

**PLANNED UNIT DEVELOPMENT &
PLANNING APPROVAL STAFF REPORT****Date: August 7, 2008****NAME**

Springhill Presbyterian Church

LOCATION10 Westminster Way
(Southeast corner of Old Shell Road and Westminster Way,
extending through to Bit and Spur Road)**CITY COUNCIL
DISTRICT**

District 5

PRESENT ZONING

R-1, Single-Family Residence District

AREA OF PROPERTY

1 Lot / 8.4 ± Acres

CONTEMPLATED USE

Planning Approval to allow the expansion of an existing church in an R-1, Single- Family Residential District, to include a new chapel building, and Planned Unit Development Approval to allow multiple buildings on a single building site.

**TIME SCHEDULE
FOR DEVELOPMENT**

No timeframe provided.

ENGINEERING**COMMENTS**

Detention must be provided for any impervious areas constructed in excess of 4,000 square feet since 1984. Must comply with all storm water and flood control ordinances. Any work performed in the right of way will require a right of way permit. Need to verify that there is sufficient capacity for the receiving drainage system(s) to accept drainage from this property.

TRAFFIC ENGINEERING**COMMENTS**

Driveway number, size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards. Final approval for all driveways, proposed and existing will be given upon submittal of final plans.

URBAN FORESTRY**COMMENTS**

Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64). Removal of any live oak tree 24-inches or larger in diameter will require review and approval by Urban Forestry. Preservation status granted for the 64-inch red oak located within the project site.

FIRE DEPARTMENT**COMMENTS**

All projects must comply with the requirements of the 2003 International Fire Code, including Appendices B through D, as adopted by the City of Mobile, and the 2003 International Existing Building Code, as appropriate.

REMARKS

The applicant is requesting Planning Approval to allow the expansion of an existing church in an R-1, Single-Family Residential District, to include a new chapel building, and Planned Unit Development Approval to allow multiple buildings on a single building site. Planning Approval is required for churches in R-1 districts.

The review required for Planning Approval examines the applicant's location and site plan with regard to transportation, parking and access, public utilities and facilities, traffic congestion and hazard, and to determine if the proposal is in harmony with the orderly and appropriate development of the district.

Planned Unit Development review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating excess traffic along minor residential streets in residential districts outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD. PUD approval is site plan specific, thus if any new construction is anticipated that will change an approved site plan, an application to amend an existing, approved PUD must be made prior to any construction activities.

It is very important to note that the Planning Approval and PUD review are site plan specific; therefore *any* future changes to the overall site plan must be submitted for PUD and Planning Approval review.

The applicant proposes to expand their existing church facilities through the addition of a 150-seat chapel. The site is currently developed with a 360-seat sanctuary, a classroom building and a multi-purpose building. The site was the subject of a previous Planning Approval review at the April 7, 1988 meeting of the Planning Commission, where the addition of the multi-purpose building was approved.

The site is a legal lot of record, and has frontage onto three streets: Old Shell Road, Westminster Way, and Bit and Spur Road. Westminster Way is a proposed major street, while the others are minor streets. All streets have adequate right-of-way.

The parking requirements for churches are based upon the number of seats in the sanctuary / worship area. With 360 existing seats and 150 proposed seats, a total of 128 parking spaces are required. The site has 181 existing parking spaces, which meets the minimum requirements, and no additional parking is proposed. It should be pointed out that the drawing has errors in the number of spaces labeled in some of the parking areas (applicant claims 183 spaces), thus the site plan should be revised to accurately reflect the actual number of parking spaces.

The chapel will be located between two existing parking areas, and the site plan indicates that an existing 64-inch “water oak” will be removed. Staff has verified that the tree is in fact a specimen-quality red oak. The site plan should be revised to correctly identify all existing trees and their sizes. Adequate room exists on the project site to move the proposed chapel approximately 7 feet to accommodate the existing red oak, and Urban Forestry has recommended preservation status for the red oak tree.

The site plan also lists information regarding compliance with the tree and landscaping requirements of the Zoning Ordinance, yet the site plan does not depict actual compliance with the tree requirements: no new trees are depicted. Staff recommends that the site be brought into full compliance with the frontage tree requirements for the entire site: Old Shell Road, Westminster Way and Bit and Spur Road frontages.

Staff would also like to point out that there are no sidewalks existing along Bit and Spur Road or on the church side of Westminster Way. Staff has regularly observed “recreational” pedestrians walking along this segment of Bit and Spur, and it is therefore recommended that sidewalks be provided along the Bit and Spur frontage and that portion of Westminster Way, from Bit and Spur up to the entrance into the church parking area.

It should also be pointed out that new sidewalks proposed to serve the new chapel do not effectively serve the immediately adjacent parking areas. The sidewalks should parallel the abutting parking areas and provide direct access for these parking areas, so that church members or others using the chapel will not have to walk on designated landscape areas or between vehicles to reach sidewalks. The site plan should be revised to show improved pedestrian circulation.

No dumpster or other similar facility is depicted on the site plan. A note should be placed on the site plan stating that no dumpster will be used, or a dumpster facility should be depicted and labeled.

The site abuts a residential development to the East. The site plan should be revised to depict and provide a 10-foot wide buffer and a 6-foot high privacy fence (if there is no existing fence) along that portion of the boundary from Bit and Spur Road to the parking area where it meets the Eastern boundary.

As the chapel and the previously construct multi-purpose building were built after 1984, some level of compliance with the storm water and flood control regulations will be necessary. If a surface detention area will be necessary, the site plan should be revised to depict its general location.

Any one-way drives or parking areas should be so marked on the site plan, and should be reflected on the site with pavement markings and/or signage.

RECOMMENDATION

Planned Unit Development: Based upon the preceding, the application is recommended for Holdover until the September 4 meeting, with revisions due by August 19, so that the following can be undertaken:

- 1) Revision of the site plan to move the proposed chapel approximately 7 feet further way from the existing 64-inch red oak tree so that the tree can be retained;
- 2) Revision of the site plan to reflect compliance with revised Engineering comments, including the depiction of the general location of any surface detention areas that will be provided (*Detention must be provided for any impervious areas constructed in excess of 4,000 square feet since 1984. Must comply with all storm water and flood control ordinances. Any work performed in the right of way will require a right of way permit. Need to verify that there is sufficient capacity for the receiving drainage system(s) to accept drainage from this property.*)
- 3) Revision of the site plan to correctly number the quantity of parking spaces in each parking area;
- 4) Correct identification of all existing trees depicted on the site plan and their size;
- 5) Revision of the site plan to depict full compliance with the frontage tree requirements of the Zoning Ordinance for the entire site;
- 6) Revision of the site plan to show sidewalks along Bit and Spur Road, and along that portion of Westminster Way from Bit and Spur north to the first entrance into the church parking area;
- 7) Revision of the site plan to show sidewalks adjacent to all parking areas abutting the chapel and leading to parking access roads;
- 8) Depiction of curbing and/or bumper stops for all parking areas abutting the chapel site in order to protect adjacent landscape and sidewalk areas;
- 9) Revision of the site plan to depict the one-way circulation drives, to include the marking of the drives with arrows and/or signage to ensure correct circulation;
- 10) Revision of the site plan to depict and label a 10-foot wide buffer along the entire Eastern boundary, and a vegetative buffer or a 6-foot high privacy fence from Bit and Spur Road along the Eastern boundary of the site to the parking area where it meets the Eastern boundary; and
- 11) Placement of a note on the site plan stating that any changes to the site plan will require new applications for Planning Approval and Planned Unit Development approval prior to the issuance of any permits.

Planning Approval: Based upon the preceding, the application is recommended for Holdover until the September 4 meeting, with revisions due by August 19, so that the following can be undertaken:

- 1) Revision of the site plan to move the proposed chapel approximately 7 feet further way from the existing 64-inch red oak tree so that the tree can be retained;
- 2) Revision of the site plan to reflect compliance with revised Engineering comments, including the depiction of the general location of any surface detention areas that will be provided (*Detention must be provided for any impervious areas constructed in excess of 4,000 square feet since 1984. Must comply with all storm water and flood control ordinances. Any work performed in the right of way will require a right of way permit. Need to verify that there is sufficient capacity for the receiving drainage system(s) to accept drainage from this property.*)

- 3) Revision of the site plan to correctly number the quantity of parking spaces in each parking area;
- 4) Correct identification of all existing trees depicted on the site plan and their size;
- 5) Revision of the site plan to depict full compliance with the frontage tree requirements of the Zoning Ordinance for the entire site;
- 6) Revision of the site plan to show sidewalks along Bit and Spur Road, and along that portion of Westminster Way from Bit and Spur north to the first entrance into the church parking area;
- 7) Revision of the site plan to show sidewalks adjacent to all parking areas abutting the chapel and leading to parking access roads;
- 8) Depiction of curbing and/or bumper stops for all parking areas abutting the chapel site in order to protect adjacent landscape and sidewalk areas;
- 9) Revision of the site plan to depict the one-way circulation drives, to include the marking of the drives with arrows and/or signage to ensure correct circulation;
- 10) Revision of the site plan to depict and label a 10-foot wide buffer along the entire Eastern boundary, and a vegetative buffer or a 6-foot high privacy fence from Bit and Spur Road along the Eastern boundary of the site to the parking area where it meets the Eastern boundary; and
- 11) Placement of a note on the site plan stating that any changes to the site plan will require new applications for Planning Approval and Planned Unit Development approval prior to the issuance of any permits.

Revised for the September 4th meeting:

Staff met with the applicant's representative regarding the project, and revised site plans were submitted based upon the meeting and the conditions listed above.

The applicant states that their intention is to keep the existing 64-inch red oak tree, and it appears that the proposed chapel will be approximately 28-feet from the trunk. Tree protection barriers at least 10-feet from the trunk of the tree, as well as any other tree that will be retained, should be depicted on any plans submitted for building permits, and should be in place prior to any land disturbance activities. Preservation status should be granted for any tree with a 50-inch or greater diameter breast height on the site.

Two new frontage trees are proposed along Bit and Spur Road, and two are proposed along Westminster Way. No additional frontage trees should be required other than as proposed due to existing trees either within the frontage area, or with large canopies that would interfere with the growth of new trees planted in the frontage areas.

The applicant will apply for a sidewalk waiver along Bit and Spur Road, but will provide a sidewalk along Westminster Way, adjacent to the project area. The sidewalk waiver application should be submitted prior to application for land disturbance or building permits.

The parking area was revised to provide bumper stops for those spaces lacking curbing, and to indicate directional arrows for circulation.

Finally, the applicant stated that additional sidewalks serving the existing parking area would be unnecessary, as the proposed sidewalks as designed would be adequate for the proposed chapel.

RECOMMENDATION

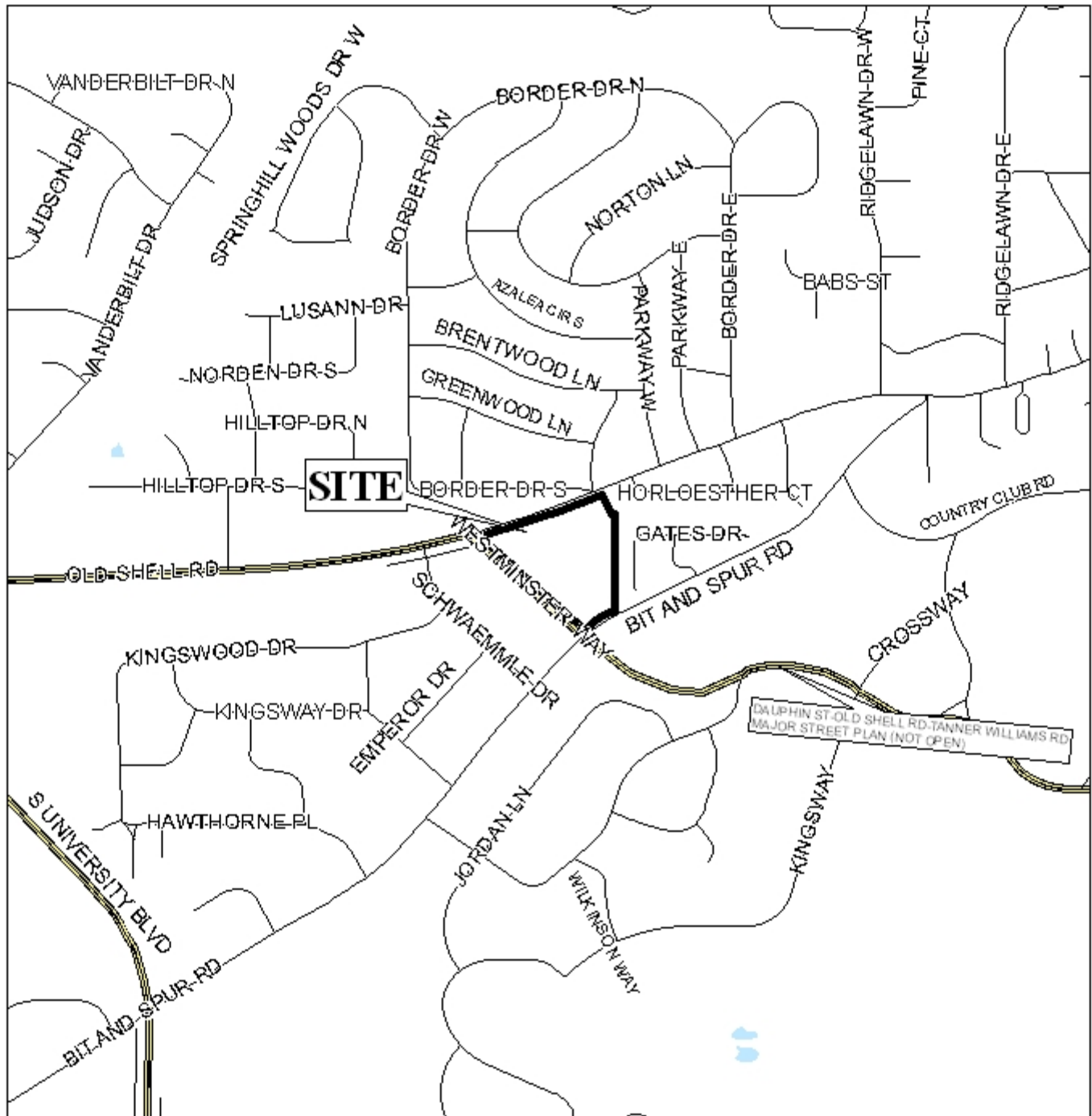
Planned Unit Development: *Based upon the revised site plan, the application is recommended for Approval, subject to the following conditions:*

- 1) *Compliance with Engineering comments (Detention must be provided for any impervious areas constructed in excess of 4,000 square feet since 1984. Must comply with all storm water and flood control ordinances. Any work performed in the right of way will require a right of way permit. Need to verify that there is sufficient capacity for the receiving drainage system(s) to accept drainage from this property.)*
- 2) *Preservation status granted to any tree with a DBH of 50-inches or larger ;*
- 3) *Compliance with Urban Forestry comments (Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64). Removal of any live oak tree 24-inches or larger in diameter will require review and approval by Urban Forestry.)*
- 4) *Provision of two new frontage trees each along Bit and Spur Road and Westminster Way, as depicted;*
- 5) *Submission of an application for a sidewalk waiver prior to submittal for land disturbance or building permits; and*
- 6) *Provision of tree protection barriers at least 10-feet from the trunk of the tree, as well as any other tree that will be retained (depicted on any plans submitted for land disturbance or building permits), to be in place prior to any land disturbance activities.*

Planning Approval: *Based upon the revised site plan, the application is recommended for Approval, subject to the following conditions:*

- 1) *Compliance with Engineering comments (Detention must be provided for any impervious areas constructed in excess of 4,000 square feet since 1984. Must comply with all storm water and flood control ordinances. Any work performed in the right of way will require a right of way permit. Need to verify that there is sufficient capacity for the receiving drainage system(s) to accept drainage from this property.)*
- 2) *Preservation status granted to any tree with a DBH of 50-inches or larger ;*
- 3) *Compliance with Urban Forestry comments (Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64). Removal of any live oak tree 24-inches or larger in diameter will require review and approval by Urban Forestry.)*
- 4) *Provision of two new frontage trees each along Bit and Spur Road and Westminster Way, as depicted;*
- 5) *Submission of an application for a sidewalk waiver prior to submittal for land disturbance or building permits; and*
- 6) *Provision of tree protection barriers at least 10-feet from the trunk of the tree, as well as any other tree that will be retained (depicted on any plans submitted for land disturbance or building permits), to be in place prior to any land disturbance activities.*

LOCATOR MAP



APPLICATION NUMBER 3 & 4 DATE September 4, 2008
APPLICANT Springhill Presbyterian Church
REQUEST Planned Unit Development, Planning Approval

N
NTS

PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The site is surrounded by single family residential units and there is a grocery store to the northeast.

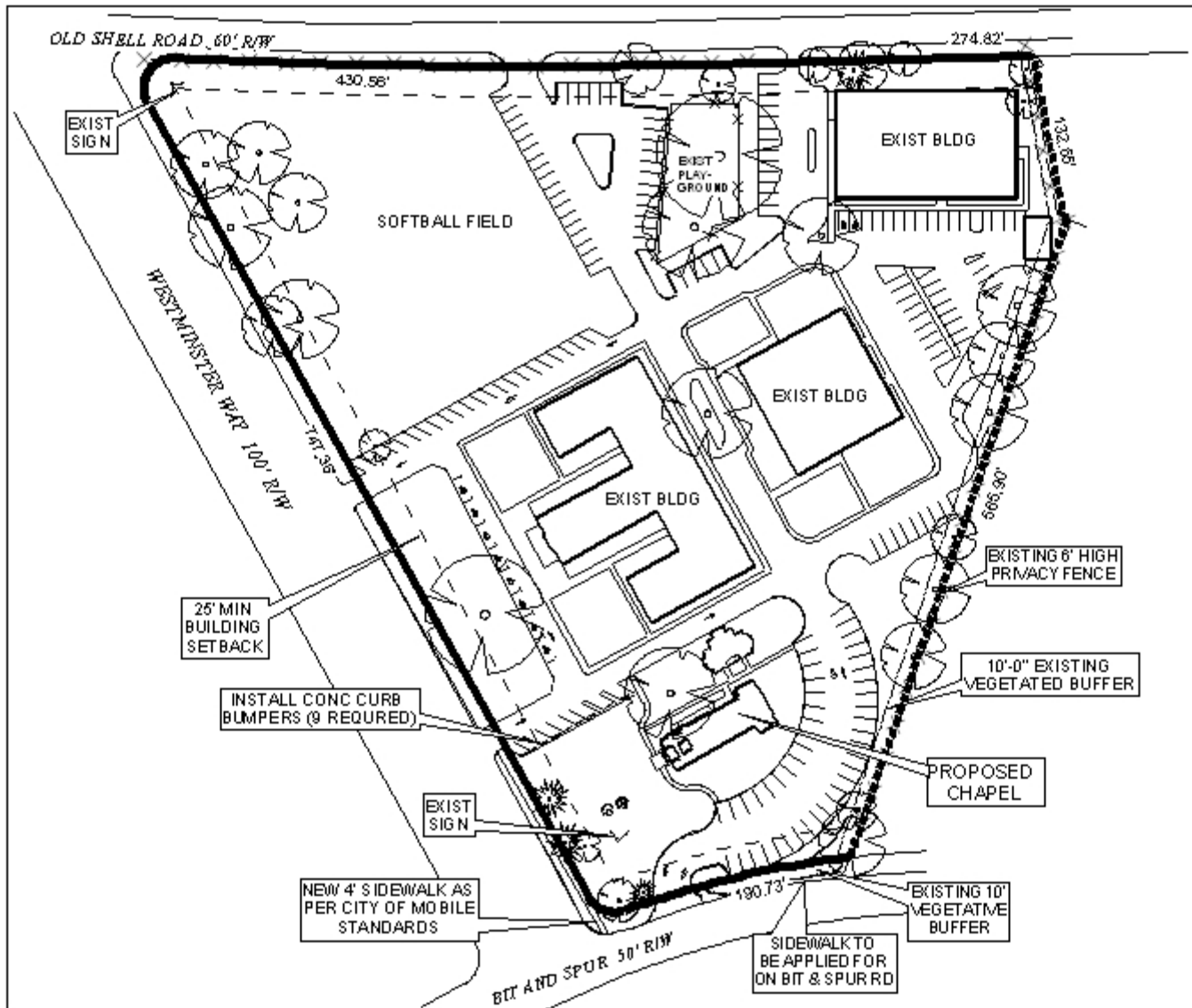
APPLICATION NUMBER 3 & 4 DATE September 4, 2008

APPLICANT Springhill Presbyterian Church

REQUEST Planned Unit Development, Planning Approval

LEGEND NTS

SITE PLAN



The site plan illustrates existing buildings, proposed building, landscaping, and parking.

APPLICATION NUMBER 3 & 4 DATE September 4, 2008
 APPLICANT Springhill Presbyterian Church
 REQUEST Planned Unit Development, Planning Approval

N
 NTS

PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The site is surrounded by single family residential units and there is a grocery store to the northeast.

APPLICATION NUMBER 3 & 4 DATE September 4, 2008
APPLICANT Springhill Presbyterian Church
REQUEST Planned Unit Development, Planning Approval

N
NTS