

**PLANNED UNIT DEVELOPMENT
& SUBDIVISION STAFF REPORT****Date: December 6, 2012****DEVELOPMENT NAME**

West Mobile Boulevard Center Subdivision

SUBDIVISION NAME

West Mobile Boulevard Center Subdivision

LOCATION6575 Airport Boulevard
(South side of Airport Boulevard, 675' ± East of Providence
Hospital Drive)**CITY COUNCIL
DISTRICT**

District 6

AREA OF PROPERTY

6-Lots /19.8± Acres

CONTEMPLATED USE

Planned Unit Development Approval to amend a previously approved Planned Unit Development to allow shared access between multiple building sites and Subdivision approval to create six legal lots of record from three existing legal lots of record.

**TIME SCHEDULE
FOR DEVELOPMENT**

Upon approvals.

**ENGINEERING
COMMENTS**

The following comments concerning the Final Plat should be addressed prior to review, acceptance and signature by the City Engineer:

- 1) Provide all of the required information on the Final Plat (i.e. signature blocks, signatures, certification statements, legal description, required notes).
- 2) Add the County Engineer disclaimer stating that no ROW or easement is accepted for maintenance.
- 3) A signature block and signature for the Traffic Engineering Department shall be placed on the Final Plat.
- 4) Add a note to the Plat stating that storm water detention will be required for any unapproved development (since 1984) and any future addition(s) and/or land disturbing activity in accordance with the Storm Water Management and Flood Control Ordinance (Mobile City Code, Chapter 17 , Ordinance #65-007 & #65-045).

Any work performed in the existing Airport Blvd ROW (right-of-way) such as driveways, sidewalks, utility connections, drainage, irrigation, or landscaping will require a ROW permit from the City of Mobile Engineering Department (208-6070) and must comply with the City of Mobile Right-of-Way Construction and Administration Ordinance (Mobile City Code, Chapter 57, Article VIII). Any and all proposed development will need to be in conformance with the Storm Water Management and Flood Control Ordinance (Mobile City Code, Chapter 17,

Ordinance #65-007 & #65-045); the City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control. According to the City of Mobile's 1984 aerial photographs these parcels were unimproved and wooded; therefore a detention facility(ies) will be required for any land disturbing activity. Need engineer's analysis of the capacity of the receiving storm system to ensure that the receiving system is capable of handling the additional flow. Detention facilities can either be designed for the entire subdivision in which each separate facility must be located within Common Area(s), each individual Lot in which the detention facility must be located within an easement on the Lot, or a combination where an initial facility is constructed within a Common Area to account for the development of the subdivision ROW's and future facility(ies) will be constructed on each individual Lot. It appears that storm water discharge from the site may cross private residential properties. If this is the case care needs to be taken to ensure no damage is caused to downstream properties. The applicant will need to provide a court recorded release agreement from any and all effected downstream properties or each detention facility would need to be designed for the 100 year storm event with a release rate for the 2 year storm. The surface grading for the proposed dumpster pad(s) must be contained and directed to a surface drain that is connected to the Sanitary Sewer system. The drainage from any dumpster pads cannot discharge to storm sewer. An ADEM NOR is required for any land disturbance activity over 1 acre. Provide a copy of the ADEM registration information for the site prior to the issuance of a Land Disturbance permit. Provide additional proposed sidewalk from the ramp on the west side of the west entrance to the existing sidewalk along the service road. unless a sidewalk waiver is approved.

TRAFFIC ENGINEERING

COMMENTS

A Traffic Impact Study has previously been prepared for this site, however, a revised study is required to account for the site changes and how the improvements may be impacted. Although the exact land use for the out parcels is unknown, the study should account for an estimated trip generation based on potential development that could occur.

URBAN FORESTRY

COMMENTS

Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

Preservation status is to be given to the 60" Live Oak Tree located on the East side of Lot 3 and the 60" Live Oak Tree located on the West side of Lot 6. Any work on or under these trees is to be permitted and coordinated with Urban Forestry; removal to be permitted only in the case of disease or impending danger.

FIRE DEPARTMENT

COMMENTS

All projects within the City of Mobile Fire Jurisdiction must comply with the requirements of the 2009 International Fire Code, as adopted by the City of Mobile.

REMARKS

The applicant is seeking Planned Unit Development Approval to amend a previously approved Planned Unit Development to allow shared access between

multiple building sites and Subdivision approval to create six legal lots of record from three existing legal lots of record. The site is located in Council District 6 and according to the applicant is served by public water and sanitary sewer.

Planned Unit Development review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating excess traffic along minor residential streets in residential districts outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD. PUD approval is site plan specific, thus any changes to the site plan / Subdivision plat will require approval by the Planning Commission.

Subdivision review examines the site with regard to promoting orderly development, protecting general health, safety and welfare, and ensuring that development is correlated with adjacent developments and public utilities and services, and to ensure that the subdivision meets the minimum standards set forth in the Subdivision Regulations for lot size, road frontage, lot configuration, etc.

The applicant is submitting this subdivision under Section VIII.C. Innovative Design section of the Subdivision Regulations. Under this section, the Planning Commission may allow a subdivision that illustrates innovative or unconventional design. In this case, the applicant illustrates the site will consist of six lots (Lot 6 is actually common area and should be labeled as such and not as a lot), which the preliminary plat illustrates Lots 3-5 are considered flag shaped lots (Lot 3 [120-feet, Lot 4 [25-feet and Lot 5 [50-feet]); therefore, this development may be considered innovative or unconventional, due to the layout of two frontage lots taking up more than 60-percent of the street frontage along Airport Boulevard.

The proposed lots 1-3, as depicted, meet the minimum size and frontage requirements as regulated by the Subdivision Regulations. However, lots 4 & 5 do not provide the minimum 60-feet of road frontage on a public street due to the number of lots and the design of the PUD. Therefore, to allow these lots as illustrated on the preliminary plat as flag shaped lots, a waiver of Section V.D.1. of the Subdivision Regulations would be required. The 25-foot minimum building setback lines are depicted; however, they should be retained on the Final Plat, if approved. The lot area sizes in square feet/acres are depicted on the preliminary plat and should be retained on the Final Plat, if approved.

It should be noted that this site has been the subject of two previously approved subdivisions and Planned Unit Development applications. The present applications simply subdivide the existing Lot 1, West Airport Boulevard Center Subdivision into three lots and a sizable detention/retention/common area and illustrate lots 4 and 5 of the current proposal to allow these lots to be flag shaped lots.

Conditions of the previous approvals would also be conditions of these approvals are:

- 1) intersection improvement (including signalization) and median modifications, as recommended by Traffic Engineering, to be completed prior to the issuance of any Certificates of Occupancy;
- 2) completion of the subdivision process;
- 3) provision of an 8-foot privacy fence along the south and East property lines, where the site abuts residential properties;
- 4) the provision of a 10-foot buffer planting strip, or an 8-foot wooden privacy fence beginning at the southwest corner and extending northwardly along the west property line for approximately 270-feet where the site adjoins the residential facility to the west; and
- 5) full compliance with all municipal codes and ordinances.

The site is located on Airport Boulevard, a major street on the city's Major Street Plan; therefore, access to Airport Boulevard is a concern. The plan as submitted indicates two access points to Airport Boulevard, one of which would require a new median cut and signalization. A second access point to the site is along the west side of the site. As previously mentioned, the site plan illustrates the same number and size of the previously approved subdivision and PUD with the only change in the addition in the number of lots and the design of Lots 4 and 5.

In a meeting with the developer, attended by representatives of Urban Development, Engineering and Traffic Engineering, it was determined that the Traffic Impact Study submitted with the approved West Airport Boulevard Center Subdivision in 2007 would stand for the development of the proposed Lots 1-3; however, prior to the development of Lots 4 and 5, the Traffic Impact Study must be updated to allow for any changes to traffic movement onto the site. Comments from the Traffic Engineering Department also state that any changes to Airport Boulevard are subject to approval by Traffic Engineering.

It should be noted that the subdivision name West Airport Boulevard Center Subdivision should be changed as a subdivision with this name already exists.

A city standard sidewalk is indicated along the Airport Boulevard and would be required in compliance with the developmental standards of Engineering. In addition, the site plan does indicate a dumpster and contains a note stating the proposed buffering. However, the site plan should be revised to indicate the dumpster is in compliance with Section 64-4.D.9. of the Zoning Ordinance, with a compliant sanitary sewer connection.

Parking spaces provided exceed the required ratio by approximately 47 percent for the proposed development of Lot 3, which is located in a B-3, Community Business District, and access ways and maneuvering areas comply with the requirements of the Ordinance.

The plan does illustrate one common area, of approximately 3.65± acres noted as Lot 6 on the preliminary plat, if approved, the area should be labeled as retention/detention/common area (not Lot 6) on the Final Plat and should include a note stating that maintenance of the common area is the responsibility of the property owner(s).

The site abuts R-1 zoned property to the South; therefore, the provision of a 10-foot buffer strip or an 8-foot privacy fence should be required where the site abuts residentially zoned property as per Section 64-4 of the Zoning Ordinance.

The site plan does not illustrate the location or number of the tree planting requirements of the Ordinance. Therefore, a condition of approval should be that the development comply with the landscaping and tree planting requirements of the Ordinance.

The geographic area defined by the city of Mobile and its planning jurisdiction, including this site, may contain Federally-listed threatened or endangered species as well as protected non-game species. Development of the site must be undertaken in compliance with all local, state and Federal regulations regarding endangered, threatened or otherwise protected species.

RECOMMENDATION

Subdivision: The request is recommended for tentative approval, subject to the following conditions:

- 1) retention of the 25-foot minimum building setback along Airport Boulevard on the Final Plat;
- 2) retain on the Final Plat of labeling each lot with its size in square feet and acres, or the furnishing of a table on the Final Plat providing the same information;
- 3) placement of a note on the Final Plat stating that the development is limited to two curb cuts to Airport Boulevard (existing curb cut and private street access point), with the size, design, and exact location to be approved by Traffic Engineering and conform to AASHTO standards, any changes to Airport Boulevard are subject to approval by Traffic Engineering and any intersection improvement (including signalization) and median modifications, as recommended by Traffic Engineering, to be completed prior to the issuance of any Certificates of Occupancy;
- 4) relabeling of Lot 6 on the Final Plat as detention/retention/common area and the placement of a note on the Final Plat stating that the maintenance of the common area is the responsibility of the property owners;
- 5) provision of a 8-foot privacy fence along the south and East property lines, where the site abuts residential properties;
- 6) the provision of a 10-foot buffer planting strip, or an 8-foot wooden privacy fence beginning at the southwest corner and extending northwardly along the west property line for approximately 270-feet where the site adjoins the residential facility to the west;
- 7) placement of a note on the Final Plat stating that development of the site must be undertaken in compliance with all local, state and Federal regulations regarding endangered, threatened or otherwise protected species;
- 8) compliance with Engineering comments: *(The following comments concerning the Final Plat should be addressed prior to review, acceptance and signature by the City Engineer: 1) provide all of the required information on the Final Plat (i.e. signature blocks, signatures, certification statements, legal description, required notes). 2) add the County Engineer disclaimer stating that no ROW or easement is accepted for maintenance; 3) a signature block and signature for the Traffic Engineering Department shall be placed on the Final Plat; and 4) add a note to the Plat stating that storm water detention will be required for any unapproved development (since 1984) and any future addition(s) and/or land disturbing activity in accordance with the Storm Water Management and Flood Control Ordinance (Mobile City Code, Chapter 17 , Ordinance #65-007 & #65-045). Any work performed in the existing Airport Blvd ROW (right-of-way) such as driveways, sidewalks, utility connections, drainage, irrigation, or landscaping will require a ROW permit from the City of Mobile Engineering Department (208-6070) and must comply*

with the City of Mobile Right-of-Way Construction and Administration Ordinance (Mobile City Code, Chapter 57, Article VIII). Any and all proposed development will need to be in conformance with the Storm Water Management and Flood Control Ordinance (Mobile City Code, Chapter 17, Ordinance #65-007 & #65-045); the City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control. According to the City of Mobile's 1984 aerial photographs these parcels were unimproved and wooded; therefore a detention facility(ies) will be required for any land disturbing activity. Need engineer's analysis of the capacity of the receiving storm system to ensure that the receiving system is capable of handling the additional flow. Detention facilities can either be designed for the entire subdivision in which each separate facility must be located within Common Area(s), each individual Lot in which the detention facility must be located within an easement on the Lot, or a combination where an initial facility is constructed within a Common Area to account for the development of the subdivision ROW's and future facility(ies) will be constructed on each individual Lot. It appears that storm water discharge from the site may cross private residential properties. If this is the case care needs to be taken to ensure no damage is caused to downstream properties. The applicant will need to provide a court recorded release agreement from any and all effected downstream properties or each detention facility would need to be designed for the 100 year storm event with a release rate for the 2 year storm. The surface grading for the proposed dumpster pad(s) must be contained and directed to a surface drain that is connected to the Sanitary Sewer system. The drainage from any dumpster pads cannot discharge to storm sewer. An ADEM NOR is required for any land disturbance activity over 1 acre. Provide a copy of the ADEM registration information for the site prior to the issuance of a Land Disturbance permit. Provide additional proposed sidewalk from the ramp on the west side of the west entrance to the existing sidewalk along the service road. unless a sidewalk waiver is approved.);

- 9) compliance with Traffic Engineering comments: (A Traffic Impact Study has previously been prepared for this site, however, a revised study is required to account for the site changes and how the improvements may be impacted. Although the exact land use for the out parcels is unknown, the study should account for an estimated trip generation based on potential development that could occur.);*
- 10) compliance with Fire Department comments: (All projects within the City of Mobile Fire Jurisdiction must comply with the requirements of the 2009 International Fire Code, as adopted by the City of Mobile);*
- 11) compliance with Urban Forestry comments: (Preservation status is to be given to the 60" Live Oak Tree located on the East side of Lot 3 and the 60" Live Oak Tree located on the West side of Lot 6. Any work on or under these trees is to be permitted and coordinated with Urban Forestry; removal to be permitted only in the case of disease or impending danger.);*
- 12) the placement of a note on the Final Plat stating the provision of a 10-foot buffer and six-foot high solid privacy fence adjacent to residentially zoned properties; and*
- 13) submittal of two copies of a revised PUD site plan prior to signing the Final Plat.*

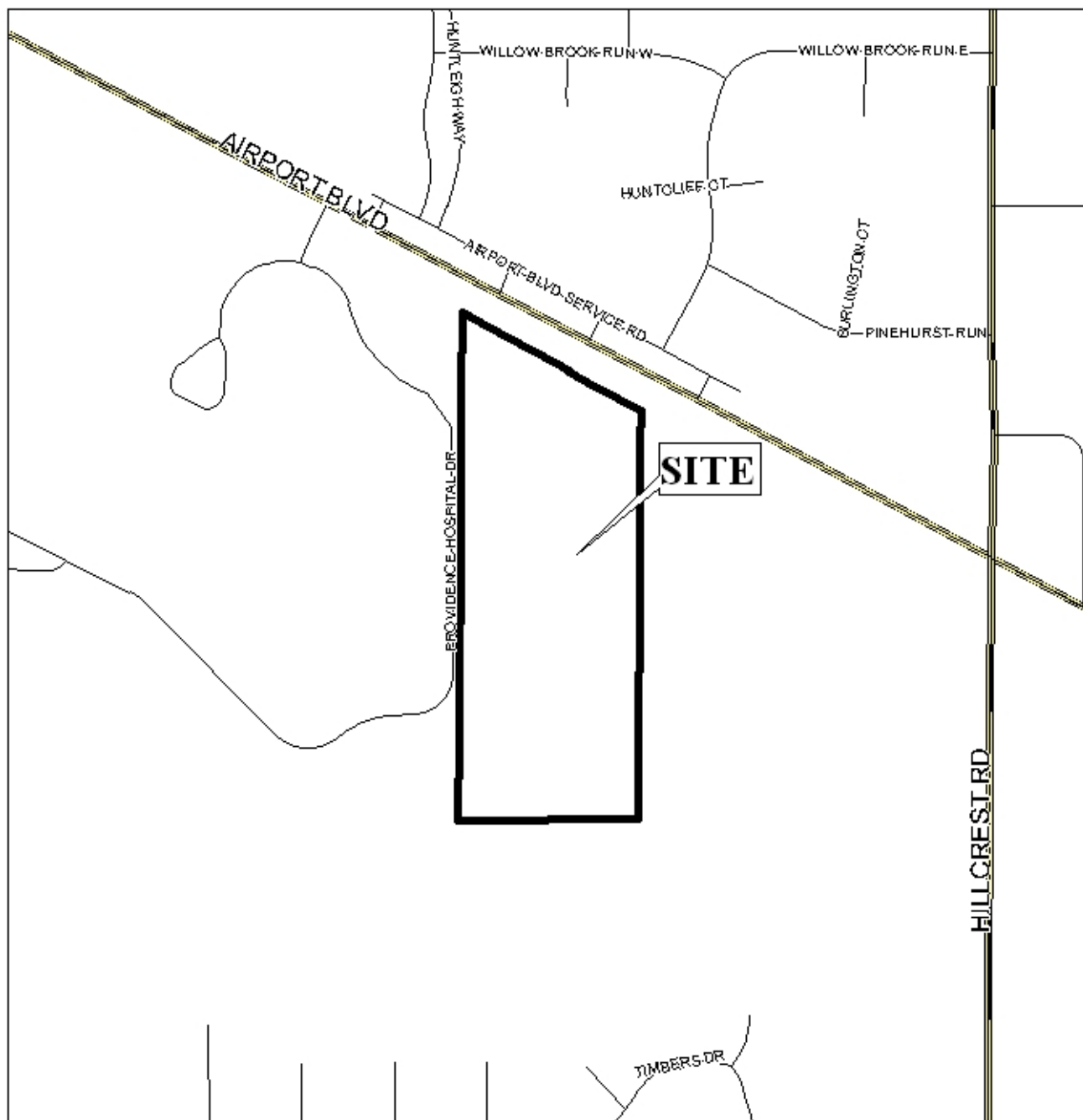
Planned Unit Development: Based upon the preceding, this application is recommended for approval subject to the following conditions:

- 1) retention of the 25-feet minimum building setback along Airport Boulevard on the PUD site plan;
- 2) retain on the PUD site plan of labeling each lot with its size in square feet and acres, or the furnishing of a table on the Final Plat providing the same information;
- 3) placement of a note on the PUD site plan stating that the development is limited to two curb cuts to Airport Boulevard (existing curb cut and private street access point), with the size, design, and exact location to be approved by Traffic Engineering and conform to AASHTO standards, any changes to Airport Boulevard are subject to approval by Traffic Engineering and any intersection improvement (including signalization) and median modifications, as recommended by Traffic Engineering, to be completed prior to the issuance of any Certificates of Occupancy;
- 4) relabeling of Lot 6 on the PUD site plan as detention/retention/common area and the placement of a note on the Final Plat stating that the maintenance of the common area is the responsibility of the property owners;
- 5) provision of a 8-feet privacy fence along the south and East property lines, where the site abuts residential properties;
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- 13) *submittal of two copies of a revised PUD site plan prior to signing the Final Plat; and*
- 14) *compliance with all other codes and ordinances.*

LOCATOR MAP



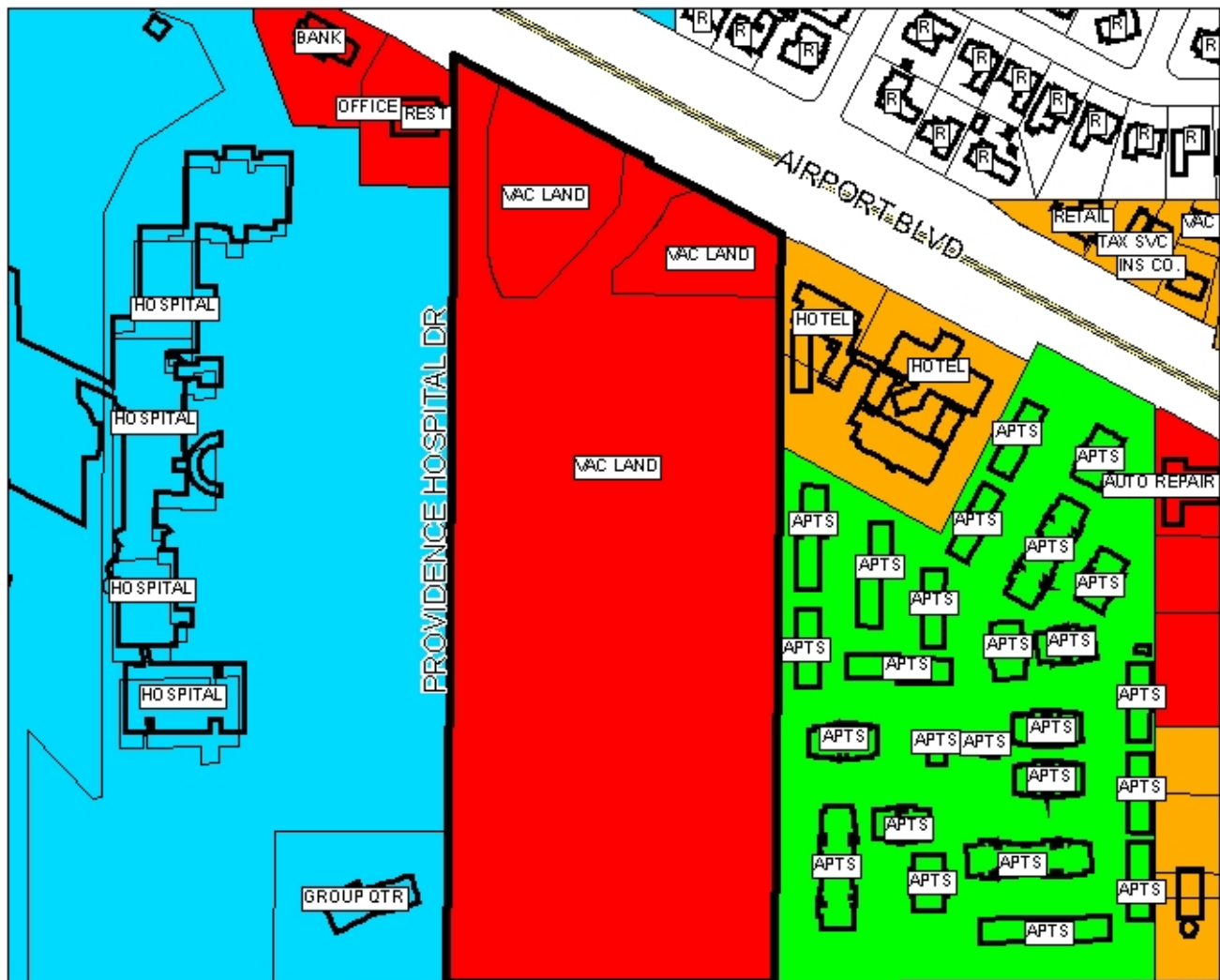
APPLICATION NUMBER 30 & 31 DATE December 6, 2012

APPLICANT West Airport Boulevard Center Subdivision

REQUEST Subdivision, Planned Unit Development



PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



A hospital is located to the west of the site. A hotel and apartment complex are located to the east.

APPLICATION NUMBER 30 & 31 DATE December 6, 2012

APPLICANT West Airport Boulevard Center Subdivision

REQUEST Subdivision, Planned Unit Development



NTS

PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



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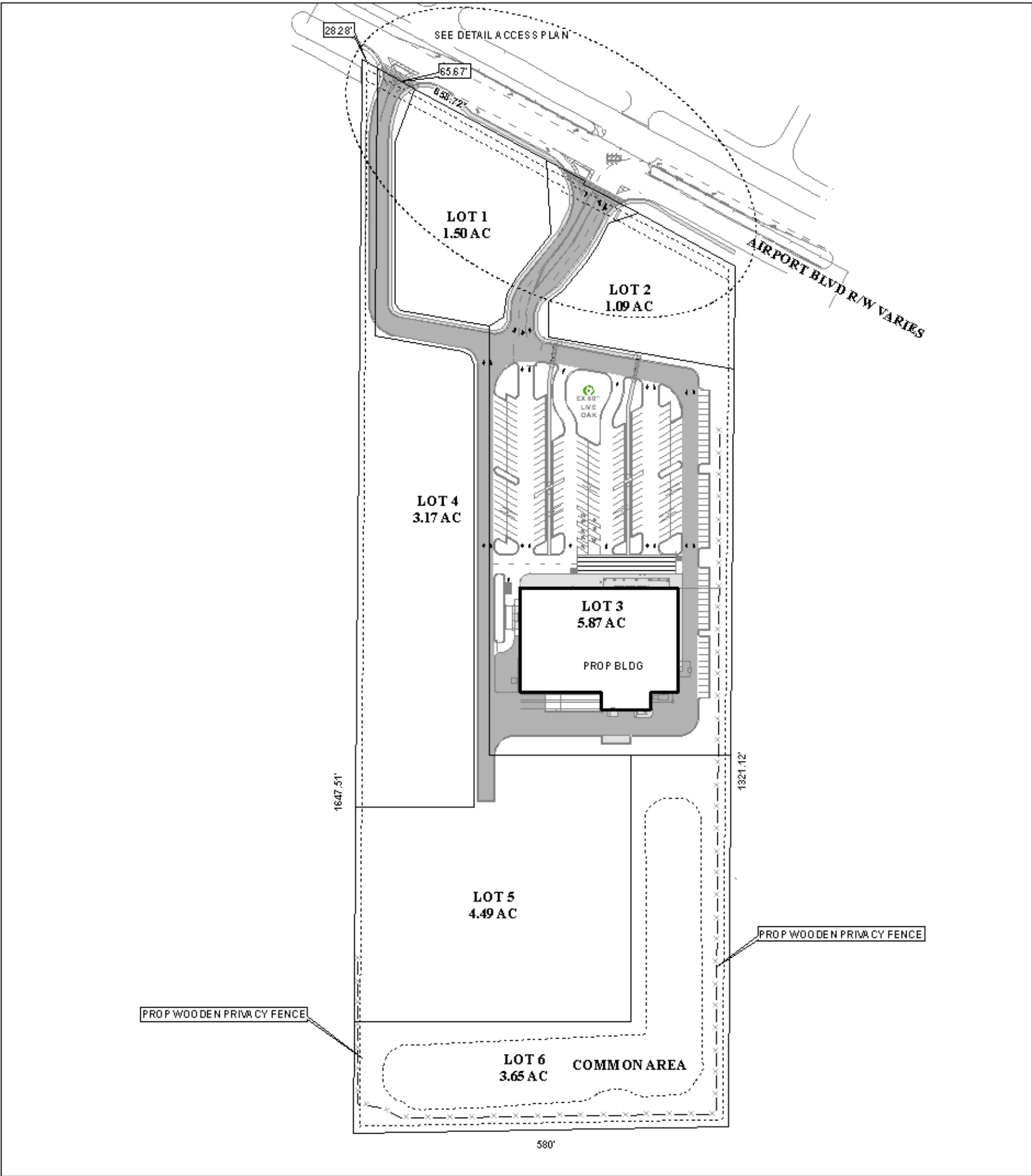
APPLICANT West Airport Boulevard Center Subdivision

REQUEST Subdivision, Planned Unit Development



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SITE PLAN

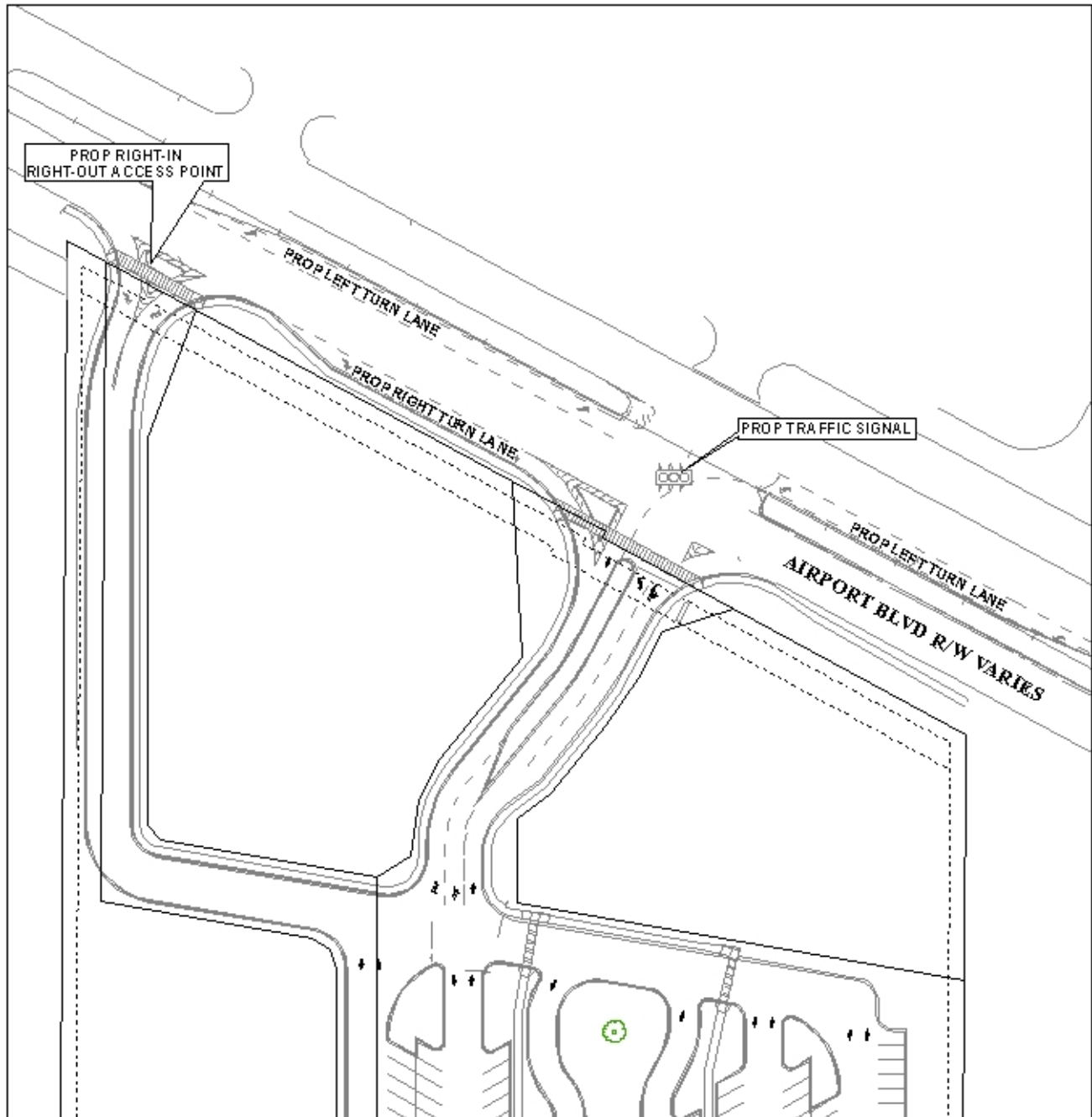


The site plan illustrates the proposed development.

APPLICATION NUMBER	30 & 31	DATE	December 6, 2012
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REQUEST	Subdivision, Planned Unit Development		

NTS

DETAIL ACCESS PLAN



APPLICATION NUMBER 30 & 31 DATE December 6, 2012

APPLICANT West Airport Boulevard Center Subdivision

REQUEST Subdivision, Planned Unit Development

