

**PLANNED UNIT DEVELOPMENT &
SIDEWALK WAIVER REQUEST
STAFF REPORT**

Date: February 21, 2008

NAME

Valenti Southeast Realty

LOCATION

3215 Airport Boulevard
(South side of Airport Boulevard, extending to the North
side of Airport Boulevard Service Road, 670'+ West of Bel
Air Boulevard)

**CITY COUNCIL
DISTRICT**

District 5

PRESENT ZONING

B-3, Community Business District

AREA OF PROPERTY

1 Lot / 1.07 ± Acres

CONTEMPLATED USE

Planned Unit Development approval to allow a reduced
front setback for a restaurant, and *Sidewalk Waiver*
approval to waive construction of sidewalks along Airport
Boulevard, Airport Boulevard Service Road, and two mall
entrance drives.

**TIME SCHEDULE
FOR DEVELOPMENT**

Immediate

**ENGINEERING
COMMENTS**

Must comply with all storm water and flood control
ordinances. Any work performed in the right of way will require a right of way permit. Based
on information provided by the applicant, it appears that the lack of space between the back of
curb and property line would prevent the construction of City standard sidewalk.

**TRAFFIC ENGINEERING
COMMENTS**

Driveway number, size, location, and design to be
approved by Traffic Engineering and conform to AASHTO standards. Handicap ramps and
crosswalk to Airport Boulevard (at Northeast or Northwest corner) will be the responsibility of
developer, with their location and design to be coordinated with Traffic Engineering. The
existing entrance should be closed and new entrances located to allow for better traffic flow and
ingress/egress, with the new entrances directly aligning with the parking access aisles adjacent to
the proposed restaurant.

**URBAN FORESTRY
COMMENTS**

Property to be developed in compliance with state and local
laws that pertain to tree preservation and protection on both city and private properties (State Act
61-929 and City Code Chapters 57 and 64).

FIRE DEPARTMENT**COMMENTS**

Fire hydrants shall comply with Section 508.5.1 of the 2003 IFC. FDC shall be within 100 feet of fire hydrant. Sprinkler systems shall be electronically monitored per Section 903.4 thru 903.4.3.

REMARKS

The applicant is requesting Planned Unit Development approval allow a reduced front setback for a restaurant, and Sidewalk Waiver approval to waive construction of sidewalks along Airport Boulevard, Airport Boulevard Service Road, and two mall entrance drives.

The site is currently a parking lot with approximately 103 spaces, and is bounded on all sides by properties generally associated with either the Colonial Bel Air Mall or Springdale Mall developments, in a B-3 district. The site is immediately bounded by Airport Boulevard to the North, a service road to the South, and mall entrance roads to the East and West.

Planned Unit Development review, stated in Section 64-5. of the Zoning Ordinance, examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating excess traffic along minor residential streets in residential districts outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD. PUD approval is site plan specific, thus any changes to the site plan must be approved by the Planning Commission.

The applicant is requesting a reduction in the required 25-foot building setback along the southern boundary to accommodate the proposed 6,270 square foot restaurant. The main portion of the building will be approximately 13 feet from the southern property boundary, however, the enclosure for the dumpsters will be only 6 feet from the southern property boundary. The site plan should be revised to depict a 25-foot minimum building setback line from all street frontages, except for where the site plan depicts the building and dumpster enclosure encroaching upon the normal setback.

It also appears that some of the curbing associated with the parking area may cross the property boundary on the South and West sides. Any new construction must take place fully within the property, with the exception of the proposed pedestrian access to Airport Boulevard.

The site plan depicts the building and 63 parking spaces, with 3 additional spaces provided for motorcycles. Interior circulation appears to meet minimum dimensional requirements as it relates to the width of access aisles, but the layout of the parking does not appear to provide much accommodation for vehicular turning radii, thus setting the stage for congestion and accidents on the site during parking maneuvering and circulation – thus the site may be being overbuilt. The congestion caused by the site design may result in back-ups into adjacent roads. The number of parking spaces, however, does meet the minimum quantity requirements for the restaurant.

As previously mentioned, the site has frontage on Airport Boulevard, Airport Boulevard Service Road, and two mall entrance drives, all of which are public streets with adequate rights-of-way. Since Airport Boulevard is a proposed major street, and as the site is entirely bounded by streets, access management for the site is a concern. Traffic counts from 2006 indicate that approximately 43,300 cars per day passed the site on average on Airport Boulevard. The site plan submitted with the application depicts two curb-cuts onto the service road along the southern boundary of the site: the two existing curb-cuts are to be closed and replaced with two new curb-cuts. Staff believes that two curb-cuts are appropriate for the site, and that the location of the curb-cuts along the southern boundary would be most appropriate for the site due to the high traffic volumes along Airport Boulevard as well as the entrance and exit roads for the mall property. Therefore the site should be limited to a maximum of two curb-cuts along the southern boundary of the property. The new curb-cut on the Western side of the building should align with the parking aisle, rather than being offset, even though this will require additional cost due to an existing storm water drain inlet. The size, design and location of the new curb-cuts are to be approved by Traffic Engineering and designed in conformance with AASHTO standards.

Calculations are provided for the landscape area, and it appears that the total area will exceed the minimum requirements of the Zoning Ordinance. Additional landscape area, thus additional area for tree planting, can be provided by curving the corners of the parking area where the original curb-cuts are being eliminated, and by replacing the proposed motorcycle parking spaces with landscape areas. A tree plan depicts a sufficient quantity of trees to meet the frontage requirements, and appears to provide additional extra overstory trees within the lot interior, however, three (3) additional understory trees may be required for the parking lot requirements. The palm trees that exist around the perimeter of the site appear to either be on the site or to straddle the property line: if on the site, they can count towards meeting the frontage tree requirement. Full compliance with the tree requirements of the Zoning Ordinance should be coordinated with Urban Forestry.

The applicant is requesting the waiver of the sidewalk requirements along all street frontages. Staff believes that adjacent parking areas will be utilized by customers, and thus that pedestrian traffic may be increased due to the restaurant. However, sidewalk waivers were granted for the bookstore and coffee house located to the West of the site, thus not granting a waiver for this location could be considered arbitrary and capricious. It should also be pointed out that public rights-of-way adjacent to the site may lack adequate width, and would require the removal of existing landscape materials, including large shrubs and palm trees. Traffic Engineering has requested that a provision for pedestrian access be provided to Airport Boulevard, at the Northeast or Northwest corner of the site.

RECOMMENDATION

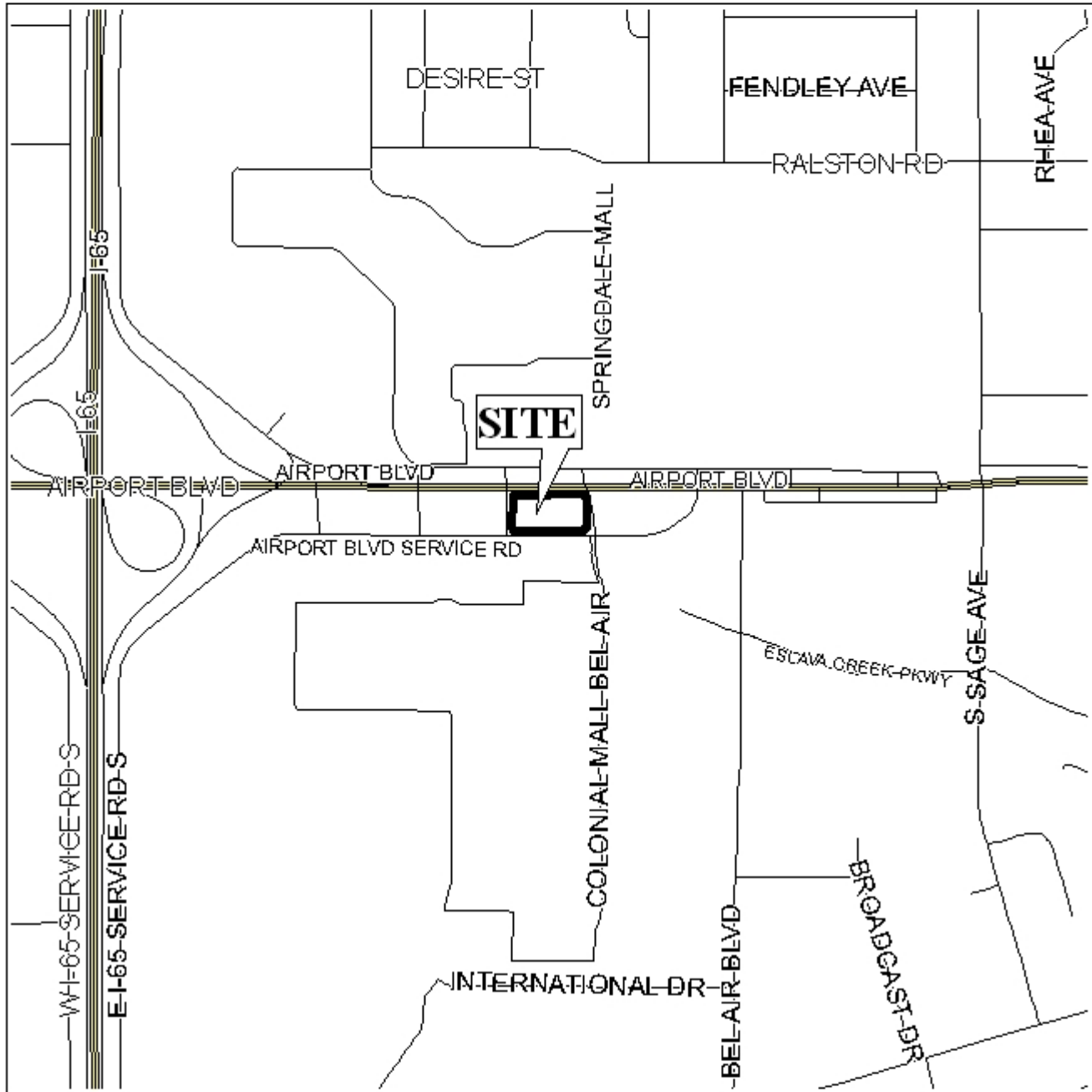
Planned Unit Development: The PUD request is recommended for Approval, subject to the following conditions:

- 1) The site is limited to two curb-cuts to the Airport Boulevard Service Road to align with the parking aisles located immediately next to the proposed restaurant, and the site plan should be revised to adjust the Western curb-cut to align with the parking aisle, with the size and design to be approved by Traffic Engineering and to conform with AASHTO standards;
- 2) Existing curb-cuts are to be removed and landscaped to match adjacent right-of-way;

- 3) The site is denied any curb-cuts onto to Airport Boulevard;
- 4) Revision of the site plan to depict the 25-foot minimum building setback line along all street frontages, except where the building and dumpster will encroach upon the setback – the setback line to follow the building and dumpster enclosure as depicted, not to be less than 6-feet from the property line;
- 5) Compliance with Traffic Engineering comments (*Driveway number, size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards. Handicap ramps and crosswalk to Airport Boulevard will be the responsibility of developer, with their location – either the Northeast or Northwest corner – and design to be coordinated with Traffic Engineering. The existing entrance should be closed and new entrances located to allow for better traffic flow and ingress/egress, with the new entrances directly aligning with the parking access aisles adjacent to the proposed restaurant.*);
- 6) Revision of the site plan to ensure that all parking and new curbing for the parking areas occurs within the boundaries of the site, and that vehicles will not overhang the property boundaries – installing wheel stops as necessary;
- 7) Coordination with Urban Forestry regarding compliance with the tree requirements of the Zoning Ordinance;
- 8) Revision of the site plan to provide additional landscape area by curving the parking area where the original curb-cuts are eliminated, and consideration of replacing the motorcycle parking spaces with additional landscape area; and
- 9) Submission of a revised PUD site plan, per the above conditions, prior to the submittal of revised drawings for the commercial site plan review process.

Sidewalk Waiver: The request is recommended for Approval, with the exception that pedestrian access is to be provided at the Northeast or Northwest corner of the site, to be coordinated with Traffic Engineering.

LOCATOR MAP



APPLICATION NUMBER 34 & 35 DATE February 21, 2008

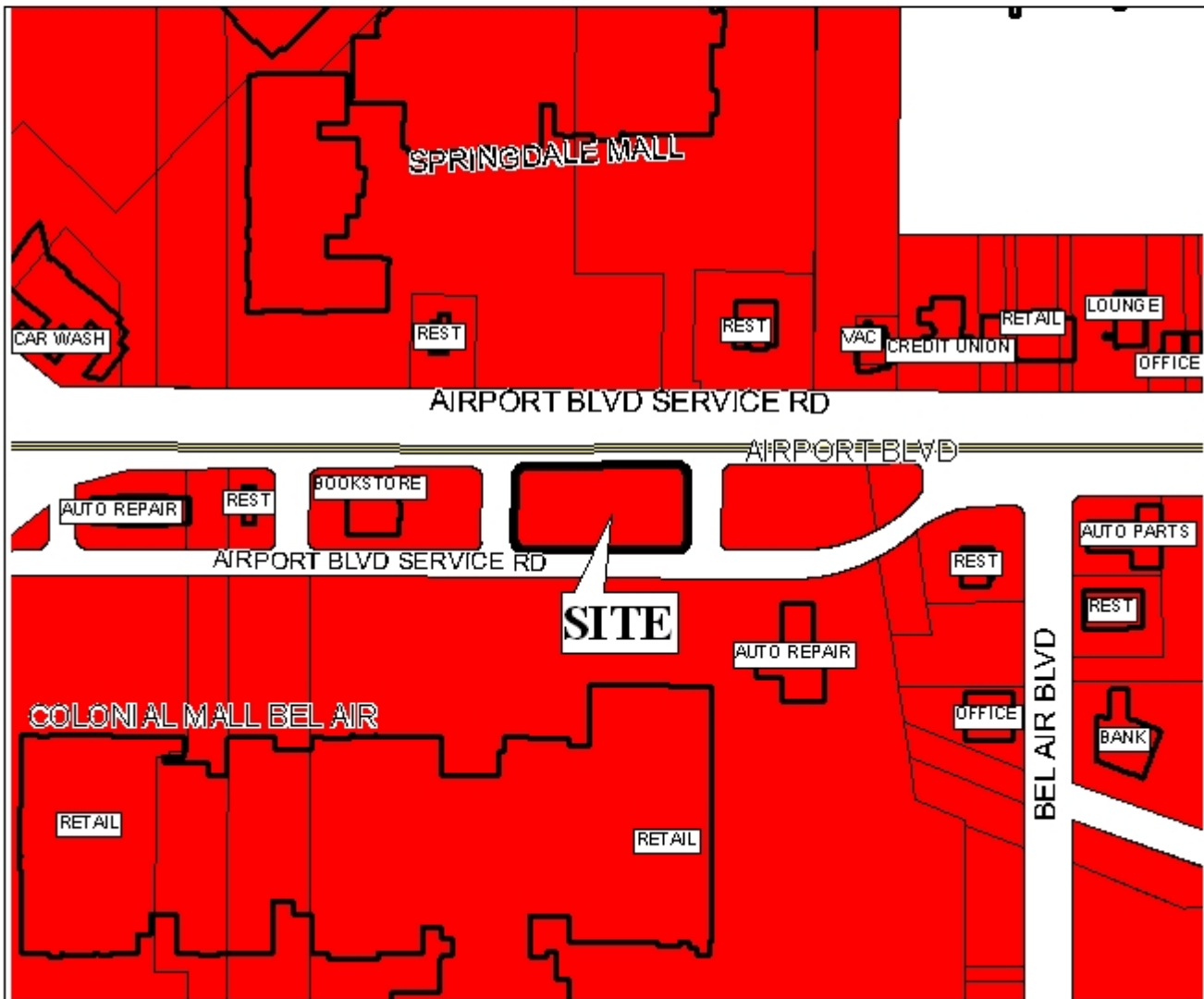
APPLICANT Valenti Southeast Realty

REQUEST PUD, Sidewalk Waiver



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PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



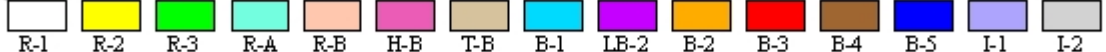
The site is surrounded by miscellaneous commercial land uses.

APPLICATION NUMBER 34 & 35 DATE February 21, 2008

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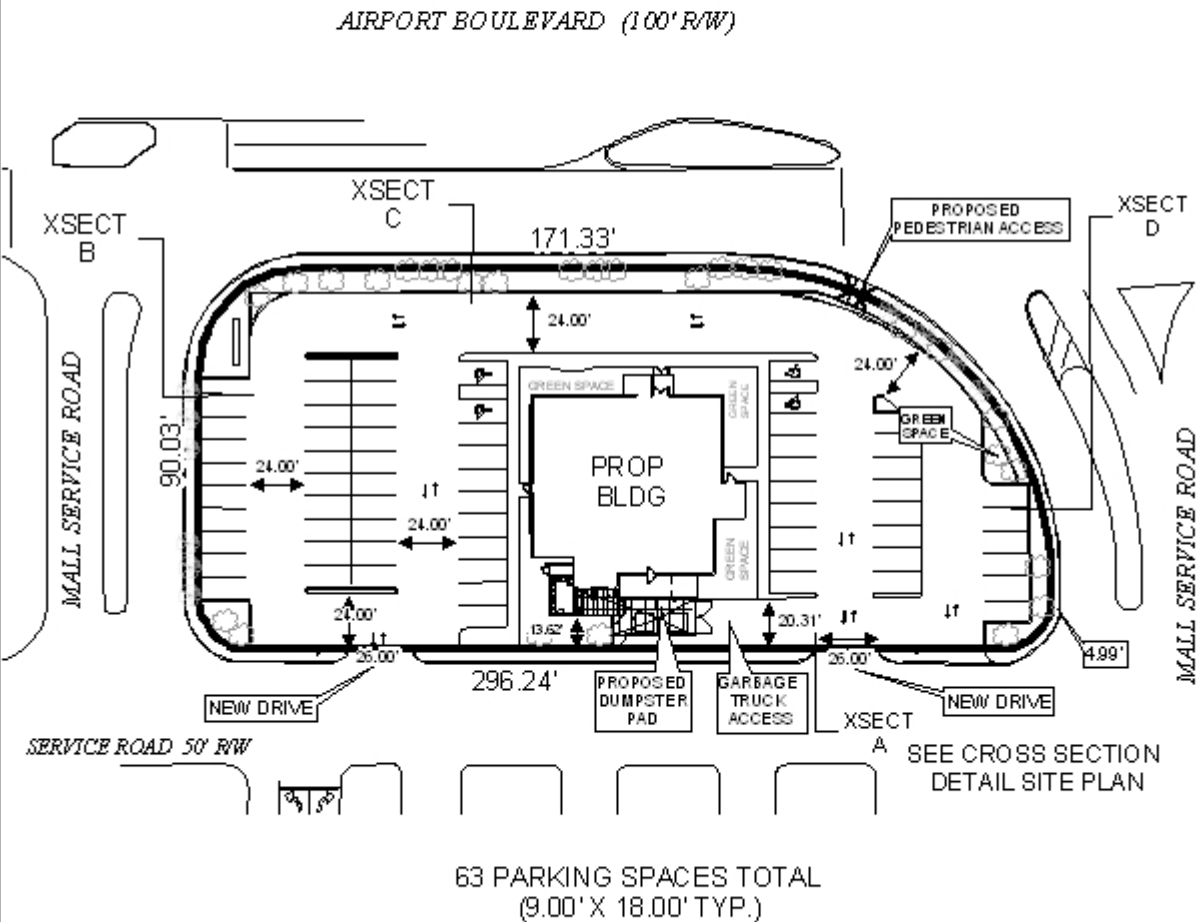
REQUEST Planned Unit Development, Sidewalk Waiver

LEGEND



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NTS

SITE PLAN



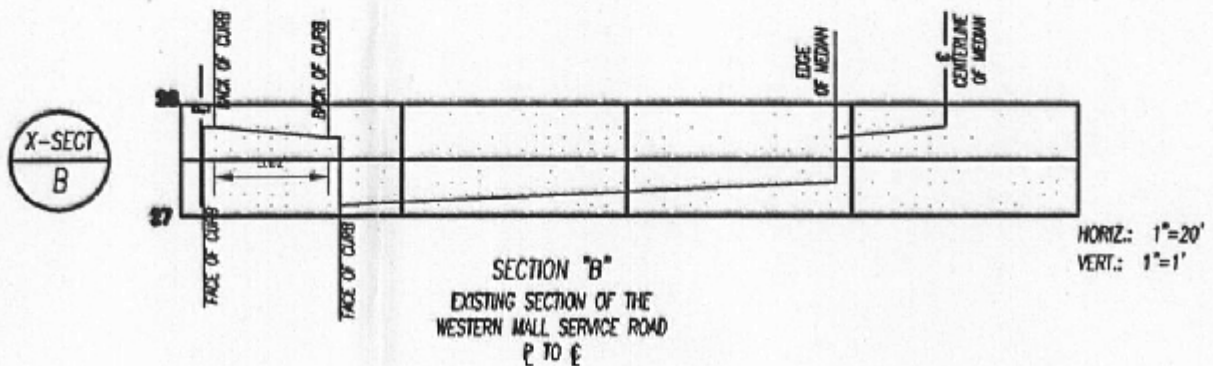
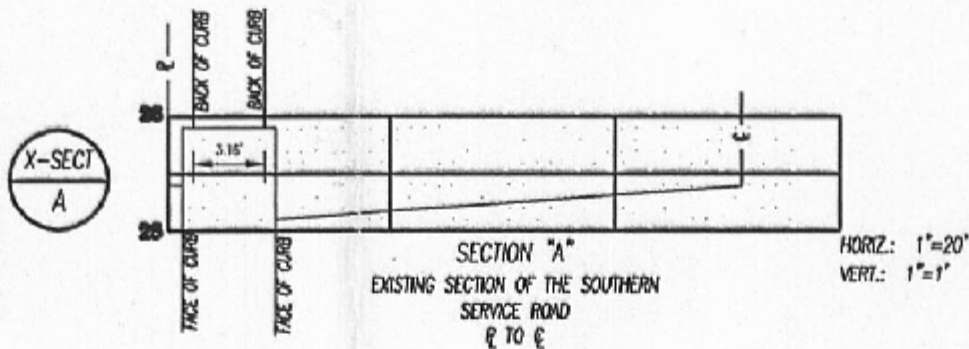
The site plan illustrates the proposed building, parking, green spaces, dumpster pad, and garbage truck access.

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 REQUEST Sidewalk Waiver, Planned Unit Development



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CROSS SECTION DETAIL #1

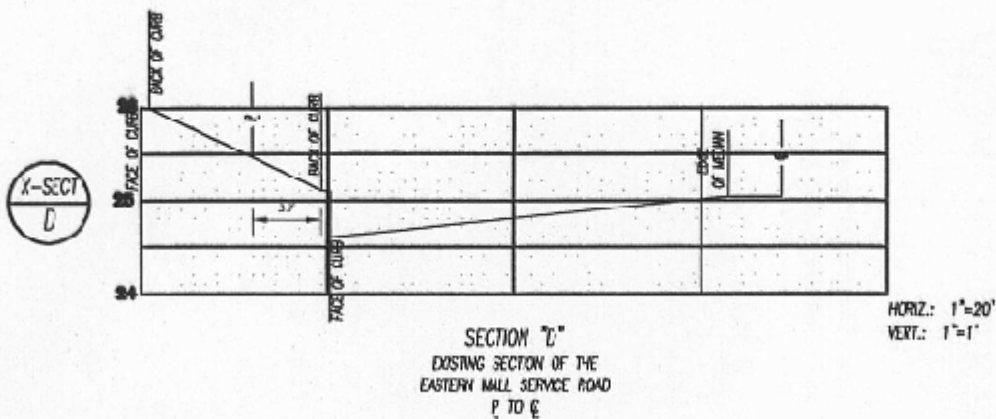
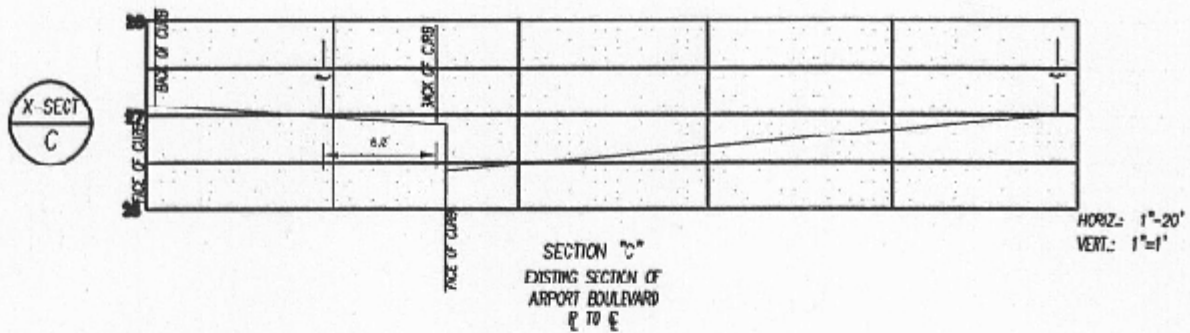


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CROSS SECTION DETAIL #2



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