

**PLANNED UNIT DEVELOPMENT,  
PLANNING APPROVAL &  
SUBDIVISION STAFF REPORT**

**Date: September 5, 2013**

<b><u>NAME</u></b>	Blue Creek Coal Terminal Subdivision
<b><u>SUBDIVISION NAME</u></b>	Blue Creek Coal Terminal Subdivision
<b><u>LOCATION</u></b>	80 Virginia Street (Area bounded by CSX Railroad, New Jersey Street, Old Water Street, and Mobile River, extending to the North side of Virginia Street, 465'± East of Conception Street))
<b><u>CITY COUNCIL DISTRICT</u></b>	District 2
<b><u>PRESENT ZONING</u></b>	I-2, Heavy-Industry District
<b><u>AREA OF PROPERTY</u></b>	1 lot / 36.9 acres ±
<b><u>CONTEMPLATED USE</u></b>	Planning Approval to allow a coal handing terminal in an I-2, Heavy-Industry District, Planned Unit Development approval to allow multiple buildings on a single building site, and Subdivision approval to create one legal lot.
<b><u>TIME SCHEDULE FOR DEVELOPMENT</u></b>	After appropriate approvals are obtained.

**ENGINEERING  
COMMENTS**

**Subdivision**

The following comments should be addressed prior to acceptance and signature by the City Engineer:

- 1) Provide all of the required information on the Plat ( i.e. signature blocks, signatures, required notes).
- 2) Add a note to the Plat stating that storm water detention will be required for any existing development (since 1984) that did not receive Land Disturbance permitting and any future addition(s) and/or land disturbing activity in accordance with the Storm Water Management and Flood Control Ordinance (Mobile City Code, Chapter 17 , Ordinance #65-007 & #65-045).
- 3) Add a signature block for the Owner, Notary, Planning Commission and Traffic Engineer.
- 4) Provide and label the monument set or found at each subdivision corner.
- 5) Show and label the existing parcel lines.

- 6) Provide the Surveyor’s, Owner’s (notarized), Planning Commission, and Traffic Engineering signatures.
- 7) Add a note to the Plat stating that the approval of all applicable federal, state, and local agencies (including all stormwater runoff, wetland and floodplain requirements) would be required prior to the issuance of a permit for any land disturbance activity.

**Planned Unit Development and Planning Approval**

- 1) Any work performed in the existing ROW (right-of-way) such as driveways, sidewalks, utility connections, grading, drainage, irrigation, or landscaping will require a ROW permit from the City of Mobile Engineering Department (208-6070) and must comply with the City of Mobile Right-of-Way Construction and Administration Ordinance (Mobile City Code, Chapter 57, Article VIII).
- 2) Any and all proposed development will need to be in conformance with the Storm Water Management and Flood Control Ordinance (Mobile City Code, Chapter 17, Ordinance #65-007 & #65-045); the City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control. Additional requirements may be added during the Land Disturbance Permit review process depending on the proposed containment and CBMPP related to for water quality issues related to runoff.
- 3) A complete set of construction plans for the site work – including, but not limited to, drainage, utilities, grading, storm water detention systems, paving, and all above ground structures, will be required to be submitted with the Land Disturbance permit. These plans are to be submitted and approved prior to beginning any of the construction work.
- 4) According to the FEMA flood map information, this property is located within a Special Flood Hazard Area. You will need to show and label the flood hazard area(s) on PUD Site Plan. Also, you will need to list the Minimum Finished Floor Elevation (MFFE) for the Lot.
- 5) No Land Disturbance Permit will be issued until any on-site remediation is complete.
- 6) Must comply with all Engineering Department Policy Letters:
  - A) 5-13-2009 Policy Letter(Car wash drains and dumpster pads to drain to Sanitary Sewer System)
  - B) 8-4-2004 Policy Letter (Video inspection of new Storm Sewer System Piping)
  - C) 3-18-2004 Policy Letter (Additional subdivision street requirements)

**TRAFFIC ENGINEERING**  
**COMMENTS**

Driveway number, size, location and design to be approved by Traffic Engineering and conform to AASHTO standards.

**URBAN FORESTRY**  
**COMMENTS**

Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

**FIRE DEPARTMENT**

**COMMENTS**

All projects within the City of Mobile Fire Jurisdiction must comply with the requirements of the 2009 International Fire Code, as adopted by the City of Mobile.

**MAWSS**

**COMMENTS**

MAWSS has water and sewer services available, but a Capacity Assurance application for sewer service has not been applied for. MAWSS cannot guarantee sewer service until the Capacity application is approved by Volkert Engineering Inc.

**REMARKS**

The applicant is requesting Planning Approval to allow a coal handing terminal in an I-2, Heavy-Industry District, Planned Unit Development approval to allow multiple buildings on a single building site, and Subdivision approval to create one legal lot. Coal handing terminals require Planning Approval when located in I-2 districts.

The applicant proposes to redevelop a vacant site that was once a coke and iron ore handling facility. All structures on the site were demolished in 2011-2012 to make way for a modern coal handling facility. The proposed facility is approximately 2,000 feet North of the McDuffie Coal Terminal.

The review required for Planning Approval examines the applicant's location and site plan with regard to transportation, parking and access, public utilities and facilities, traffic congestion and hazard, and to determine if the proposal is in harmony with the orderly and appropriate development of the district.

Planned Unit Development review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating excess traffic along minor residential streets in residential districts outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD.

**Planning Approval and PUD approval are site plan specific, thus the plan must be accurate at time of submittal, and any changes to the site plan must be approved by the Planning Commission.**

Subdivision review examines the site with regard to promoting orderly development, protecting general health, safety and welfare, and ensuring that development is correlated with adjacent developments and public utilities and services, and to ensure that the subdivision meets the minimum standards set forth in the Subdivision Regulations for lot size, road frontage, lot configuration, etc.

The site fronts onto Virginia Street, a minor street at this location, with an existing right-of-way of 50-feet. No dedication will be required.

The preliminary plat depicts additional street rights-of-way adjacent to the proposed lot, but information available to staff indicates that the rights-of-way abutting the North side of the site, and that portion of Virginia Street that is East of the proposed entrance to the property have been vacated. The surveyor should verify that abutting rights-of-way have been vacated, and accordingly revise the plat.

The site plan submitted with the application depicts one proposed curb-cut to Virginia Street. As PUD and Planning Approval review are site plan specific, the site should be limited to the single proposed curb-cut, with the size, design and location to be approved by Traffic Engineering, and to comply with AASHTO standards. It should be noted, however, that the presence of a guard building at the entrance to the site implies to staff that the entrance may be gated. If an entry gate is proposed, it should be depicted on the site plan and it should be set at least 60-feet from the right-of-way of Virginia Street so that emergency vehicles will have sufficient room to turn around if necessary.

The applicant proposes to construct a new dock, stockpile coal shuttle conveyors, stockpile transfer conveyors, a ship loader, a barge unloader, maintenance and office buildings, and parking spaces for employees and visitors. Two storm water detention areas will be provided on-site.

The proposed buildings on the site include a 2,400 square foot office, a 1,500 square foot bathroom and break room building, a 900 square foot stevedore building, and a 5,000 square foot maintenance facility. Twenty (20) parking spaces are proposed, which will comply with the requirements of the Zoning Ordinance. A guard building is also proposed as the entrance to the site.

Regarding the coal handling aspects, the site will have the capacity for two coal stockpiles of 180,000 metric tons each. Various conveyors will link the dock area on the North side of the dredged slip, which services barges, to the stockpiles, and to the dock area on the South side of the dredged slip, which will serve ships up to approximately 1,000 feet in length. The facility will receive coal deliveries by barge, and export coal by ship.

The site plan depicts trees and landscape areas on the site. As the site is in an I-2 zone, only frontage trees are required. Calculations by staff indicate that 14 frontage trees are required, to be planted within the 25-foot setback along the Virginia Street frontage. Only 13 trees are depicted, and none appear to be within the setback area. Measurements of the site area by staff indicate that the non-navigable water portion of the site is no more than 22 acres of the total 36.9 acres. As such, the landscape area calculations on the site plan should be revised to only consider the dry land area, not any portion of the site over water (such as a dock), or the water area associated with the Mobile River: essentially any area behind the bulkhead should provide the basis for the landscape area calculations. The site plan and calculations should be revised to reflect the reduced landscape area requirements.

The site plan does not indicate which portions of the on-site access road will be paved, versus surfaced with aggregate or other materials. The site plan should be revised to depict paved

versus aggregate surfacing. Additionally, the parking area should include concrete wheel stops if no curb-and-gutter will be provided.

Regarding the Subdivision, it appears that the entire site is within a flood zone associate with the Mobile River. The preliminary plat includes a minimum finished floor elevation, and such should be depicted on the final plat, if approved.

As the site is located within a flood zone, development of the site must comply with all local, state and federal regulations regarding development within a flood zone.

The size of the proposed lot on the application was stated as 36.9 ± acres, while a note on the plat indicates that it is 37.1 ± acres. The final plat should be labeled with the correct area, in both square feet and acres, as should the PUD and Planning Approval site plans.

As previously mentioned, the preliminary plat depicts abutting street rights-of-way that according to information available to staff have been vacated. The plat should be revised to remove vacated rights-of-way from the plat.

A note on the plat indicates the abutting railroad is the Louisville and Nashville Railroad. The L & N Railroad disappeared in 1983 when it was merged into a new rail provider that is now CSX. The plat should be revised to re-label the abutting railroad as CSX.

Finally, the 25-foot minimum building setback line is depicted on the preliminary plat but is not depicted on the site plans. The site plans should be revised to also depict the 25-foot minimum building setback line.

## **RECOMMENDATION**

**Planning Approval:** The request is recommended for Approval, subject to the following conditions:

- 1) Revision, if necessary, to label the correct size of the lot in square feet and acres, to match that on the final plat;
- 2) Revision of the site plan to depict any proposed entrance gate at least 60 feet from the edge of the Virginia Street right-of-way;
- 3) Revision of the site plan to depict the 25-foot minimum building setback line from Virginia Street;
- 4) Revision of the site plan to depict 14 frontage trees located within the 25-foot minimum building setback;
- 5) Revision of the site plan and landscape area calculations to only provide frontage and total landscape area based upon only that portion of the site that is actual land (that is, behind the bulkhead);
- 6) Compliance with Traffic Engineering comments (*Driveway number, size, location and design to be approved by Traffic Engineering and conform to AASHTO standards.*);
- 7) Compliance with Engineering comments (*I) Any work performed in the existing ROW (right-of-way) such as driveways, sidewalks, utility connections, grading, drainage, irrigation, or landscaping will require a ROW permit from the City of Mobile*

- Engineering Department (208-6070) and must comply with the City of Mobile Right-of-Way Construction and Administration Ordinance (Mobile City Code, Chapter 57, Article VIII). 2) Any and all proposed development will need to be in conformance with the Storm Water Management and Flood Control Ordinance (Mobile City Code, Chapter 17, Ordinance #65-007 & #65-045); the City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control. Additional requirements may be added during the Land Disturbance Permit review process depending on the proposed containment and CBMPP related to for water quality issues related to runoff. 3) A complete set of construction plans for the site work – including, but not limited to, drainage, utilities, grading, storm water detention systems, paving, and all above ground structures, will be required to be submitted with the Land Disturbance permit. These plans are to be submitted and approved prior to beginning any of the construction work. 4) According to the FEMA flood map information, this property is located within a Special Flood Hazard Area. You will need to show and label the flood hazard area(s) on PUD Site Plan. Also, you will need to list the Minimum Finished Floor Elevation (MFFE) for the Lot. 5) No Land Disturbance Permit will be issued until any on-site remediation is complete. 6) Must comply with all Engineering Department Policy Letters: A) 5-13-2009 Policy Letter (Car wash drains and dumpster pads to drain to Sanitary Sewer System); B) 8-4-2004 Policy Letter (Video inspection of new Storm Sewer System Piping); C) 3-18-2004 Policy Letter (Additional subdivision street requirements.);*
- 8) Compliance with Urban Forestry comments (*Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).;*)
  - 9) Compliance with Fire comments (*All projects within the City of Mobile Fire Jurisdiction must comply with the requirements of the 2009 International Fire Code, as adopted by the City of Mobile.;*)
  - 10) Development of the site must comply with all local, state and federal regulations regarding development within a flood zone;
  - 11) Revision of the site plan to indicate which portions of the on-site access road and parking area will be paved in asphalt, concrete or similar, versus aggregate;
  - 12) Depiction of concrete wheel stops in the parking area if no curb-and-gutter will be provided;
  - 13) Submission of a revised site plan to the Planning Section prior to the signing of the final plat; and
  - 14) Full compliance with all other municipal codes and ordinances, including the obtaining of the appropriate permits for land disturbance and new construction.

**Planned Unit Development:** The request is recommended for for Approval, subject to the following conditions:

- 1) Revision, if necessary, to label the correct size of the lot in square feet and acres, to match that on the final plat;
- 2) Revision of the site plan to depict any proposed entrance gate at least 60 feet from the edge of the Virginia Street right-of-way;
- 3) Revision of the site plan to depict the 25-foot minimum building setback line from Virginia Street;

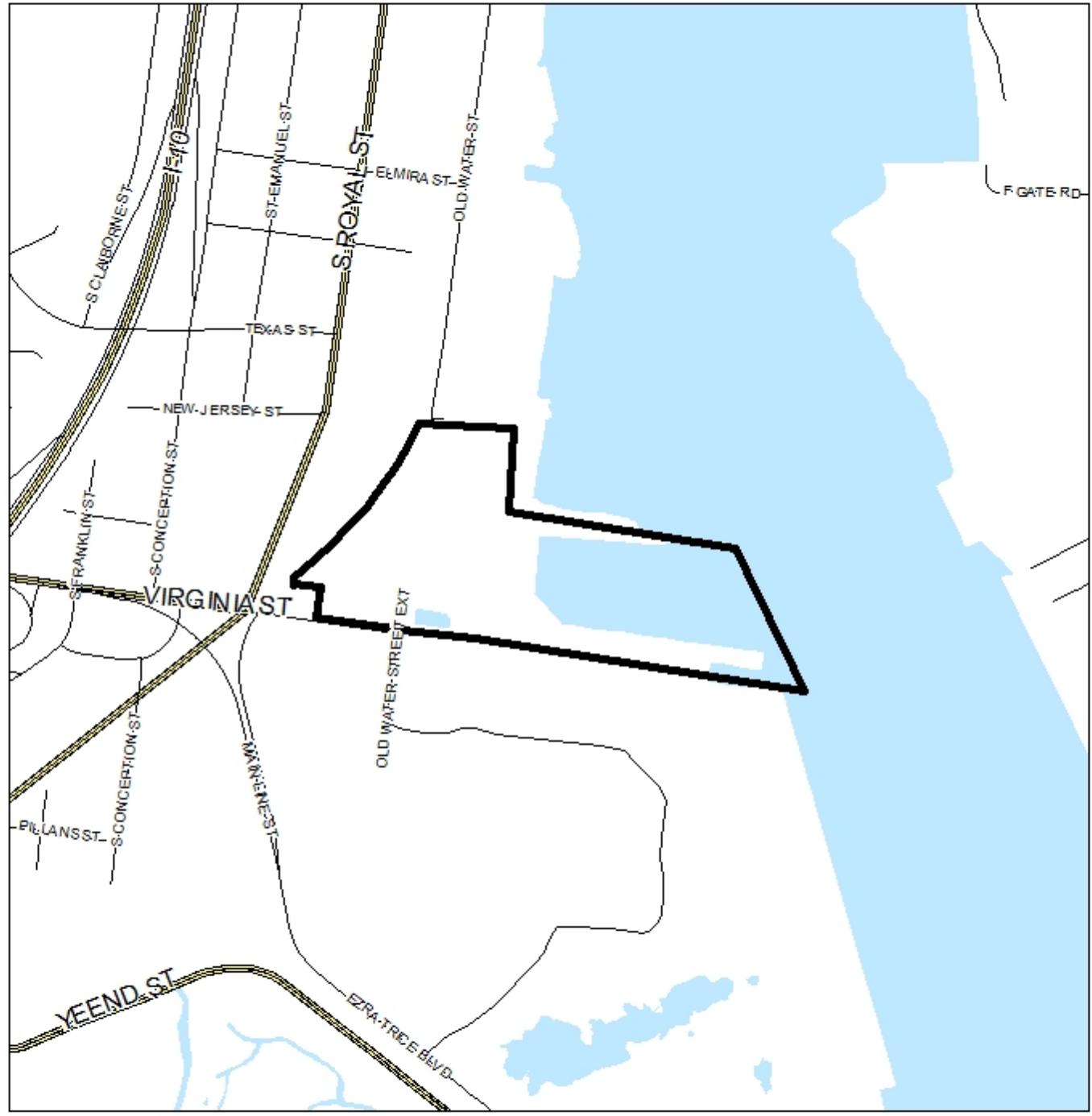
- 4) Revision of the site plan to depict 14 frontage trees located within the 25-foot minimum building setback;
- 5) Revision of the site plan and landscape area calculations to only provide frontage and total landscape area based upon only that portion of the site that is actual land (that is, behind the bulkhead);
- 6) Compliance with Traffic Engineering comments (*Driveway number, size, location and design to be approved by Traffic Engineering and conform to AASHTO standards.*);
- 7) Compliance with Engineering comments (*1) Any work performed in the existing ROW (right-of-way) such as driveways, sidewalks, utility connections, grading, drainage, irrigation, or landscaping will require a ROW permit from the City of Mobile Engineering Department (208-6070) and must comply with the City of Mobile Right-of-Way Construction and Administration Ordinance (Mobile City Code, Chapter 57, Article VIII). 2) Any and all proposed development will need to be in conformance with the Storm Water Management and Flood Control Ordinance (Mobile City Code, Chapter 17, Ordinance #65-007 & #65-045); the City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control. Additional requirements may be added during the Land Disturbance Permit review process depending on the proposed containment and CBMPP related to for water quality issues related to runoff. 3) A complete set of construction plans for the site work – including, but not limited to, drainage, utilities, grading, storm water detention systems, paving, and all above ground structures, will be required to be submitted with the Land Disturbance permit. These plans are to be submitted and approved prior to beginning any of the construction work. 4) According to the FEMA flood map information, this property is located within a Special Flood Hazard Area. You will need to show and label the flood hazard area(s) on PUD Site Plan. Also, you will need to list the Minimum Finished Floor Elevation (MFFE) for the Lot. 5) No Land Disturbance Permit will be issued until any on-site remediation is complete. 6) Must comply with all Engineering Department Policy Letters: A) 5-13-2009 Policy Letter(Car wash drains and dumpster pads to drain to Sanitary Sewer System); B) 8-4-2004 Policy Letter (Video inspection of new Storm Sewer System Piping); C) 3-18-2004 Policy Letter (Additional subdivision street requirements).);*
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- 9) Compliance with Fire comments (*All projects within the City of Mobile Fire Jurisdiction must comply with the requirements of the 2009 International Fire Code, as adopted by the City of Mobile.*);
- 10) Development of the site must comply with all local, state and federal regulations regarding development within a flood zone;
- 11) Revision of the site plan to indicate which portions of the on-site access road and parking area will be paved in asphalt, concrete or similar, versus aggregate;
- 12) Depiction of concrete wheel stops in the parking area if no curb-and-gutter will be provided;
- 13) Submission of a revised site plan to the Planning Section prior to the signing of the final plat; and

- 14) Full compliance with all other municipal codes and ordinances, including the obtaining of the appropriate permits for land disturbance and new construction.

**Subdivision:** The request is recommended for Tentative Approval, subject to the following conditions:

- 1) Revision, if necessary, to label the correct size of the lot in square feet and acres;
- 2) Revision of the plat to remove or label all abutting vacated rights-of-way, as appropriate;
- 3) Revision of the plat to label the abutting railroad as CSX;
- 4) Placement of a note on the plat stating that the lot is limited to one curb-cut to Virginia Street, with the size, design and location to be approved by Traffic Engineering, and to comply with AASHTO standards;
- 5) Full compliance with all local, state and federal regulations regarding development within a flood zone;
- 6) Compliance with Traffic Engineering comments (*Driveway number, size, location and design to be approved by Traffic Engineering and conform to AASHTO standards.*);
- 7) Compliance with Engineering comments (*The following comments should be addressed prior to acceptance and signature by the City Engineer: 1) Provide all of the required information on the Plat ( i.e. signature blocks, signatures, required notes). 2) Add a note to the Plat stating that storm water detention will be required for any existing development (since 1984) that did not receive Land Disturbance permitting and any future addition(s) and/or land disturbing activity in accordance with the Storm Water Management and Flood Control Ordinance (Mobile City Code, Chapter 17 , Ordinance #65-007 & #65-045). 3) Add a signature block for the Owner, Notary, Planning Commission and Traffic Engineer. 4) Provide and label the monument set or found at each subdivision corner. 5) Show and label the existing parcel lines. 6) Provide the Surveyor's, Owner's (notarized), Planning Commission, and Traffic Engineering signatures. 7) Add a note to the Plat stating that the approval of all applicable federal, state, and local agencies (including all stormwater runoff, wetland and floodplain requirements) would be required prior to the issuance of a permit for any land disturbance activity.*);
- 8) Compliance with Urban Forestry comments (*Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).*);
- 9) Compliance with Fire comments (*All projects within the City of Mobile Fire Jurisdiction must comply with the requirements of the 2009 International Fire Code, as adopted by the City of Mobile.*); and
- 10) Submission of revised PUD and Planning Approval site plans prior to the signing of the final plat.

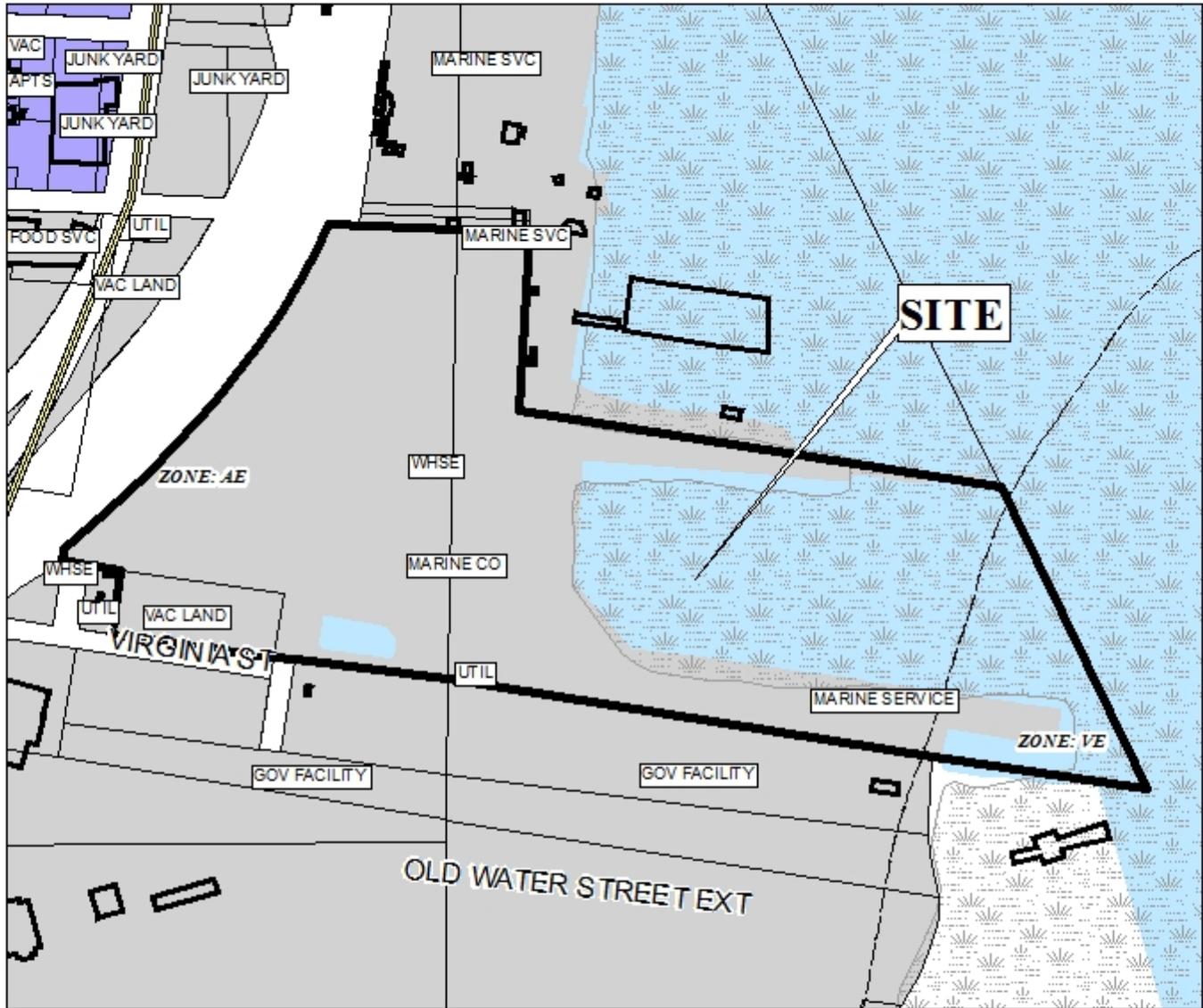
# LOCATOR MAP



APPLICATION NUMBER 25, 26 & 27 DATE September 5, 2013  
APPLICANT Blue Creek Coal Terminal  
REQUEST Subdivision, Planning Approval, PUD



# PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The site is surrounded by industrial land use units.

APPLICATION NUMBER 25, 26 & 27 DATE September 5, 2013

APPLICANT Blue Creek Coal Terminal

REQUEST Subdivision, Planning Approval, PUD

LEGEND															
	R-1	R-2	R-3	R-A	R-B	H-B	T-B	B-1	LB-2	B-2	B-3	B-4	B-5	I-1	I-2



# PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



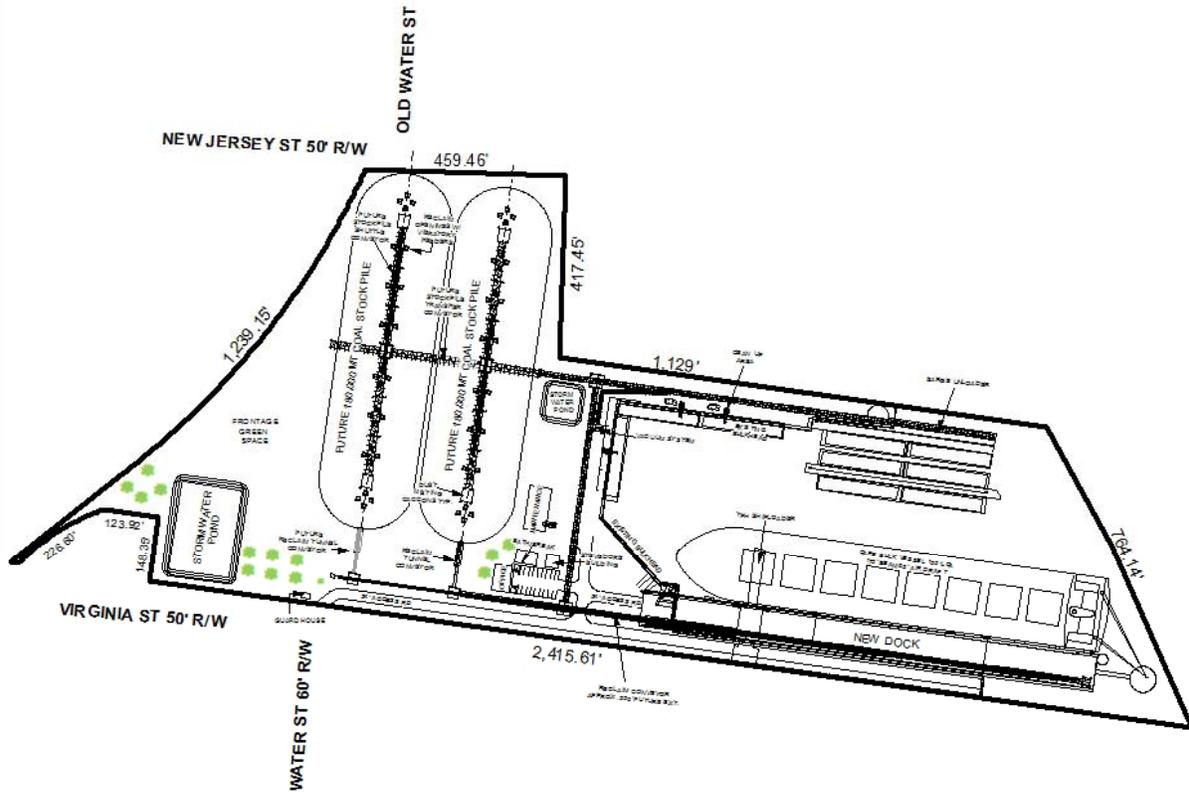
The site is surrounded by industrial land use units.

APPLICATION NUMBER 25, 26 & 27 DATE September 5, 2013  
APPLICANT Blue Creek Coal Terminal  
REQUEST Subdivision, Planning Approval, PUD



NTS

# SITE PLAN



The site illustrates the future coal stock piles and transfer conveyor, new dock, future reclaim tunnel conveyor, water ponds, and buildings.

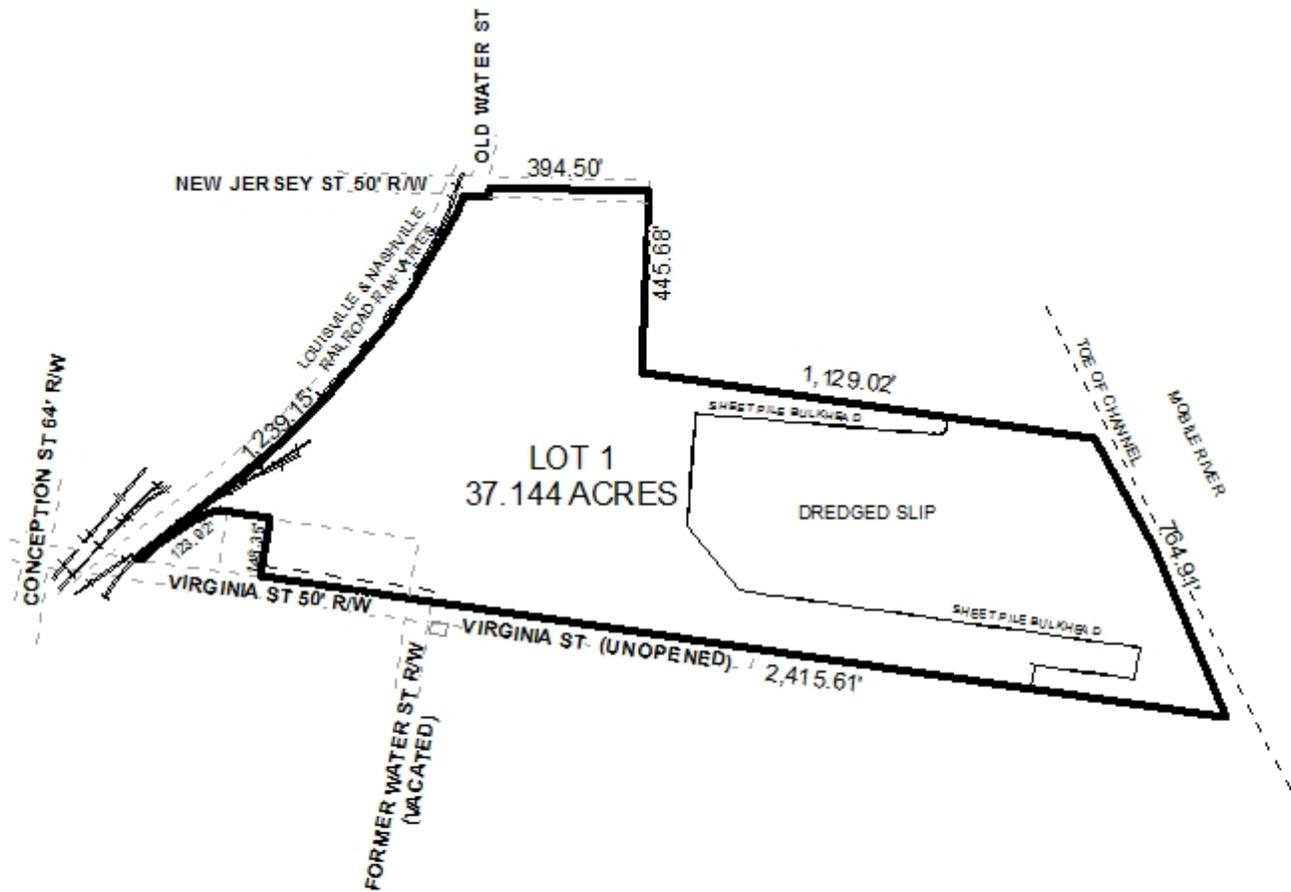
APPLICATION NUMBER 25, 26 & 27 DATE September 5, 2013

APPLICANT Blue Creek Coal Terminal

REQUEST Subdivision, Planning Approval, Planned Unit Development



# DETAIL SITE PLAN



APPLICATION NUMBER 25, 26 & 27 DATE September 5, 2013

APPLICANT Blue Creek Coal Terminal

REQUEST Subdivision, Planning Approval, PUD

