

**PLANNED UNIT DEVELOPMENT  
STAFF REPORT****Date: November 1, 2012****DEVELOPMENT NAME**

DAA Acquisitions of Alabama

**LOCATION**3030 McVay Drive North  
(East side of McVay Drive North, 2/10± mile South of U.S.  
Highway 90).**CITY COUNCIL  
DISTRICT**

District 4

**PRESENT ZONING**

B-3, Community Business District

**AREA OF PROPERTY**

1 Lot / 13.8± Acres

**CONTEMPLATED USE**Planned Unit Development Approval to amend a  
previously approved Planned Unit Development to allow a  
reduction in the front landscaping requirements.**TIME SCHEDULE  
FOR DEVELOPMENT**

January, 2013

**ENGINEERING  
COMMENTS**

No comments.

**TRAFFIC ENGINEERING  
COMMENTS**Driveway number, size, location, and design to be  
approved by Traffic Engineering and conform to AASHTO standards.**URBAN FORESTRY  
COMMENTS**Property to be developed in compliance with state and local  
laws that pertain to tree preservation and protection on both city and private properties (State Act  
61-929 and City Code Chapters 57 and 64).**FIRE DEPARTMENT****COMMENTS**All projects within the City of Mobile Fire Jurisdiction  
must comply with the requirements of the 2009 International Fire Code, as adopted by the City  
of Mobile.**REMARKS**The applicant is requesting Planned Unit Development  
approval to amend a previously approved Planned Unit Development to allow a reduction in the  
front landscaping requirements, and to allow the expansion of the parking area.

The site is used for automobile auctions. In September, 2010, a PUD was approved for the site to allow multiple buildings on a single building site. In December, 2011, an amended PUD application was submitted to allow a two-phase expansion of the site, and to allow a reduction in front landscaping requirements. However, only Phase One, a 210-space inventory vehicle parking expansion was approved, and its development has been completed. The area originally proposed as Phase Two was not approved and was revised to be frontage landscaping area along McMae Drive. The applicant now proposes to pursue approval for Phase Two to include 195 additional parking spaces for inventory vehicles and a future 6,000 square-foot building to be used for vehicle detailing and repairs and office space. The previous PUD indicated 116 inventory parking spaces for Phase Two plus the future building. As the building is indicated to be future and not part of the current request, and is indicated within the area of the proposed parking expansion, a further PUD will be required when construction is proposed for that building.

Pertaining to the request to reduce the front landscaping requirements, the 2011 PUD approval allowed the reduced landscaping requirement for the site with a condition being *“revision of the site plan to depict landscaping islands in the areas currently shown as striped paving (hatched) on the site plan in Phase One”*. That revision was made to the site plan and such is incorporated into the site plan submitted with this application. However, the approved 2011 site plan also indicated 15,810 square feet of frontage landscaping along McMae Drive for Phase Two, but the applicant now proposes only 8,066 square feet of frontage landscaping along McMae Drive. The site is required to have 43,124 square feet of frontage landscaping and was approved with 29,310 square feet (68% of the required) by the previous PUD. The applicant now proposes to provide 21,566 square feet, or only 50% of the required total. The site would require 71,874 square feet of total landscaping, and 122,889 square feet are provided.

Planned Unit Development review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating excess traffic along minor residential streets in residential districts outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD. PUD approval is site plan specific, thus if any new construction is anticipated that will change an approved site plan, an application to amend an existing, approved PUD must be made prior to any construction activities.

It is stated by the applicant that the approval of the landscaping requirement reduction is needed in order to provide an adequate area to develop to meet inventory vehicle parking needs. It is noted that McMae Drive is a dead end cul-de-sac with a minimal 199 linear feet of frontage and due to this, useable property in this location is very limited. The applicant states that restricting the Phase Two development to meet the front landscaping requirements creates a hardship by denying needed additional inventory parking space and the proposed future service building.

It should be noted that the site plan does not incorporate two conditions of approval of the previous PUD. One required placement of a note on the site plan stating that the placement of

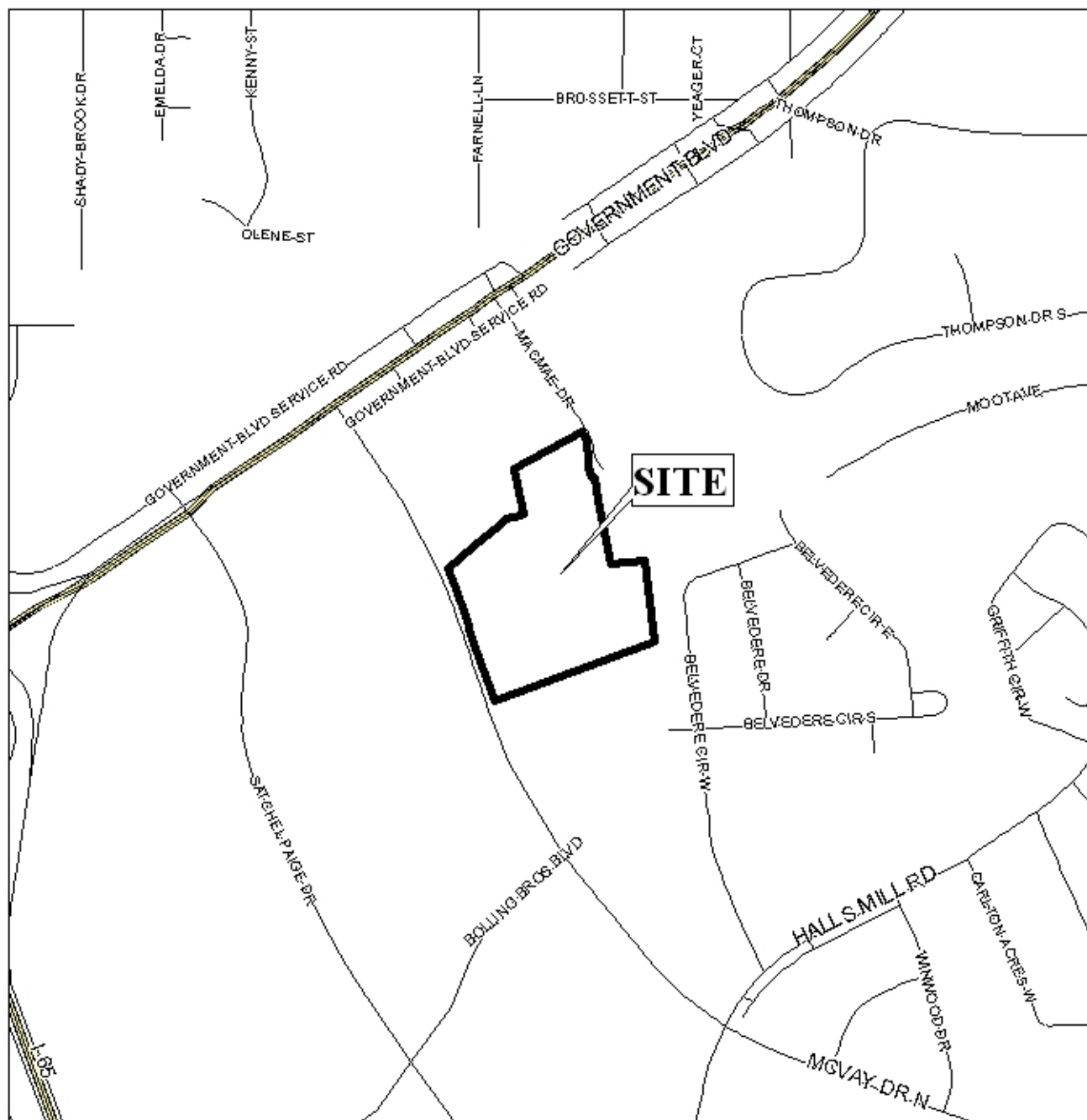
additional dumpsters on the site will require a new PUD submittal. The other required compliance with the Traffic Engineering comments: *“Driveway on McMae Drive should be limited to 24’ wide. Radii for McMae Drive driveway are not illustrated on the site plan. A 20’ radius is typical for passenger cars and a 30’ radius is desirable for large trucks”*. Neither the dumpster note nor the revision of the McMae Drive driveway to a 24’ width is indicated.

As proposed, the site will be over-developed and the previous PUD approval did make an exception to the frontage landscaping requirement in spite of this fact. The applicant was aware of the landscaping requirement with the previous PUD approval and revised the site plan to indicate the removal of the proposed future building and provide a landscaped area between the main area of the site and McMae Drive. The applicant now wants to further reduce the landscaping requirement. To allow a further reduction in the requirement would go against the intent of the minimum landscape requirements of the Ordinance to protect and enhance the ecological and aesthetic environments of the City of Mobile, more specifically, to off-set the effects of large impervious surfaces and to provide an attractive appearance to streets.

**RECOMMENDATION**

**Planned Unit Development:** Based upon the preceding, the request to amend a previously approved Planned Unit Development to allow a further reduction in the front landscaping requirements is recommended for denial.

## LOCATOR MAP



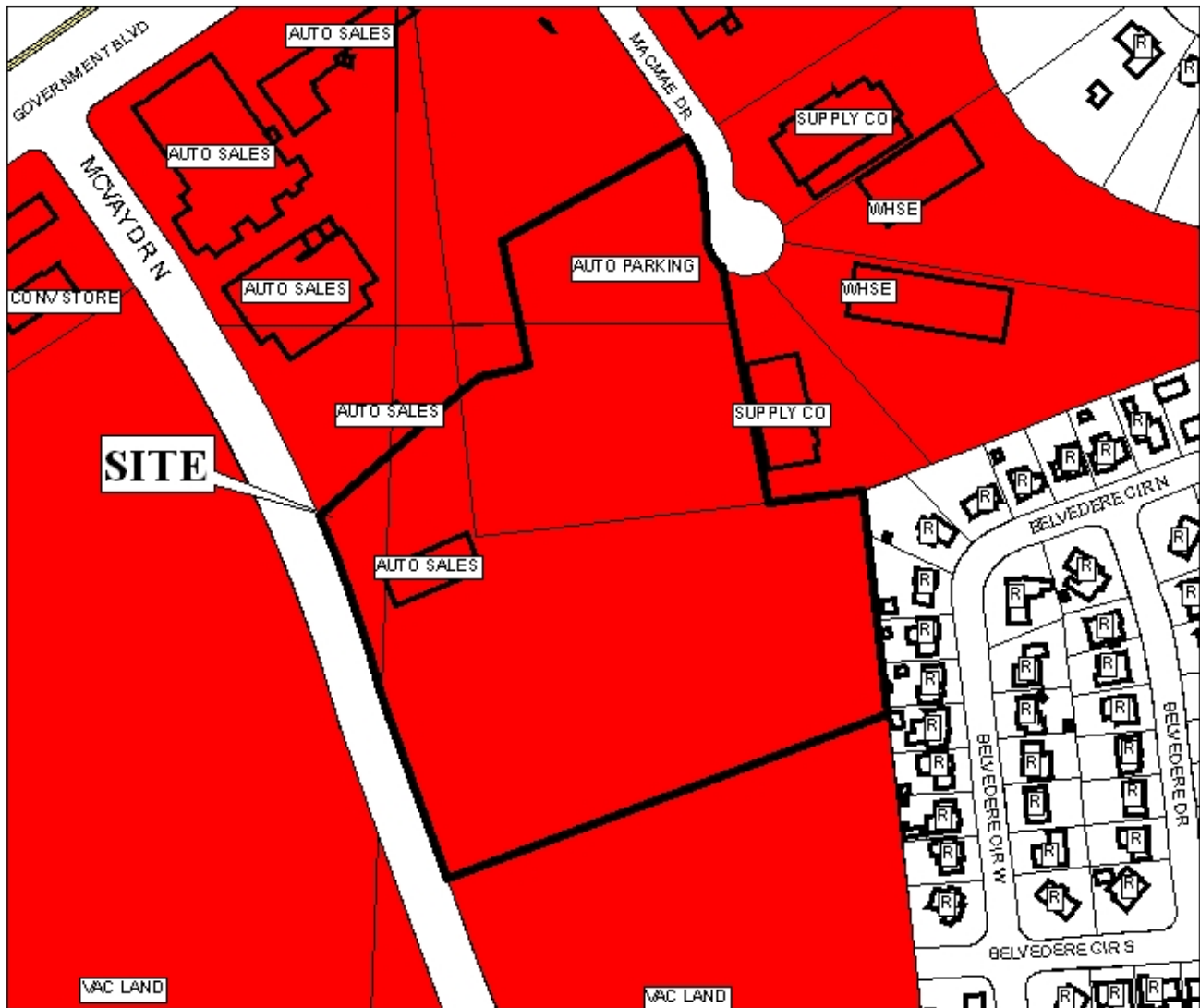
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# PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



Single-family residential units are located to the east of the site. Business land use is located to the north of the site.

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LEGEND



## PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



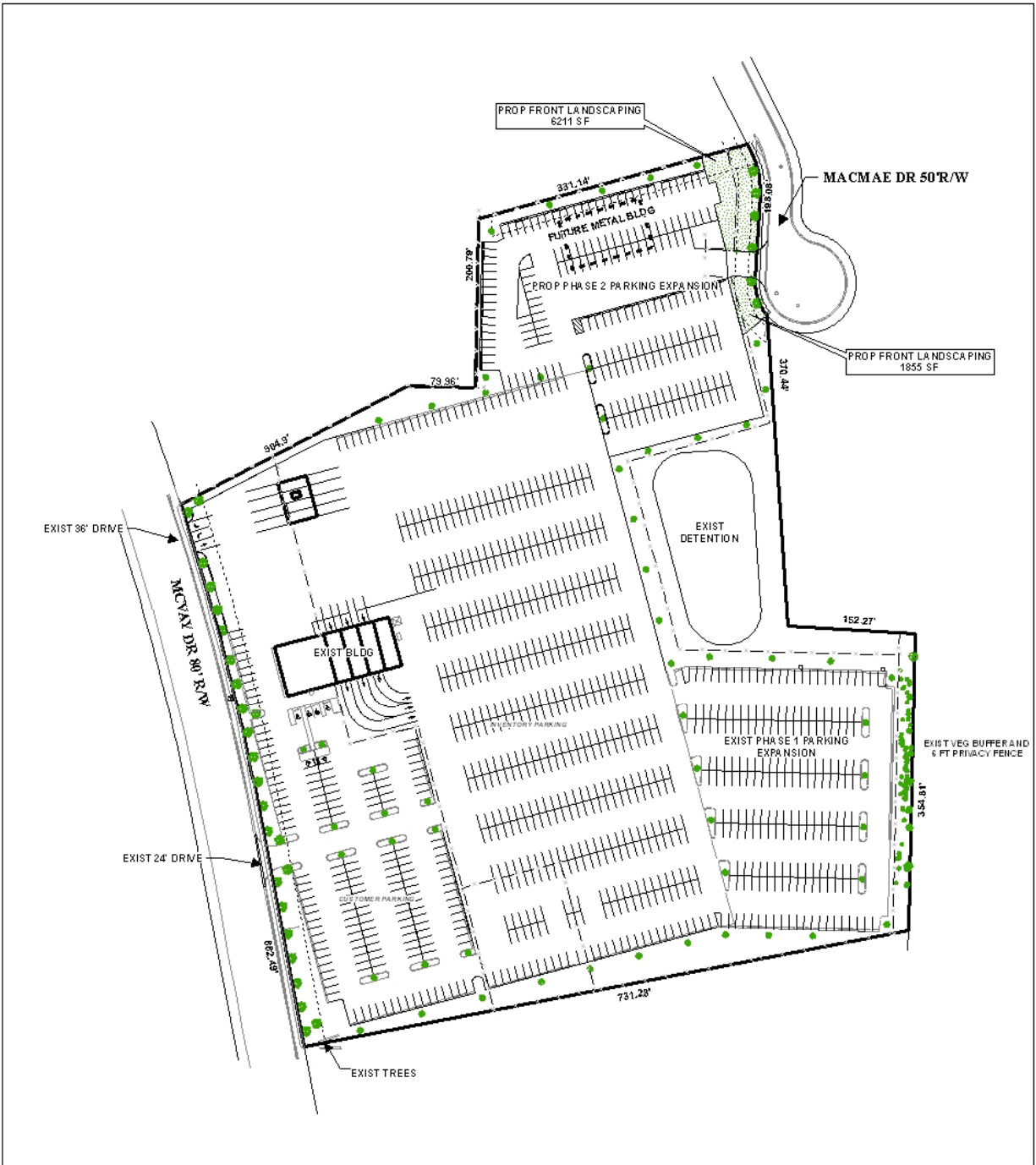
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# SITE PLAN



The site plan illustrates the existing development and proposed parking expansion.

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