

**ZONING AMENDMENT, &  
SUBDIVISION STAFF REPORT****Date: February 7, 2008**

<b><u>APPLICANT NAME</u></b>	Blackwood, Inc., et al (Blacksher White-Spunner, Agent)
<b><u>SUBDIVISION NAME</u></b>	Hutson-Key Subdivision, Unit Two, Resubdivision of Lot3
<b><u>LOCATION</u></b>	Northwest corner of West I-65 Service Road South and Pleasant Valley Road, extending to the Northeast corner of Michael Boulevard and Hutson Drive, and extending to the South side of Key Street, 490'± West of West I-65 Service Road South
<b><u>CITY COUNCIL DISTRICT</u></b>	District 5
<b><u>PRESENT ZONING</u></b>	I-1, Light Industry
<b><u>PROPOSED ZONING</u></b>	B-3, Community Business
<b><u>AREA OF PROPERTY</u></b>	3 Lots/16.4± acres
<b><u>CONTEMPLATED USE</u></b>	<p>Subdivision approval to create 2 lots, and Zoning approval, and to rezone the site from I-1, Light Industry to B-3, Community Business to allow a hotel.</p> <p><b>It should be noted, however, that any use permitted in the proposed district would be allowed at this location if the zoning is changed. Furthermore, the Planning Commission may consider zoning classifications other than that sought by the applicant for this property.</b></p>
<b><u>REASON FOR REZONING</u></b>	Applicant is requesting rezoning of the site to allow a hotel.
<b><u>TIME SCHEDULE FOR DEVELOPMENT</u></b>	No schedule provided.
<b><u>ENGINEERING COMMENTS</u></b>	Due to undersized downstream drainage system, provide 100-year on-site storm water detention. It is the responsibility of the applicant to look up the site in the City of Mobile (COM) GIS system and verify if NWI wetlands are depicted on the site. If the COM GIS shows wetlands on the site, it is the responsibility of the applicant to confirm or deny the existence of wetlands on-site. If wetlands are present, they should be depicted on plans and/or plat, and no work/disturbance can be performed without a permit from the Corps of

Engineers. Must comply with all storm water and flood control ordinances. Any work performed in the right of way will require a right of way permit.

## **TRAFFIC ENGINEERING**

### **COMMENTS**

Driveway number, size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards.

## **URBAN FORESTRY**

### **COMMENTS**

Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

## **FIRE DEPARTMENT**

### **COMMENTS**

Subdivisions which include new cul-de-sacs longer than 150 feet must comply with the design requirements of Appendix D of the 2003 International Fire Code, as adopted by the City of Mobile. For new cul-de-sacs longer than 750 feet in length, the preliminary plat must include the location of fire hydrants on the preliminary plat, or must provide an intermediate turn-around that complies with Appendix D.

## **REMARKS**

The applicant is requesting Subdivision approval to create a 2-lot subdivision, and Zoning approval to rezone the site from I-1, Light Industry to B-3, Community Business district to allow a hotel. Hotels and Motels are allowed in B-3 districts by right.

The majority of the site is undeveloped; however, a portion of the site was subdivided and rezoned from I-1 to B-3, Community Business District, in February 2005 and as recently in September 2007. And there have been instances over the last several years whereby industrial zoned properties in the area (or general vicinity) have been rezoned to allow development of uses not allowed in I-1 districts.

The site is bounded to the West by vacant land in an I-1 district, to the North by B-3 uses in an I-1 district, and to the South, by vacant land in an I-1 district. This site is located within the middle of the block entirely zoned I-1; however, along this section of West I-65 Service Road the probability of more of the overall site to further downzone to B-3 may be forthcoming.

As stated in Section 64-9. of the Zoning Ordinance, the intent of the Ordinance and corresponding Zoning Map is to carry out the comprehensive planning objective of sound, stable and desirable development. While changes to the Ordinance are anticipated as the city grows, the established public policy is to amend the ordinance only when one or more of the following conditions prevail: 1) there is a manifest error in the Ordinance; 2) changing conditions in a particular area make a change in the Ordinance necessary and desirable; 3) there is a need to increase the number of sites available to business or industry; or 4) the subdivision of land into building sites makes reclassification of the land necessary and desirable.

The site depicted as commercial on the General Land Use Component of the Comprehensive Plan, which is meant to serve as a general guide, not a detailed lot and district plan or mandate for development. Moreover, the General Land Use Component allows the Planning Commission and City Council to consider individual cases based on additional information such as the classification request, the surrounding development, the timing of the request, and the appropriateness and compatibility of the proposed use and zoning classification.

There are no issues relating to the site plan submitted, the applicant has address all points in determining the design of the extended stay hotel and fully complies with the Zoning Ordinance requirements.

Subdivision review examines the site with regard to promoting orderly development, protecting general health, safety and welfare, and ensuring that development is correlated with adjacent developments and public utilities and services, and to ensure that the subdivision meets the minimum standards set forth in the Subdivision Regulations for lot size, road frontage, lot configuration, etc.

The site was previously approved as a 25-lot subdivision in 2004, with only one lot completed and the Final Plat submitted. Since the preliminary approval had expired, a new application has been submitted, however, this application is only 2-lots. This is not unusual for an undeveloped site of 13.3 acres, whereas, the owner can further subdivide the large lot designed to attract a certain use.

The subdivision site has frontage onto five streets, West I-65 Service Road South (600'±), Pleasant Valley Road (500'±), Michael Boulevard (328'±), Hutson Drive (700'±), and Key Street (340'±). All the streets meet or exceed the right-of-way requirements of the Comprehensive Plan.

Access management is a concern due to the site fronting on five streets; therefore, Lot 3A should be limited to the existing curb cuts, Lot 3B should be limited to one curb-cut, to West I-65 Service Road South and one curb-cut to Pleasant Valley Road, with the size, design and location of all curb-cuts to be approved by Traffic Engineering and conform with AASHTO standards. The limiting of curb cuts to Lot 3A will encourage further subdivision of this 13± acre lot.

If a detention basin and/or common areas are proposed for the subdivision. A note should be placed on the Final Plat, if approved, stating that maintenance of the detention basin common areas, and any other common areas, are the responsibility of the subdivision's property owners.

### **RECOMMENDATION**

**Rezoning:** Based upon the preceding, the Rezoning request is recommended for Approval, subject to the following conditions:

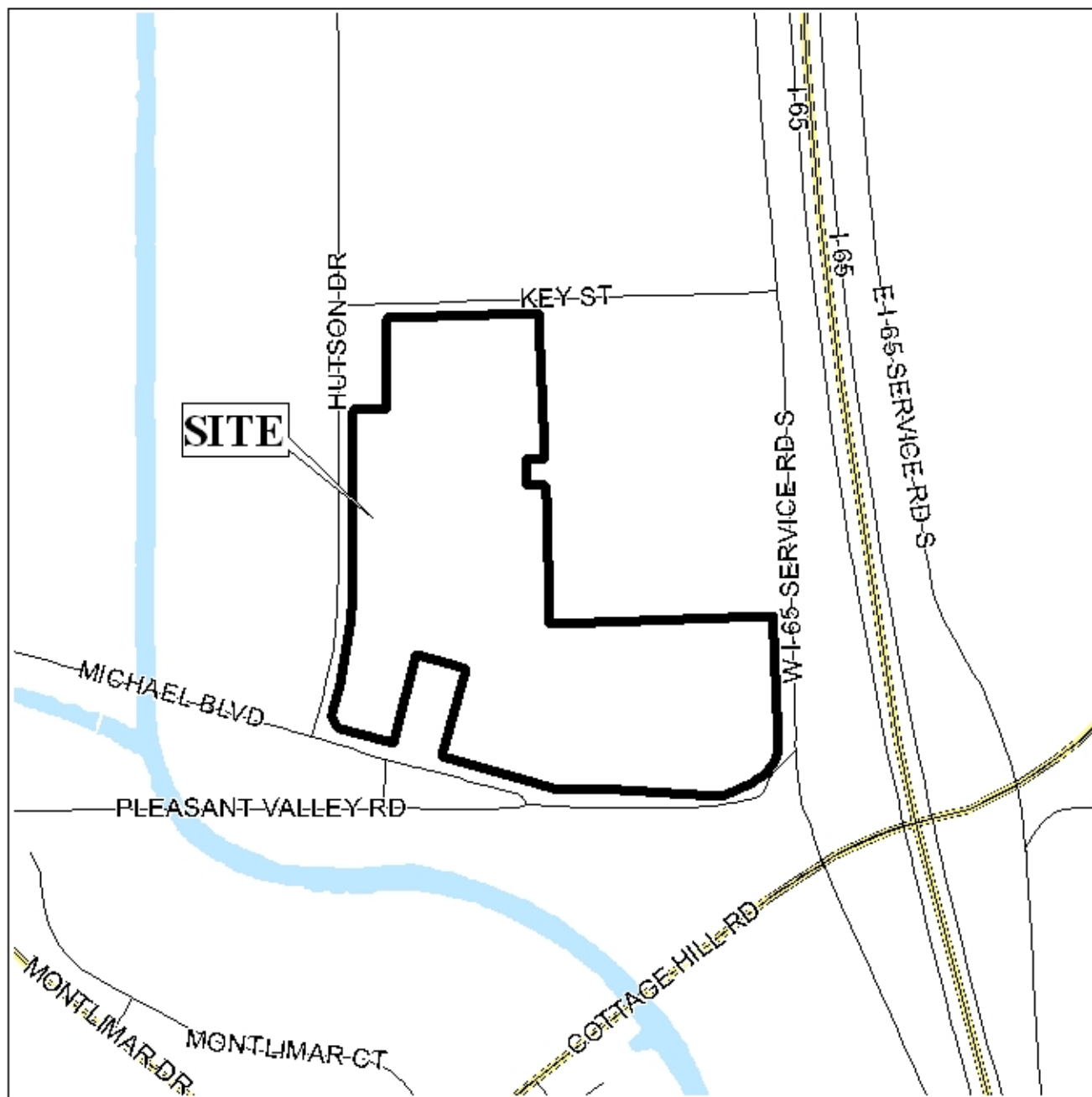
- 1) completion of the Subdivision process;
- 2) placement of a note on the final plat stating that Lot 3A be limited to the existing curb cuts, Lot 3B should be limited to one curb-cut, to West I-65 Service Road South and one curb-cut to Pleasant Valley Road, with the size, design and location of **all** curb-cuts to be approved by Traffic Engineering and conform with AASHTO standards; and

- 3) full compliance with all other municipal codes and ordinances.

**Subdivision:** Based on the preceding, the application is recommended for Tentative Approval, subject to the following conditions:

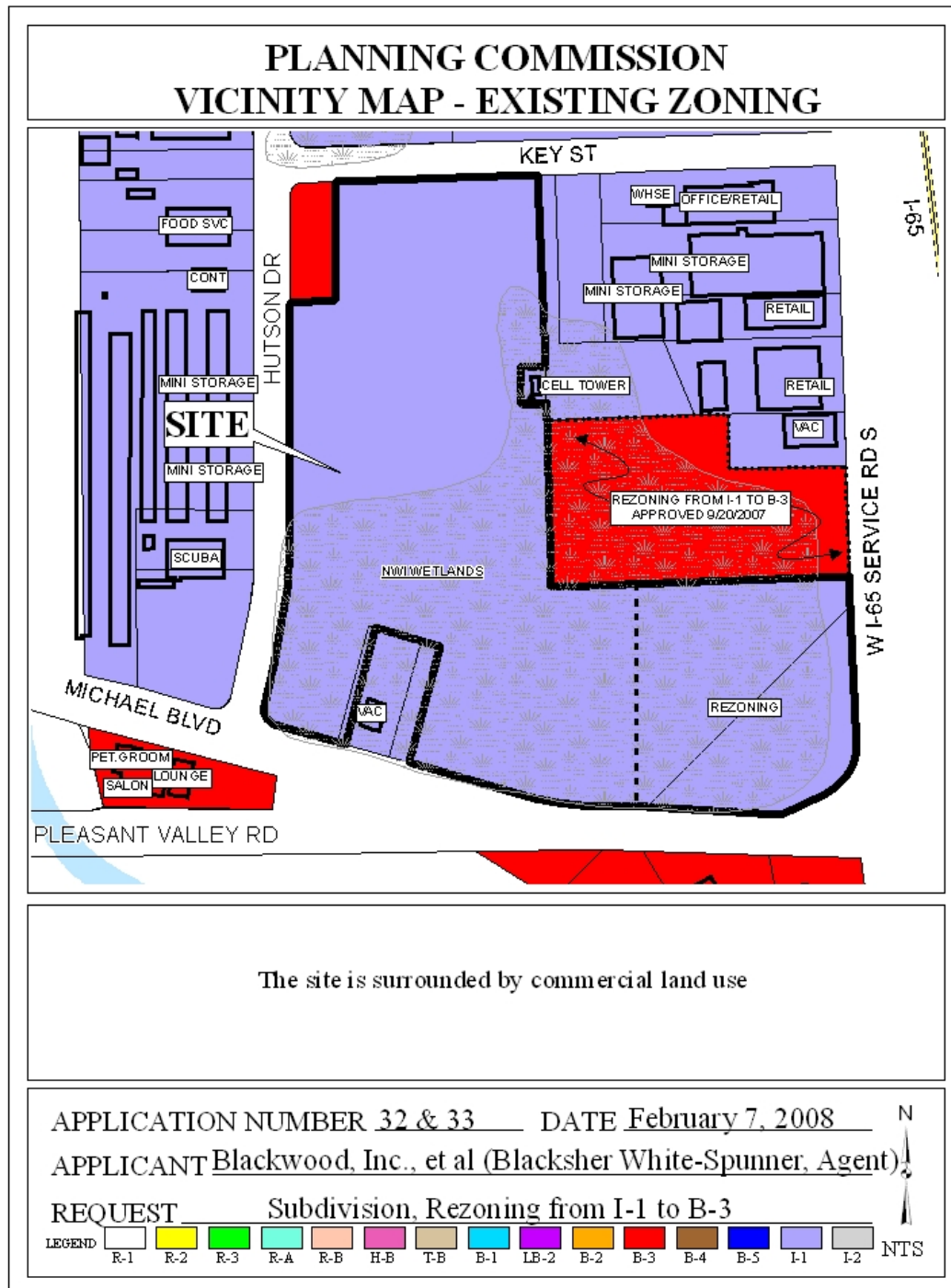
- 1) placement of a note on the final plat stating that Lot 3A be limited to the existing curb cuts, Lot 3B should be limited to one curb-cut, to West I-65 Service Road South and one curb-cut to Pleasant Valley Road, with the size, design and location of **all** curb-cuts to be approved by Traffic Engineering and conform with AASHTO standards.
- 2) the labeling of each lot with its size in square feet, or placement of a table on the plat with the same information;
- 3) compliance with Engineering comments;
- 4) placement of a note on the final plat stating that the maintenance of the detention common area is the responsibility of the property owners; and
- 5) full compliance with all other municipal codes and ordinances.

## LOCATOR MAP



APPLICATION NUMBER 32 & 33 DATE February 7, 2008  
APPLICANT Blackwood, Inc., et al (Blacksher White-Spunner, Agent)  
REQUEST Subdivision, Rezoning from I-1 to B-3

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NTS



**PLANNING COMMISSION  
VICINITY MAP - EXISTING ZONING**

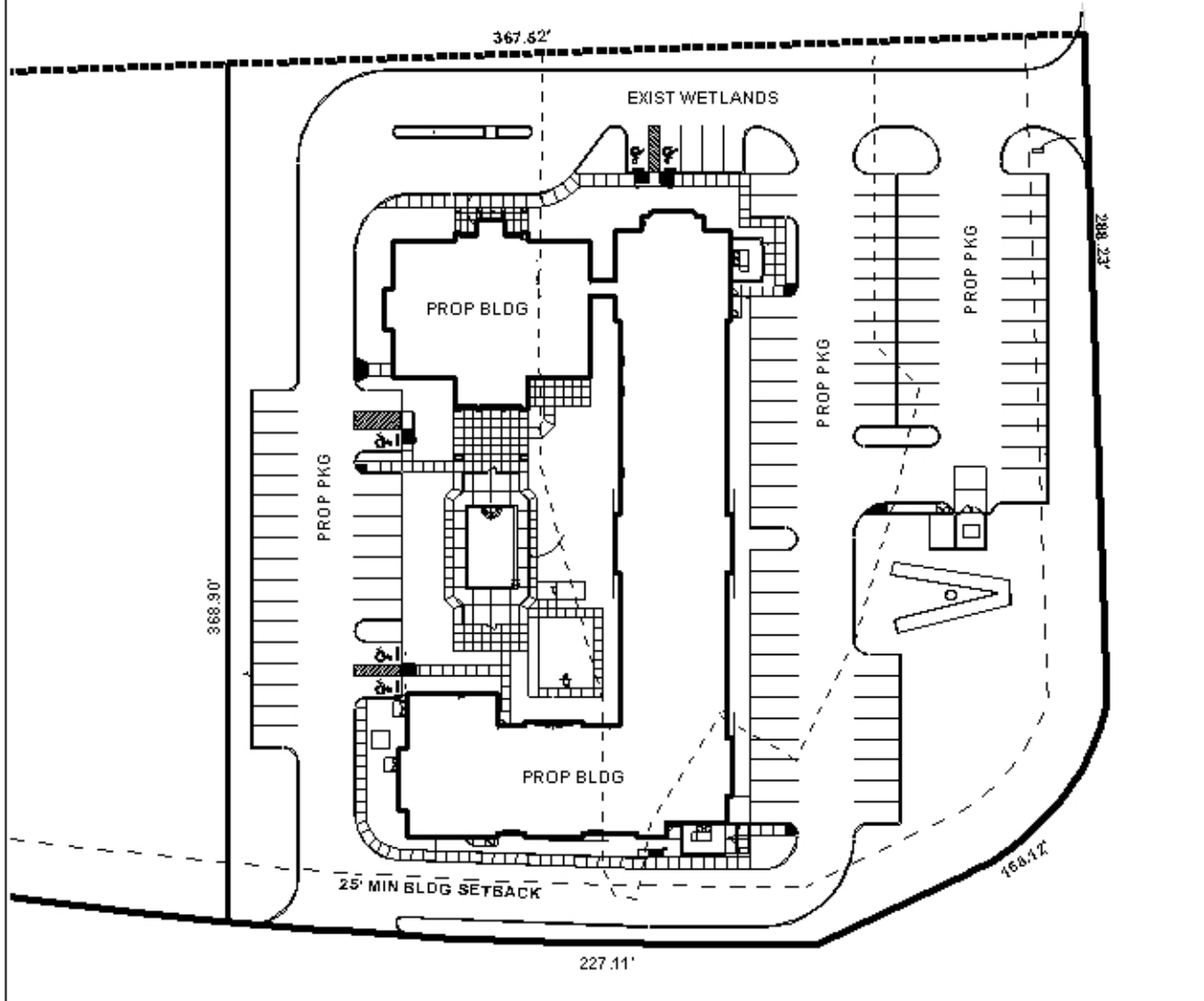


The site is surrounded by commercial land use

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# SITE PLAN



The site plan illustrates the proposed buildings and parking

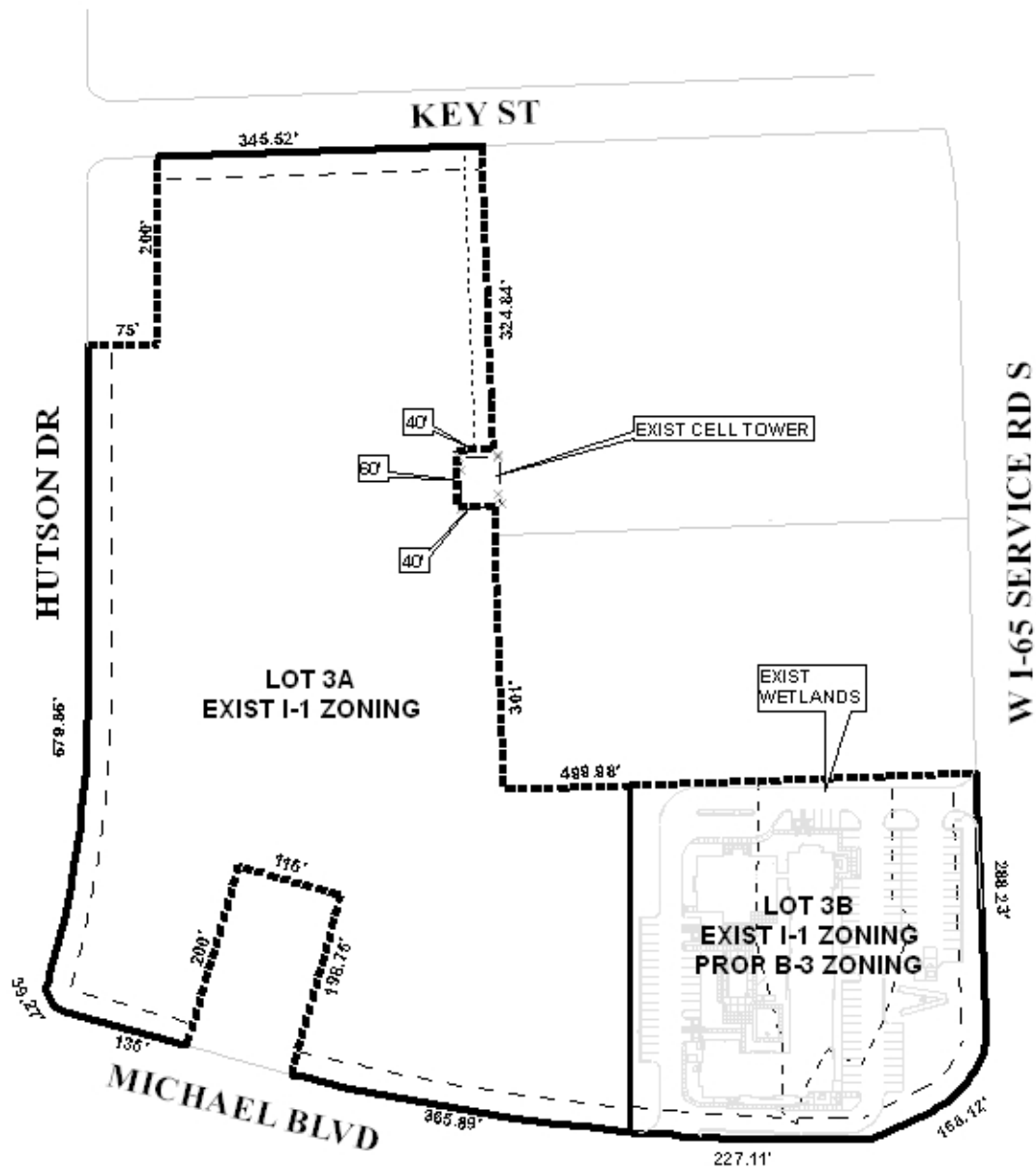
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NTS



## SUBDIVISION AND ZONING DETAIL



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