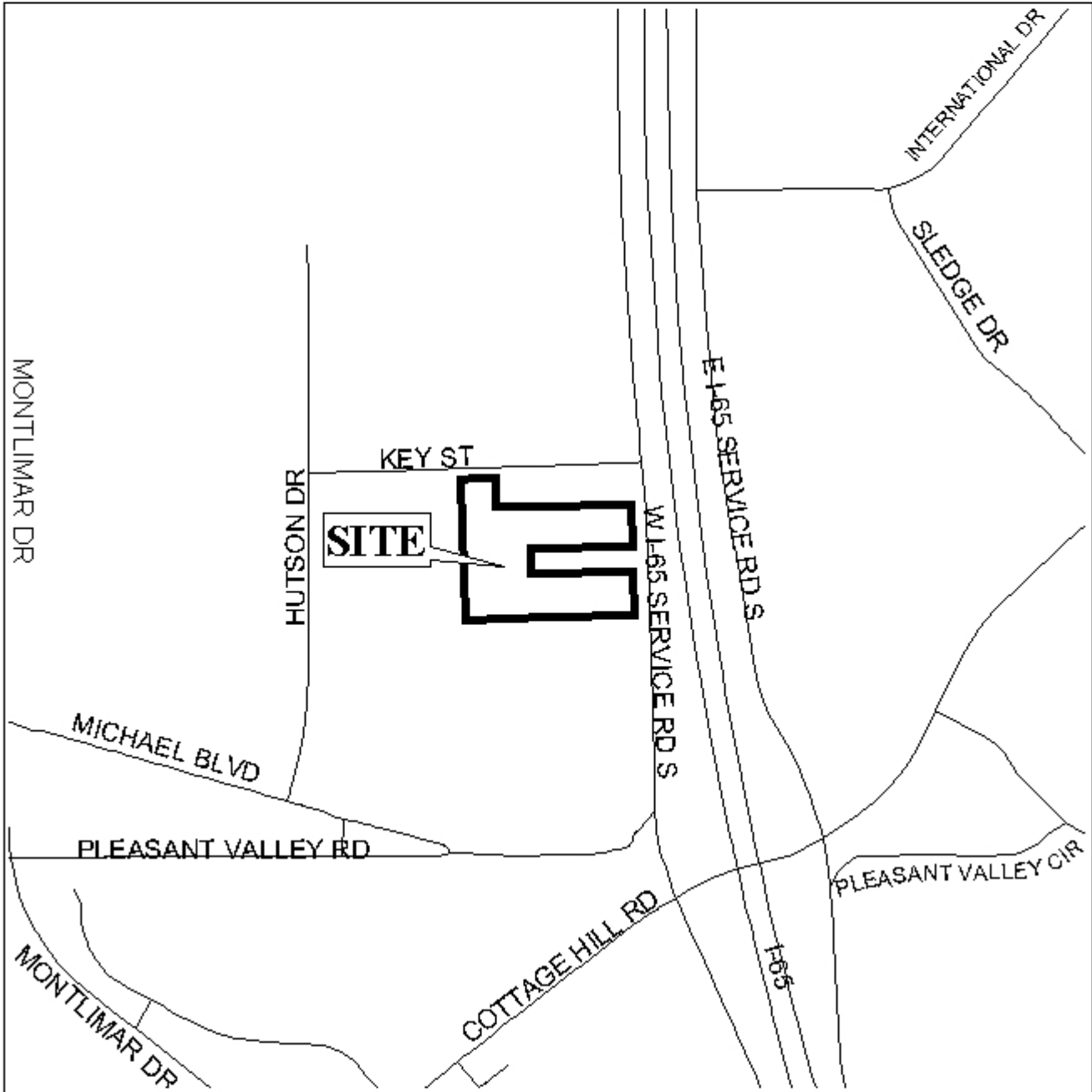
The purpose of this application is to create a four-lot subdivision from a metes and bounds parcel.

The site fronts West I-65 Service Road South, a planned major street as illustrated on the Major Street Plan and has an existing right-of-way in compliance with the Plan. However, as a means of access management, the placement of a note on the final plat stating that Lots 1 and 4 are limited to one curb cut each to the Service Road, and Lots 2 and 3 are limited to one curb cut each to Key Street, with the location, size, and design to be approved by Traffic Engineering, should be required.

The 25-foot minimum building setback lines are not shown but would be required on the final plat.

The plat meets the minimum requirements of the Subdivision Regulations and is recommended for Tentative Approval subject to the following conditions: 1) the placement of a note on the final plat stating that Lots 1 and 4 are limited to one curb cut each to the Service Road, and Lots 2 and 3 are limited to one curb cut each to Key Street, with the location, size, and design to be approved by Traffic Engineering; and 2) the placement of the 25-foot minimum setback lines on the final plat.

## LOCATOR MAP



APPLICATION NUMBER 16 DATE April 1, 2004

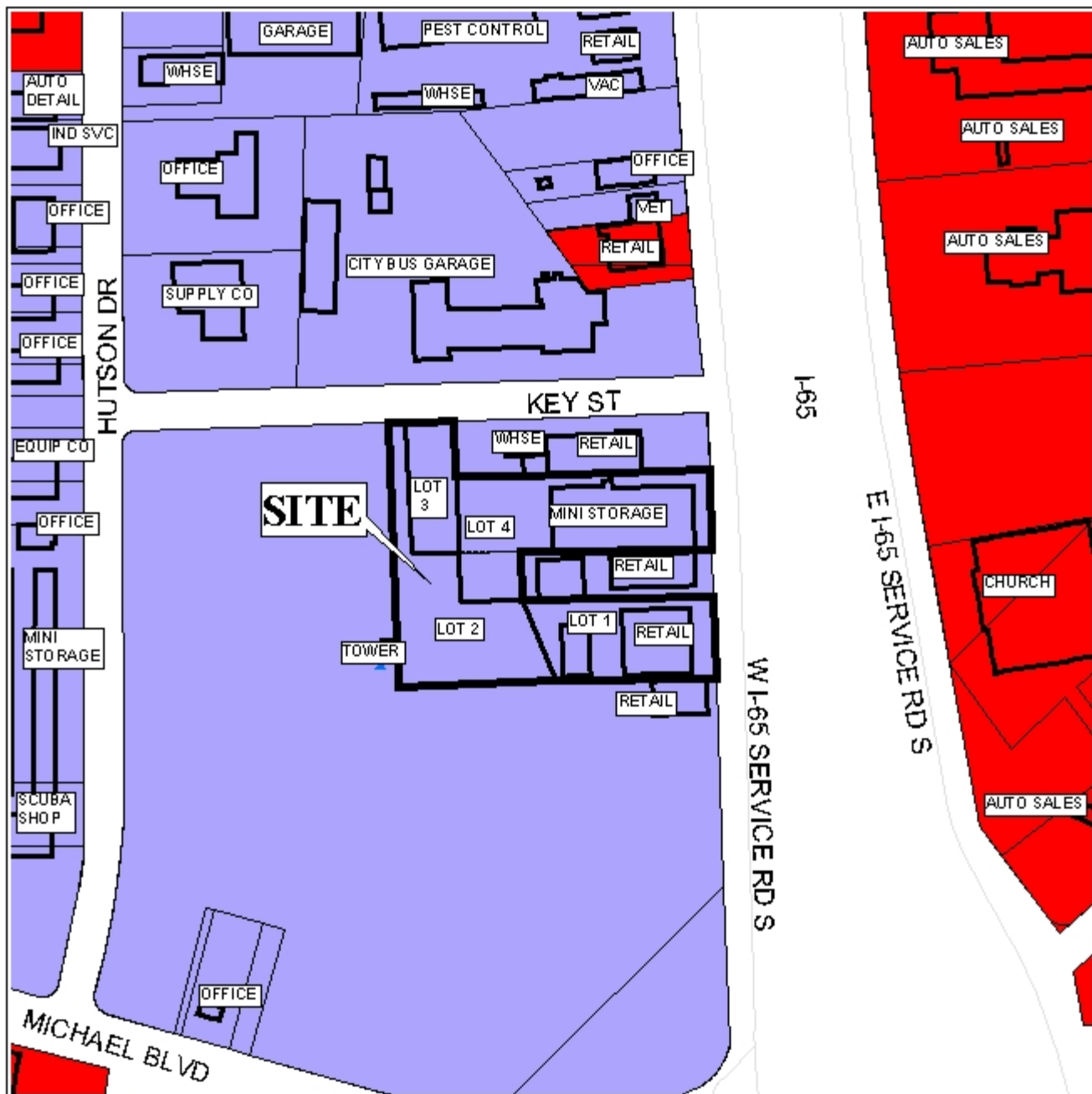
APPLICANT Key Street Commercial Park Subdivision

REQUEST Subdivision



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# KEY STREET COMMERCIAL PARK SUBDIVISION



APPLICATION NUMBER 16 DATE April 1, 2004

LEGEND

R-1	R-2	R-3	R-A	R-B	H-B	B-1	LB-2	B-2	B-3	B-4	B-5	I-1	I-2
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