

**PLANNED UNIT DEVELOPMENT &
SUBDIVISION STAFF REPORT**

Date: August 17, 2006

DEVELOPMENT NAME The Pines at The Preserve Subdivision

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LOCATION North side of Girby Road, 3/10 mile± East of Lloyd's Lane extending to the East side of Lloyd's Lane, 600'± North of Girby Road, and extending to the West side of Campground Branch Creek and the South side of Scenic West Place Subdivision

**CITY COUNCIL
DISTRICT** District 6

PRESENT ZONING R-2, Two-Family Residential

AREA OF PROPERTY 136 Lots / 84.2± acres

CONTEMPLATED USE Amend Planned Unit Development Approval to allow reduced lot widths and sizes, reduced building setbacks, and increased site coverage in a single-family residential subdivision.

**TIME SCHEDULE
FOR DEVELOPMENT** Immediately

**ENGINEERING
COMMENTS** Provide detention for 100-year storm with a 10-year release rate. The existing receiving stream is compromised and the culvert @ Girby Road is reportedly undersized. Therefore, a complete analysis of the existing and proposed conditions will be required. In addition, the development will not be permitted for Land Disturbance unless the culvert at Girby Road has a capacity to handle a 50-year storm.

The flood zones shown on the preliminary plat should be revised to more accurately delineate the FEMA designated flood zones.

Dedication of a drainage easement of the flood way and portion of the flood zone containing the actual drainage way will be required. A study of the distance from the top of bank to top of bank of the drainage way plus 15 feet for maintenance vehicle access will be required for drainage easement.

Detention should be accomplished above the 100 year flood elevation. And, the predevelopment runoff coefficient to be used for the existing wetlands areas should be accurate for wetland areas.

Must comply with all stormwater and flood control ordinances. Any work performed in the right of way will require a right of way permit. The applicant is responsible for verifying if the site contains wetlands. The site can be checked against the National Wetlands Inventory on the COM web site Environmental Viewer. If the site is included on the NWI, it is the applicant's responsibility to confirm or deny the existence of regulatory wetlands.

TRAFFIC ENGINEERING

COMMENTS

Driveway number, size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards. Traffic Engineering requires traffic calming measures for "straight" sections of roadway over 650 feet in length with location and design to be approved by Traffic Engineering.

URBAN FORESTRY

COMMENTS

Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

FIRE DEPARTMENT

COMMENTS

All commercial projects shall comply with the 2003 International Fire Code and Appendices B-G as adopted by the City of Mobile on July 6, 2004. All One- or Two-Family residential developments shall comply with Appendices B, C and D of the 2003 International Fire Code as adopted by the City of Mobile on July 6, 2004. Development of one or two family dwellings where the numbers of dwelling units exceed 30 shall be provided with separate access roads or all dwelling units shall be equipped throughout with an approved automatic sprinkler system.

REMARKS

The applicant is requesting Planned Unit Development (PUD) and Subdivision approvals to allow reduced lot widths and sizes, reduced building setbacks, and increased site coverage, in a single-family residential subdivision consisting of 136 individual lots.

Planned Unit Development review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating excess traffic along minor residential streets in residential districts outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD.

A previous PUD and Subdivision approval in 2005 allowed 231 lots on 82.7± Acres, in a similar configuration. That plan was approved subject to the following conditions:

- 1) Full compliance with the Traffic Engineering Comments (driveway number, size, location and design to be approved by Traffic Engineering and conform to AASHTO standards, traffic calming measures should be included in development);
- 2) The provision of street stubs to the landlocked parcels to the West in the vicinity of Lots 67-104 and Lots 138-146;
- 3) That the applicant obtain all necessary federal, state and local permits;
- 4) Completion of the rezoning process (to R-2) for the entire site; and
- 5) Full compliance with all municipal codes and ordinances.

The applicant completed the rezoning process in 2005; however, the subdivision and Planned Unit Development approvals expired after one-year. In this application, only a portion of the total land area will be developed.

The PUD is required to allow the reduced lot widths, increased density, and reduced setbacks for innovative design. The project as proposed consists of 136-lots to be constructed on 36+ acres of the total 84.2+ acres, which the developer also owns and is indicated on the preliminary plat for future development.

The typical lots proposed are 50-feet wide with depths varying from 100-feet to 120-feet. The minimum lot size proposed is 5,000 square feet with a minimum front setback of 15-feet, with a 3-foot and 7-foot minimum side yard setback, and an 8-foot minimum rear setback. The detached single-family residences are to be constructed will range in size from 1,850 to 2,200 square feet. The residences will consist of pre-approved plans from which builders may select. Approval of the maximum building site coverage up to 45% is requested in order to construct the maximum 2,200 square foot building footprint on the minimum lot size of 5,000 square feet.

The applicant states that common area will be provided for stormwater detention, landscaping and green areas, along with areas located in designated flood plain areas and wetlands; however, 2.1± acres of wetland area must be disturbed, permitted, and mitigated for the development of this project. It should be noted that some areas shown on the previous plan as wetlands are not incorporated into some lots.

The site adjoins a creek and as such the area may be considered environmentally sensitive; thus the approval of all applicable federal, state and local agencies would be required. Additionally, there are wetlands on the site, some of which encroach into some lots; and while reduced lot widths and sizes are allowed, the applicant should submit documentation prior to the recording of the final plat, which illustrates that there is adequate buildable area (5,000 square feet based on the minimum lot) for those lots containing wetlands.

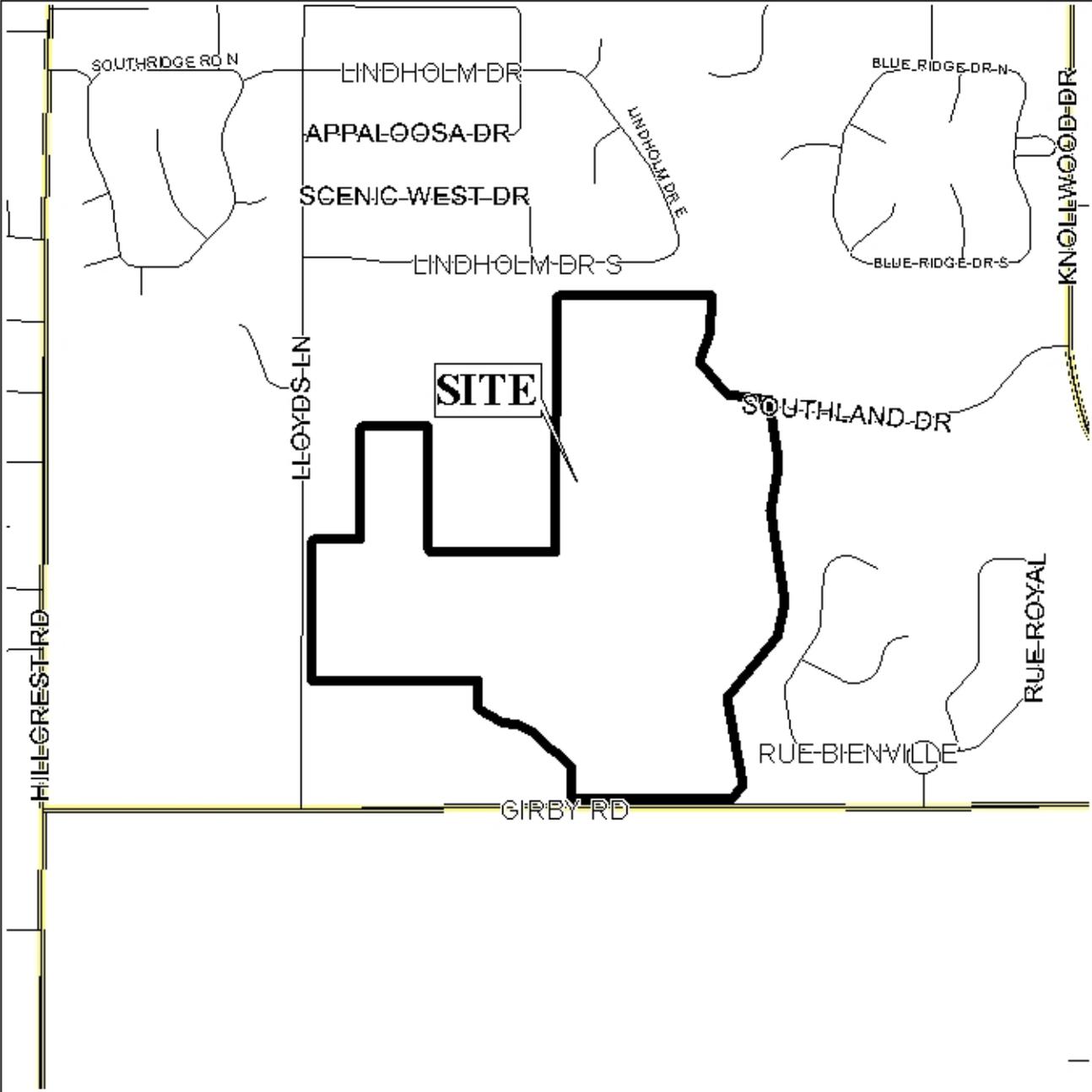
In terms of the Subdivision application, the site fronts Girby Road, a planned major street which has an existing right-of-way of 80-feet. The major Street Plan calls for 100-feet of right-of-way, thus the dedication of adequate right-of-way to provide 50-feet from the centerline of Girby Road should be required.

Section V.B.1 of the Subdivision Regulations state that “the street layout shall provide for the future projection into unsubdivided lands adjoining of a sufficient number...to provide convenient circulation.” As illustrated on the Vicinity map, the site adjoins multiple parcels to the North and West that are land locked, and the provision of one street stub between Lots 44 and 55, should be required for this phase of development. Additionally, street stubs may be required for land locked parcels to its West, when the Future Development is submitted. It should be noted that a street stub is not recommended to the East as the site borders a creek, and the Commission typically has not required street stubs to adjacent properties when the stub adjoins a waterway. Furthermore, street stubs were not required for the subdivision adjacent to the East.

RECOMMENDATION **Planned Unit Development** Based upon the preceding, this application is recommended for approval subject to the following conditions: 1) full compliance with the Traffic Engineering comments (driveway number, size, location and design to be approved by Traffic Engineering and conform to AASHTO standards, traffic calming measures should be included in the development; 2) full compliance with Engineering comments; 3) the provision of a street stub to the land locked parcel to the North in the vicinity of Lots 44-55; and 4) that the applicant obtain all necessary federal, state and local permits.

Subdivision With a waiver of Section V.D.2., the plat will meet the minimum requirements of the Subdivision Regulations and is recommended for Tentative Approval subject to the following conditions: 1) full compliance with the Traffic Engineering comments (driveway number, size, location and design to be approved by Traffic Engineering and conform to AASHTO standards, traffic calming measures should be included in the development; 2) full compliance with Engineering comments; 3) the provision of a street stub to the land locked parcel to the North in the vicinity of Lots 44-55; and 4) that the applicant obtain all necessary federal, state and local permits.

LOCATOR MAP



APPLICATION NUMBER 28 & 29 DATE August 17, 2006

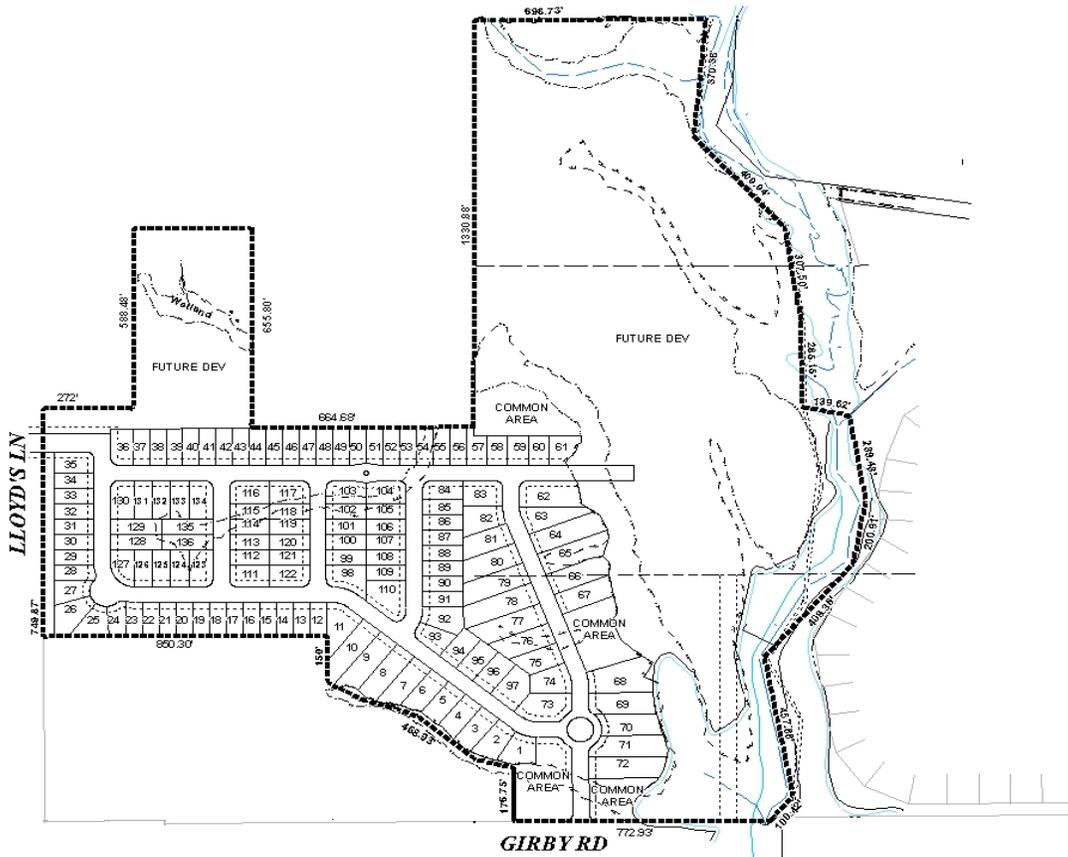
APPLICANT The Pines at the Preserve Subdivision

REQUEST Subdivision, Planned Unit Development



NTS

SITE PLAN



The site plan illustrates the proposed development

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LEGEND														
	R-1	R-2	R-3	R-A	R-B	H-B	B-1	LB-2	B-2	B-3	B-4	B-5	I-1	I-2

