

**PLANNING APPROVAL,
PLANNED UNIT DEVELOPMENT &
SUBDIVISION STAFF REPORT**

Date: June 6, 2013

<u>NAME</u>	Carmelite Nuns Discalced
<u>SUBDIVISION NAME</u>	Carmelite Nuns Discalced Subdivision
<u>LOCATION</u>	716 Dauphin Island Parkway (Northwest corner of Dauphin Island Parkway and Halls Mill Road, extending to the Southeast corner of Gosson Street and Johnston Avenue)
<u>CITY COUNCIL DISTRICT</u>	District 2
<u>PRESENT ZONING</u>	R-1, Single-Family Residential District, R-3, Multi-Family Residential District, and B-3, Community Business District.
<u>AREA OF PROPERTY</u>	1 Lot / 6.0± Acres
<u>CONTEMPLATED USE</u>	Planning Approval to allow a building expansion and expanded parking lot at an existing monastery in an R-3, Multi-Family Residential District, Planned Unit Development approval to allow multiple buildings on a single building site, and Subdivision approval to create one legal lot of record.
<u>TIME SCHEDULE FOR DEVELOPMENT</u>	Immediate.

ENGINEERING

COMMENTS

1) Provide all of the required information on the Final Plat (i.e. signature blocks, signatures, certification statements, legal description, required notes) including the Professional Land Surveyor seal and signature. 2) Provide a signature block and signature from the Planning Commission, Owner(s) (notarized), and the Traffic Engineering Department. 3) Provide a signature block for the City Engineer and the County Engineer. 4) A Land Disturbance Permit will be required for the proposed site. These improvements will require storm water detention in accordance with the Storm Water Management and Flood Control Ordinance (Mobile City Code, Chapter 17, Ordinance #65-007 & #65-045). 5) Any work performed in the existing ROW (right-of-way) such as driveways, sidewalks, utility connections, grading, drainage, irrigation, or landscaping will require a ROW permit from the

City of Mobile Engineering Department (208-6070) and must comply with the City of Mobile Right-of-Way Construction and Administration Ordinance (Mobile City Code, Chapter 57, Article VIII).

TRAFFIC ENGINEERING

COMMENTS

Driveway number, size, location and design to be approved by Traffic Engineering and conform to AASHTO standards. Standards for 90 degree parking include a 24 foot aisle. Changes should be made to the parking as shown (angled spaces or wider aisle) to meet parking standards.

URBAN FORESTRY

COMMENTS

Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64). Preservation status is to be given to the 72" Live Oak Tree located within the proposed Handicapped Parking. Any work on or under this tree is to be permitted and coordinated with Urban Forestry; removal to be permitted only in the case of disease or impending danger. Granting Preservation Status will require the relocation of proposed Handicapped Parking.

MAWSS COMMENTS

MAWSS has water and sewer services available, but a Capacity Assurance application for additional sewer service has not been applied for. MAWSS cannot guarantee additional sewer service until a Capacity application is approved by Volkert Engineering, Inc.

FIRE DEPARTMENT

COMMENTS

All projects within the City of Mobile Fire Jurisdiction must comply with the requirements of the 2009 International Fire Code, as adopted by the City of Mobile.

REMARKS

The applicant is requesting Planning Approval to allow a building expansion and expanded parking lot at an existing monastery in an R-3, Multi-Family Residential District, Planned Unit Development approval to allow multiple buildings on a single building site, and Subdivision approval to create one legal lot of record. It should be noted that the site was erroneously listed as R-1, Single-Family Residential, in the public notifications for this project. However, the site is split-zoned R-1, Single-Family Residential, R-3, Multi-Family Residential, and B-3 Community Business.

The review required for Planning Approval examines the applicant's location and site plan with regard to transportation, parking and access, public utilities and facilities, traffic congestion and hazard, and to determine if the proposal is in harmony with the orderly and appropriate development of the district.

Planned Unit Development review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating

excess traffic along minor residential streets in residential districts outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD. PUD approval is site plan specific, thus if any new construction is anticipated that will change an approved site plan, an application to amend an existing, approved PUD must be made prior to any construction activities.

It is very important to note that the Planning Approval and PUD review are site plan specific; therefore any future changes to the overall site plan must be submitted for PUD review. Additionally, if the site plan is changed from what is approved by the Planning Commission, a new application for Planning Approval will be required.

The applicant proposes to construct a new 30' by 70' gathering building on the grounds of the existing monastery to include an open assembly/dining area, food preparation area, stage, equipment storage, and restrooms. Also proposed is a parking lot expansion to provide twenty additional on-site parking spaces.

The majority of the site is zoned R-3. However, a triangular-shaped area on the East side of the property behind the adjacent B-3 zoning on Dauphin Island Parkway is also zoned B-3. And a rectangular portion in the Northwest corner of the site is zoned R-1. Due to the fact that the site is under Subdivision, Planning Approval, and Planned Unit Development Approval review, the split zoning should be tended to via a Zoning Amendment application to unify the zoning classification of the over-all site.

Regarding the Subdivision application, the site fronts Dauphin Island Parkway, Halls Mill Road, Gosson Street, and Johnston Avenue. Dauphin Island Parkway is a major street with a compliant 62' right-of-way, and Gosson Street and Johnston Avenue are minor streets with compliant 60' rights-of-way. Halls Mill Road is a component of the Major Street Plan, but no additional right-of-way is planned in this area. Therefore, no dedication would be required along any of the four street frontages. Dedication should be required at the intersection of Dauphin Island Parkway and Gosson Street to provide a 25' radius curve.

As a means of access, the site should be limited to one curb cut to Dauphin Island Parkway, with the size, location and design of the curb cut to be approved by Traffic Engineering and conform to AASHTO standards. Since Gosson Street and Johnston Avenue are residential streets, access to them should be denied, as should access to Halls Mill Road, due to its congested nature in this area. The plan should be revised to illustrate the 25' minimum building setback line along all street frontages. The Final Plat should retain the labeling of the lot size in square feet and acres, or a table should be furnished providing the same information.

Urban Forestry has determined that the 72" Live Oak Tree located North of the existing monastery building should be given Preservation status. Any work on or under this tree is to be permitted and coordinated with Urban Forestry; removal to be permitted only in the case of disease or impending danger.

Review of the Planning Approval and Planned Unit Development is limited to the Northern portion of the site where the expansion is proposed. However, these reviews are a bit more complicated with regard to site plan deficiencies.

The entrance drive and drive aisle width in the parking area is indicated to be 20'. As Traffic Engineering has expressed in its comments, the standards for 90 degree parking include a 24 foot aisle. Changes should be made to the parking as shown (angled spaces or wider aisle) to meet parking standards. The entrance drive should be widened to 24' and the curb cut to Dauphin Island Parkway modified to meet Traffic Engineering requirements. If driveway gates are to be provided, such should be indicated and shown to meet the requirement for a three-vehicle, 60' queuing space out of the public right-of-way. The required widening of the drive will require a re-positioning and/or re-configuration of the proposed parking areas. The site plan does not indicate bumper stops or curbing around the parking areas; therefore it should be revised to indicate either one or the other. Urban Forestry has determined that Preservation status be given to the 72" Live Oak tree located within the proposed handicapped parking area. Any work on or under this tree is to be permitted and coordinated with Urban Forestry; removal to be permitted only in the case of disease or impending danger. This will require the relocation of that parking area.

An existing nose-in/back-out parking area along Dauphin Island Parkway is shown to be landscaped on the site plan, but no mention is made that the parking area is to be removed and landscaped. If that is the intent, the site plan should be revised to so indicate such is proposed.

The 25' minimum building setback line should also be shown on both site plans. The site plan does not indicate any over-story trees within the first 25' of either the Dauphin Island Parkway or Johnston Avenue frontages. As the site has an abundance of over-story trees located within the project area, the applicant should coordinate with Urban Forestry on what would be required within the frontage areas.

No public sidewalk is indicated along Johnston Avenue at the Northwest corner of the project area; therefore, a compliant City-standard sidewalk should be indicated in that area or a Sidewalk Waiver application submitted. No buffering is indicated along adjacent residential sites to the North; therefore, the site plan should be revised to provide such.

No dumpster is indicated on the site plan, nor is a note on the site plan indicating that no dumpster will be utilized. Therefore, the site plan should be revised to either indicate a compliant dumpster with screening and sanitary sewer drainage connection or a note should be placed on the site plan indicating that there will be no dumpster utilized and also indicating what type of garbage collection will be utilized.

The Planned Unit Development and Planning Approval applications should be considered for holdover to the June meeting to allow the applicant to make site plan revisions to bring the site into compliance with respect to the deficiencies described above. The Subdivision application should also be heldover to the June meeting in order to be considered with revised PUD and Planning Approval applications.

RECOMMENDATION

Subdivision: Based upon the preceding, this request is recommended for holdover to the July 11th meeting in order to be considered in conjunction with revised PUD and Planning Approval applications, and a Zoning Amendment application to unify the over-all zoning of the site.

Planned Unit Development: Based upon the preceding, this request is recommended for holdover to the July 11th meeting, with revision due by June 17th, in order for the applicant to address the following items:

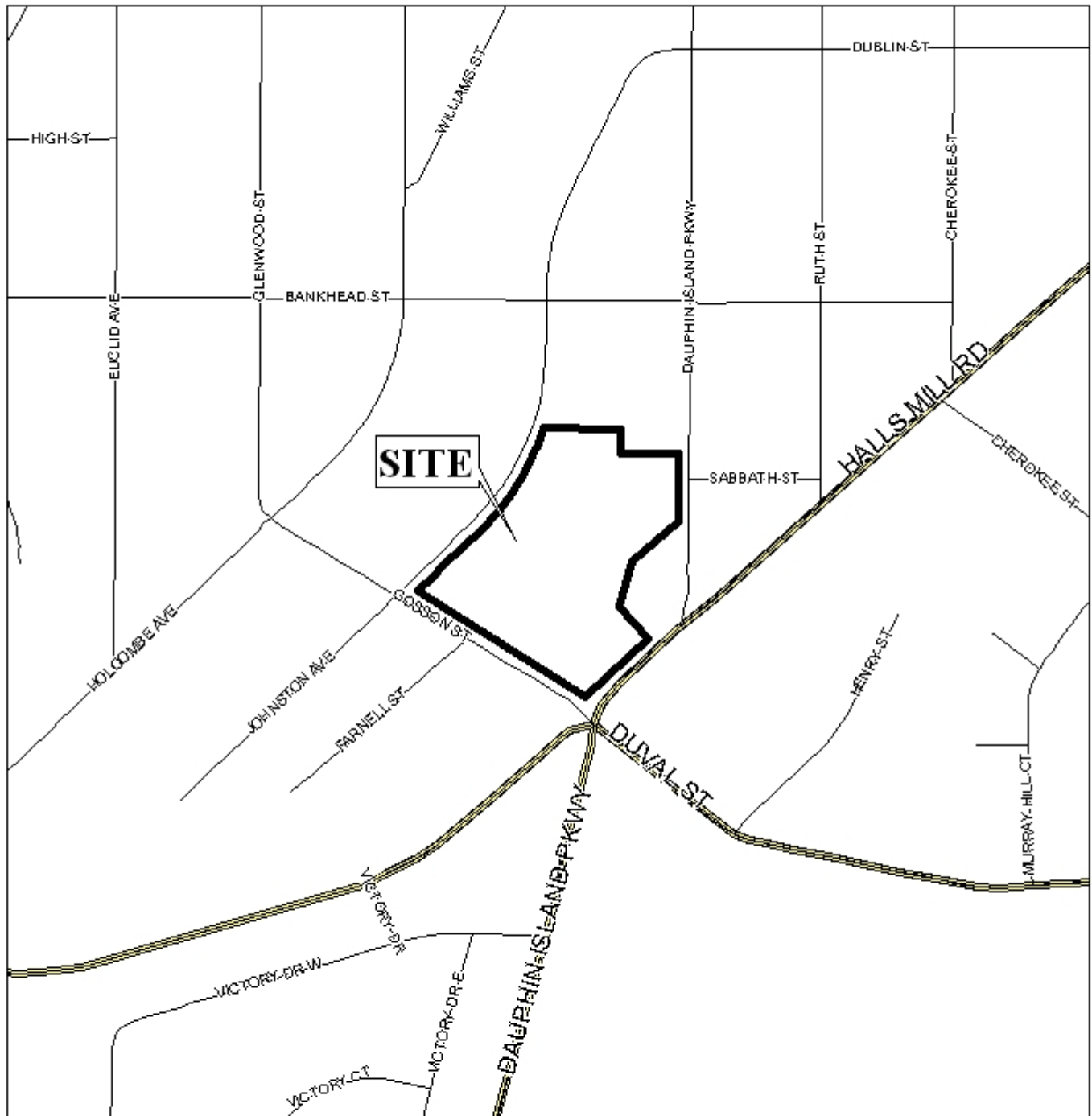
- 1) revision of the site plan to indicate a 24' wide drive aisle;
- 2) revision of the site plan to indicate a compliant curb cut to Dauphin Island Parkway, to be coordinated with Traffic Engineering;
- 3) if driveway gates are proposed, revision of the site plan to indicate a three-vehicle, 60' queuing space out of the public right-of-way;
- 4) indication of bumper stops or curbing within the parking areas;
- 5) placement of a note on the site plan stating that the 72" Live Oak tree (within the proposed handicapped parking area) is granted Preservation status and any work on or under this tree is to be permitted and coordinated with Urban Forestry; removal to be permitted only in the case of disease or impending danger;
- 6) revision of the site plan to locate the proposed handicapped parking area away from the 72" Live Oak tree given Preservation status;
- 7) revision of the site plan to indicate that the nose-in/back-out parking along Dauphin Island Parkway is to be removed and replaced with landscaping, if such is the case;
- 8) revision of the site plan to indicate the 25' minimum building setback line along all street frontages;
- 9) revision of the site plan to indicate required over-story trees within the first 25' of street frontages, to be coordinated with Urban Forestry due to the abundance of over-story trees within the project area;
- 10) revision of the site plan to indicate a City-standard public sidewalk along Johnston Avenue at the Northwest corner of the project area, or the submission of a Sidewalk Waiver application;
- 11) revision of the site plan to indicate a buffer along adjacent residential sites to the North of the project area;
- 12) revision of the site plan to indicate a compliant dumpster with enclosure and sanitary sewer drainage connection, or the placement of a note on the site plan stating that no dumpster will be utilized and stating what method of garbage collection will be utilized; and
- 13) submission of a Zoning Amendment application to unify the zoning of the over-all site.

Planning Approval: Based upon the preceding, this request is recommended for holdover to the July 11th meeting, with revision due by June 17th, in order for the applicant to address the following items:

- 1) revision of the site plan to indicate a 24' wide drive aisle;

- 2) revision of the site plan to indicate a compliant curb cut to Dauphin Island Parkway, to be coordinated with Traffic Engineering;
- 3) if driveway gates are proposed, revision of the site plan to indicate a three-vehicle, 60' queuing space out of the public right-of-way;
- 4) indication of bumper stops or curbing within the parking areas;
- 5) placement of a note on the site plan stating that the 72" Live Oak tree (within the proposed handicapped parking area) is granted Preservation status and any work on or under this tree is to be permitted and coordinated with Urban Forestry; removal to be permitted only in the case of disease or impending danger;
- 6) revision of the site plan to locate the proposed handicapped parking area away from the 72" Live Oak tree given Preservation status;
- 7) revision of the site plan to indicate that the nose-in/back-out parking along Dauphin Island Parkway is to be removed and replaced with landscaping, if such is the case;
- 8) revision of the site plan to indicate the 25' minimum building setback line along all street frontages;
- 9) revision of the site plan to indicate required over-story trees within the first 25' of street frontages, to be coordinated with Urban Forestry due to the abundance of over-story trees within the project area;
- 10) revision of the site plan to indicate a City-standard public sidewalk along Johnston Avenue at the Northwest corner of the project area, or the submission of a Sidewalk Waiver application;
- 11) revision of the site plan to indicate a buffer along adjacent residential sites to the North of the project area;
- 12) revision of the site plan to indicate a compliant dumpster with enclosure and sanitary sewer drainage connection, or the placement of a note on the site plan stating that no dumpster will be utilized and stating what method of garbage collection will be utilized; and
- 13) submission of a Zoning Amendment application to unify the zoning of the over-all site.

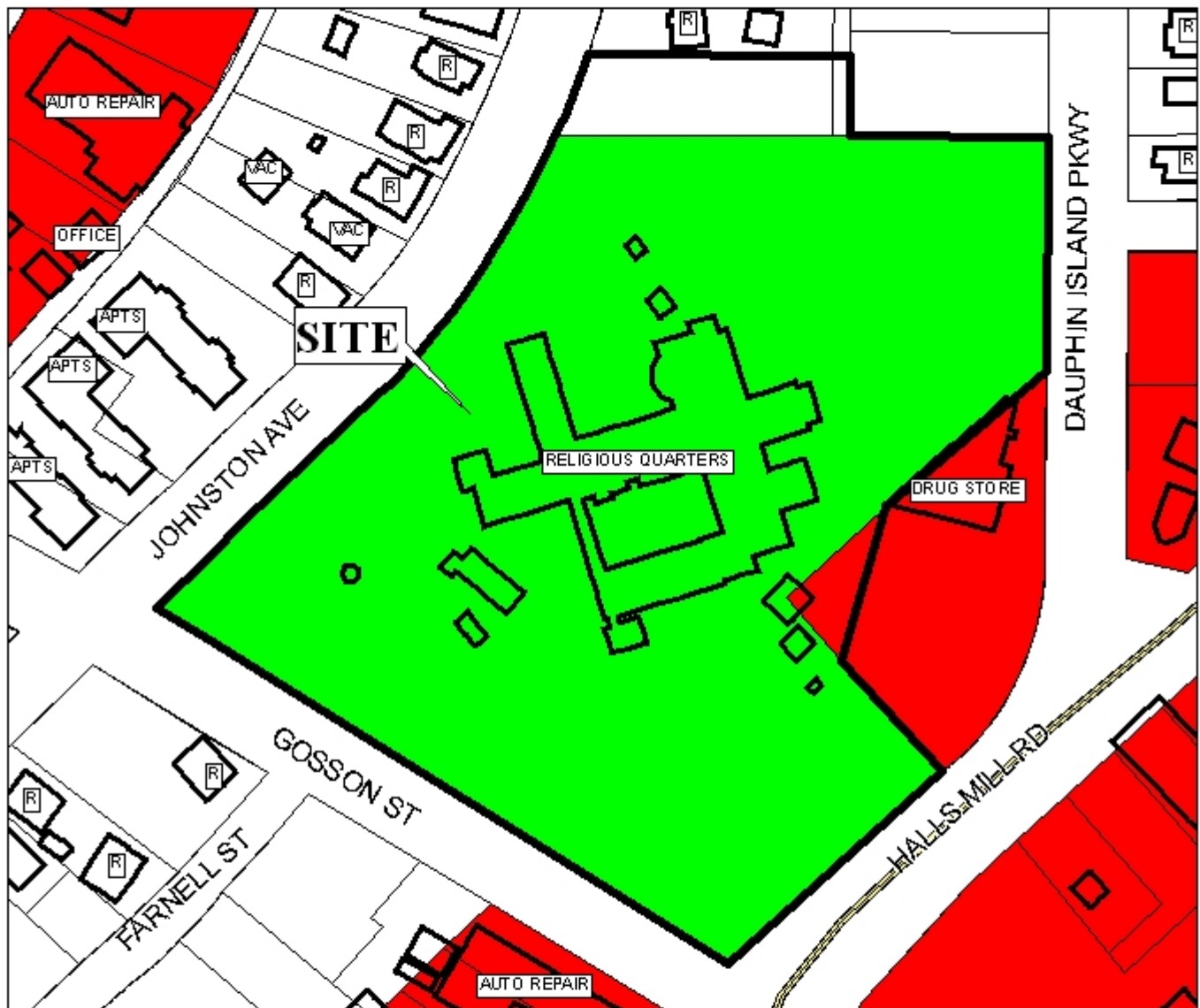
LOCATOR MAP



APPLICATION NUMBER 25, 26 & 27 DATE June 6, 2013
APPLICANT Carmelite Nuns Discalced Subdivision
REQUEST Subdivision, PUD, Planning Approval



PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



Businesses are located to the east and south of the site.
Single-family residences are located to the west and north.

APPLICATION NUMBER 25, 26 & 27 DATE June 6, 2013

APPLICANT Carmelite Nuns Discalced Subdivision

REQUEST Subdivision, PUD, Planning Approval

LEGEND

R-1	R-2	R-3	R-A	R-B	H-B	T-B	B-1	LB-2	B-2	B-3	B-4	B-5	I-1
													I-2

N
NTS

PLANNING COMMISSION VICINITY MAP - EXISTING ZONING

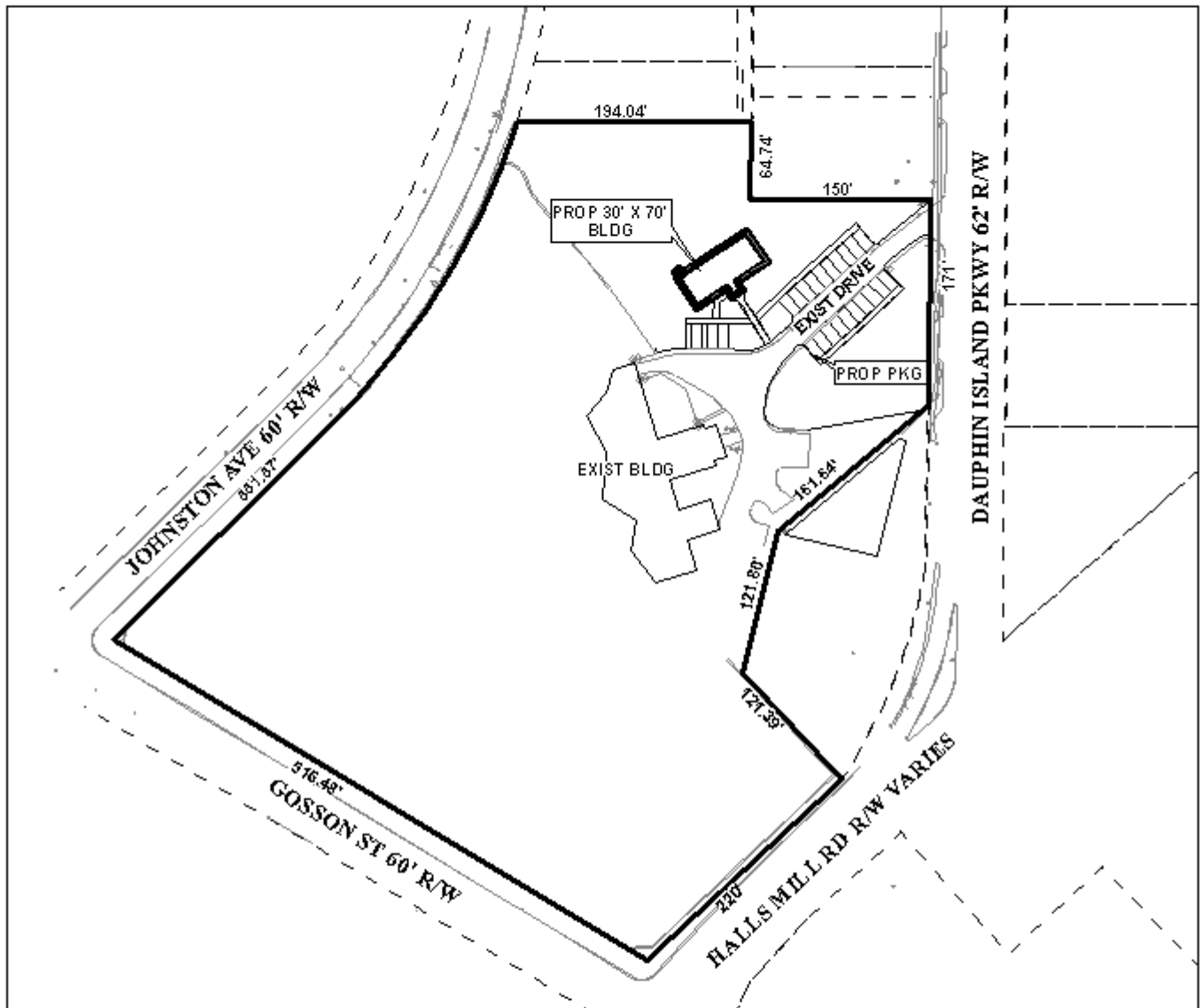


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NTS

SITE PLAN



The site plan illustrates the existing improvements, proposed building, and proposed parking.

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