

**SUBDIVISION, PLANNED UNIT DEVELOPMENT  
& SIDEWALK WAIVER STAFF REPORT****Date: April 3, 2008**

**APPLICANT NAME** Austal USA, LLC (Dees Engineering Services, Inc., Agent)

**SUBDIVISION NAME** Austal USA Modular Manufacturing Facility (MMF) Subdivision

**LOCATION** Southeast corner of Addsko Road and Dunlap Drive, extending to the North side of Pinto Pass.

**CITY COUNCIL DISTRICT** District 1

**PRESENT ZONING** I-2, Heavy Industry

**AREA OF PROPERTY** 1-Lot/102.6± acres

**CONTEMPLATED USE** Subdivision approval to create 1 lot, Planned Unit Development approval to allow multiple buildings on a single building site and sidewalk waiver to waive the construction of sidewalks along Dunlap Drive and Addsko Road.

**TIME SCHEDULE FOR DEVELOPMENT** Immediately

**ENGINEERING  
COMMENTS**

Unless a CLOMAR is approved by FEMA removing the VE designation from the property, no grading or fill allowed in the VE zone. Show the BFE and minimum FFE on the plans and the plat. Must comply with all storm water and flood control ordinances. It is the responsibility of the applicant to look up the site in the City of Mobile (COM) GIS system and verify if NWI wetlands are depicted on the site. If the COM GIS shows wetlands on the site, it is the responsibility of the applicant to confirm or deny the existence of wetlands on-site. If wetlands are present, they should be depicted on plans and/or plat, and no work/disturbance can be performed without a permit from the Corps of Engineers. Any work performed in the right of way will require a right of way permit.

**Sidewalk waiver:** Applicant did not provide any information that a City standard sidewalk could not be constructed.

**TRAFFIC ENGINEERING  
COMMENTS**

Driveway number, size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards. A traffic impact study

was submitted for this application. Recommendations from the study include the installation of a traffic signal at the intersection of Addisco Road and U.S. 90/98. The developer will be responsible for the cost of installation and will installation will have to be approved by ALDOT.

## **URBAN FORESTRY**

### **COMMENTS**

Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

## **FIRE DEPARTMENT**

### **COMMENTS**

Shall comply with 2003 IFC Section 503; 508.5.1 and appendix D.

## **REMARKS**

The applicant is requesting Subdivision approval to create a legal lot of record from a large metes and bounds parcel, Planned Unit Development approval to allow multiple buildings on a single building site and sidewalk waiver to waive the construction of a sidewalk Dunlap Drive.

The existing site is currently undeveloped and the applicant (Austal USA) proposes a new modular manufacturing facility (MMF) shipyard. The MMF facility construction will be in separate phases. Each phase will produce a complete element that is immediately implemented to create efficient improvement needed to implement the Advanced Shipbuilding (ASB) process required by the U.S. Navy.

Phase I, First half of MMF construction proposes constructing 50% of the initial MMF project, as depicted would allow two of the four modular construction lines to be in service. These two lines would support the construction of fully outfitted modules and the joining of grand blocks at the MMF facility for transport to the assembly bay. Assembly bay three would complete construction in time to support future projects anticipated by the applicant. The completion of this project would reduce the time from start of cutting metal to launch of the vessel from 65 weeks to 54 weeks making the shipbuilding process more efficient.

Phase II, Second half of MF construction is the trigger point for the authorization of constructing the remaining 50% of the MMF facility, pending the awarding of four, Littoral Combat Ship (LCS-sized vessels) in a fiscal year. The completion of Phase II would complete the MMF, bringing four modular construction lines in service and would provide for the implementation of the remaining process improvements which will support the construction of four to six LCS-sized vessels per year. The completion of the entire project would reduce the time from start of cutting metal to the launch of the vessel from 54 weeks to 46 weeks.

The site plan illustrates a 700' x 1,000' (700,000 square feet) modular manufacturing facility (MMF), a 200' x 400' (80,000 square feet) warehouse, and a 100' x 280', two-story (28,000 square feet) administrative building. The proposed site illustrates 3,057 parking spaces to accommodate employees at the existing building and the proposed construction. The applicant list the existing number of employees (1,800) and the proposed number of employees of the MMF (1,038), warehouse (12) and the administrative building.

The Zoning Ordinance requires one parking space per three manufacturing/warehouse personnel and one space per 300 square feet of office/retail space. Therefore, the required parking spaces for the completed project would be 1,137 spaces. The site would provide for three times the required parking spaces for this use; however, the Ordinance does not have a maximum number of parking spaces for a site.

Subdivision review examines the site with regard to promoting orderly development, protecting general health, safety and welfare, and ensuring that development is correlated with adjacent developments and public utilities and services, and to ensure that the subdivision meets the minimum standards set forth in the Subdivision Regulations for lot size, road frontage, lot configuration, etc.

The site has frontage onto two streets, Addesco Road (1,000'±), and Dunlap Drive (1,800'±). All the streets meet or exceed the right-of-way requirements of the Major Street Component of the Comprehensive Plan.

Access management is a concern due to the site fronting on two streets; therefore, the development should be limited to three curb cuts to Addesco Road, and to five curb-cuts, to Dunlap Drive, with the size, design and location of all curb-cuts to be approved by Traffic Engineering and conform to AASHTO standards.

If a detention basin and/or common areas are proposed for the subdivision; a note should be placed on the Final Plat, if approved, stating that maintenance of the detention basin common areas, and any other common areas, are the responsibility of the subdivision's property owners.

The applicant is also requesting the waiver of the construction of sidewalks along Dunlap Drive and Addesco Road.

The applicant states that this area is an industrial zone; therefore, not conducive to construct sidewalks within this area.

It should be noted that there have been sidewalk waivers approved located within the immediate area; however, these waivers did not propose workers travel along rights-of-way to and from their parking areas to work.

As Engineering Department states the applicant did not provide any information that a City standard sidewalk could not be constructed.

### **RECOMMENDATION**

**Planned Unit Development:** Based upon the preceding, the PUD request is recommended for Approval, subject to the following conditions:

- 1) completion of the Subdivision process;
- 2) the submission of a revised PUD site plan reflecting compliance with the landscaping and tree requirements of the Zoning Ordinance;
- 3) full compliance with all other municipal codes and ordinances.

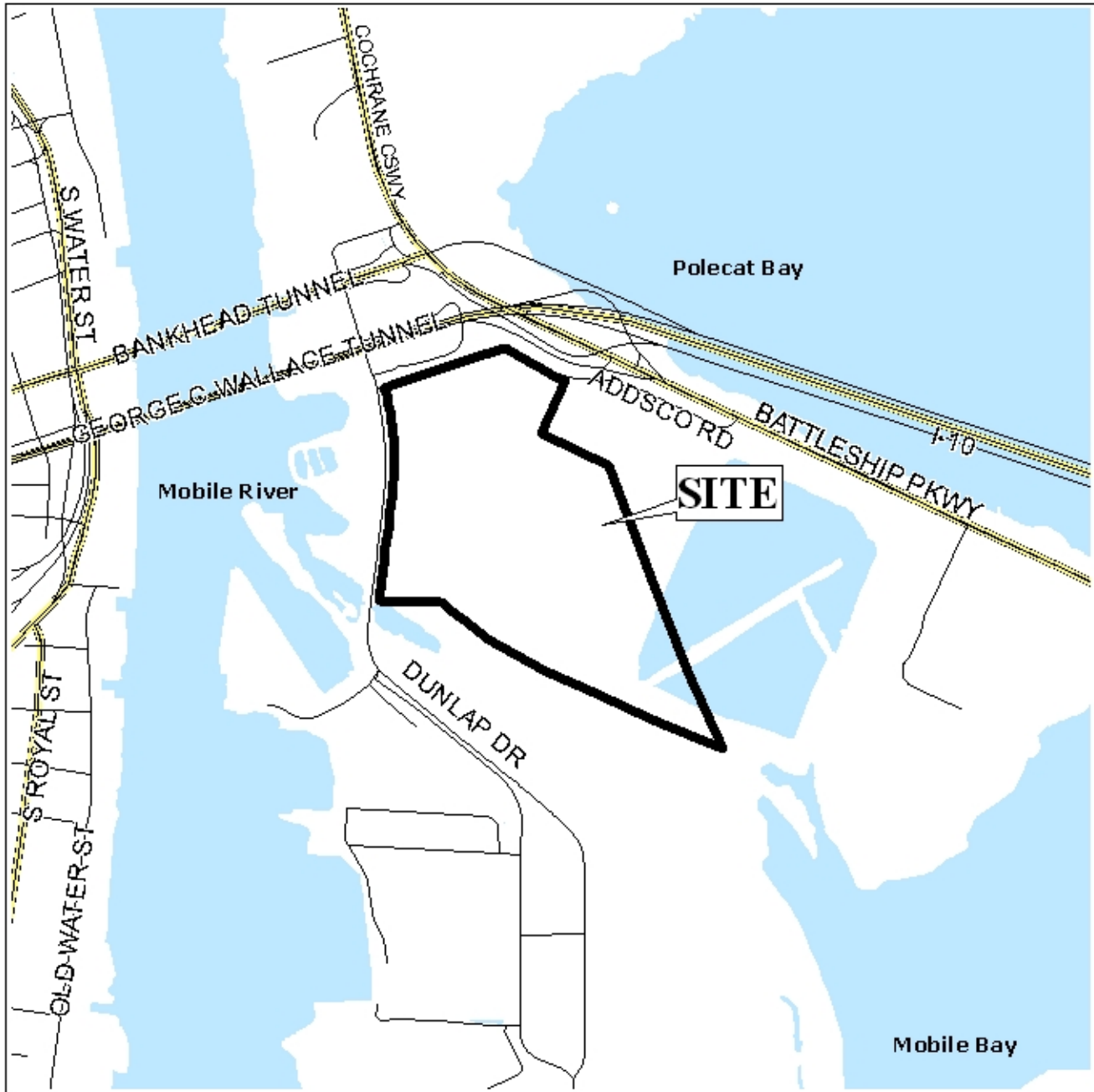
**Subdivision:** Based on the preceding, the application is recommended for Tentative Approval, subject to the following conditions:

- 1) placement of a note on the final plat stating that the development is limited to three curb cuts along Addsko Road, and five curb-cuts, to Dunlap Drive, with the size, design and location of **all** curb-cuts to be approved by Traffic Engineering and conform with AASHTO standards.
- 2) the labeling of the lot with its size in square feet, or placement of a table on the plat with the same information;
- 3) placement of a note on the Final Plat stating that the maintenance of the detention common area is the responsibility of the property owners; and
- 4) full compliance with all other municipal codes and ordinances.

**Sidewalk Waiver:** Based upon the preceding, this application for waiver of the sidewalk along Addsko Road and Dunlap Drive should be denied u, for the following reasons:

- 1) the applicant did not provide any information that a City standard sidewalk could not be constructed.

## LOCATOR MAP



APPLICATION NUMBER 21 & 22 & 23 DATE April 3, 2008

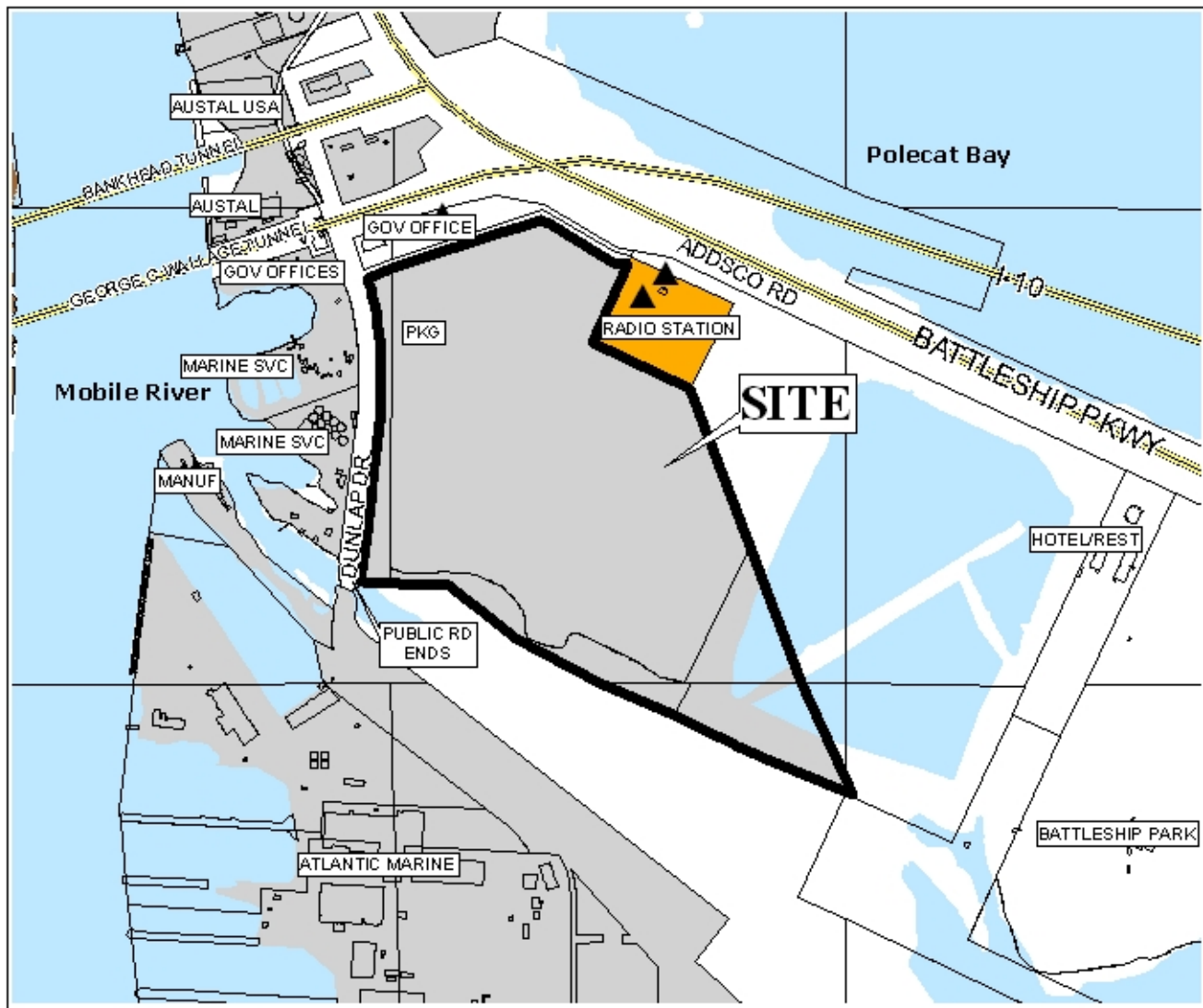
APPLICANT Austal USA, LLC (Dees Engineering Services, Inc., Agent)

REQUEST Planned Unit Development, Sidewalk Waiver, Subdivision



NTS

# PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The site is surrounded by industrial and commercial land use. A park is located to the east of the site.

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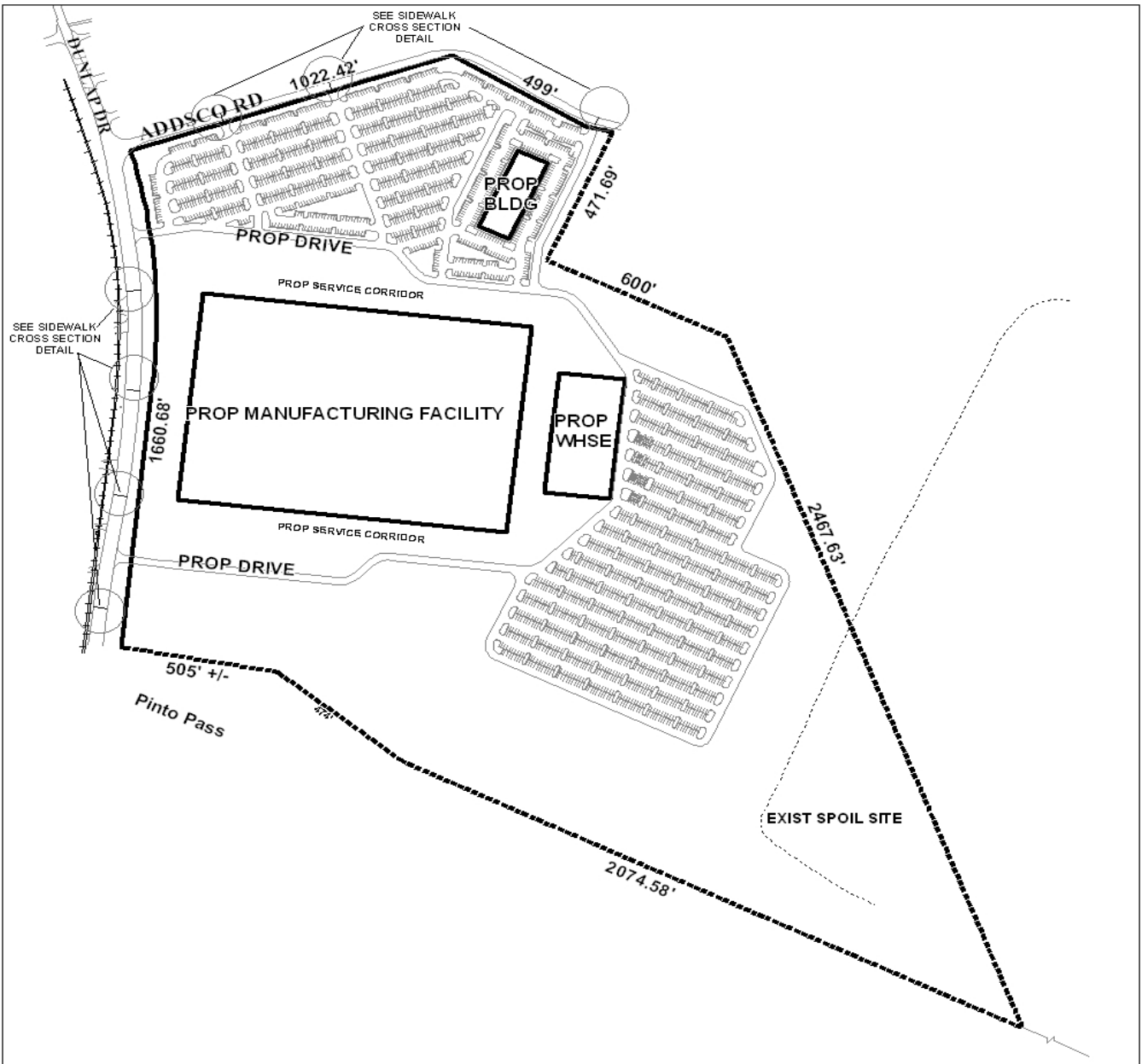
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LEGEND

R-1	R-2	R-3	R-A	R-B	H-B	T-B	B-1	LB-2	B-2	B-3	B-4	B-5	I-1	I-2
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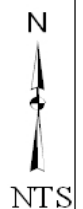
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# SITE PLAN



The site plan illustrates the proposed buildings and parking

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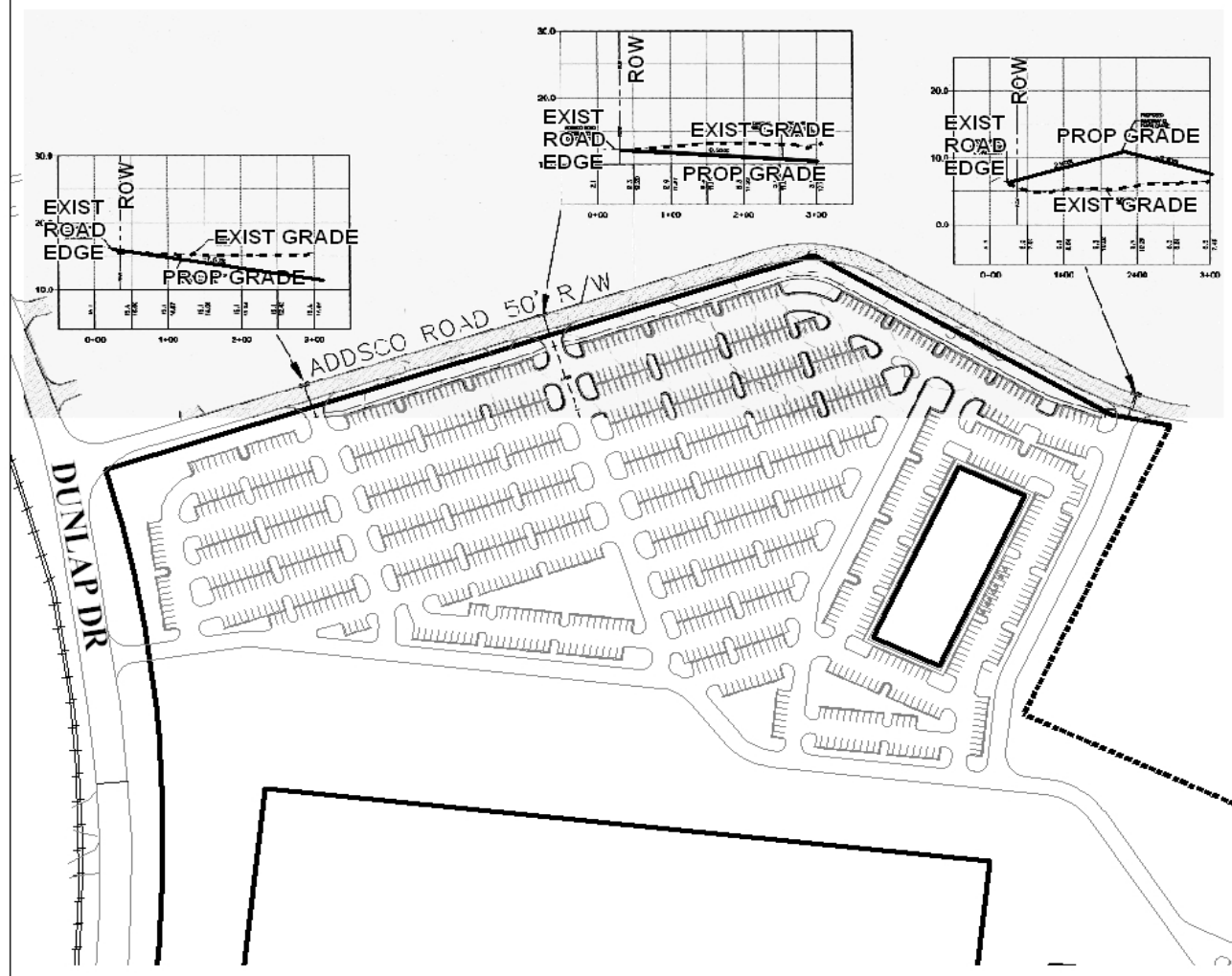
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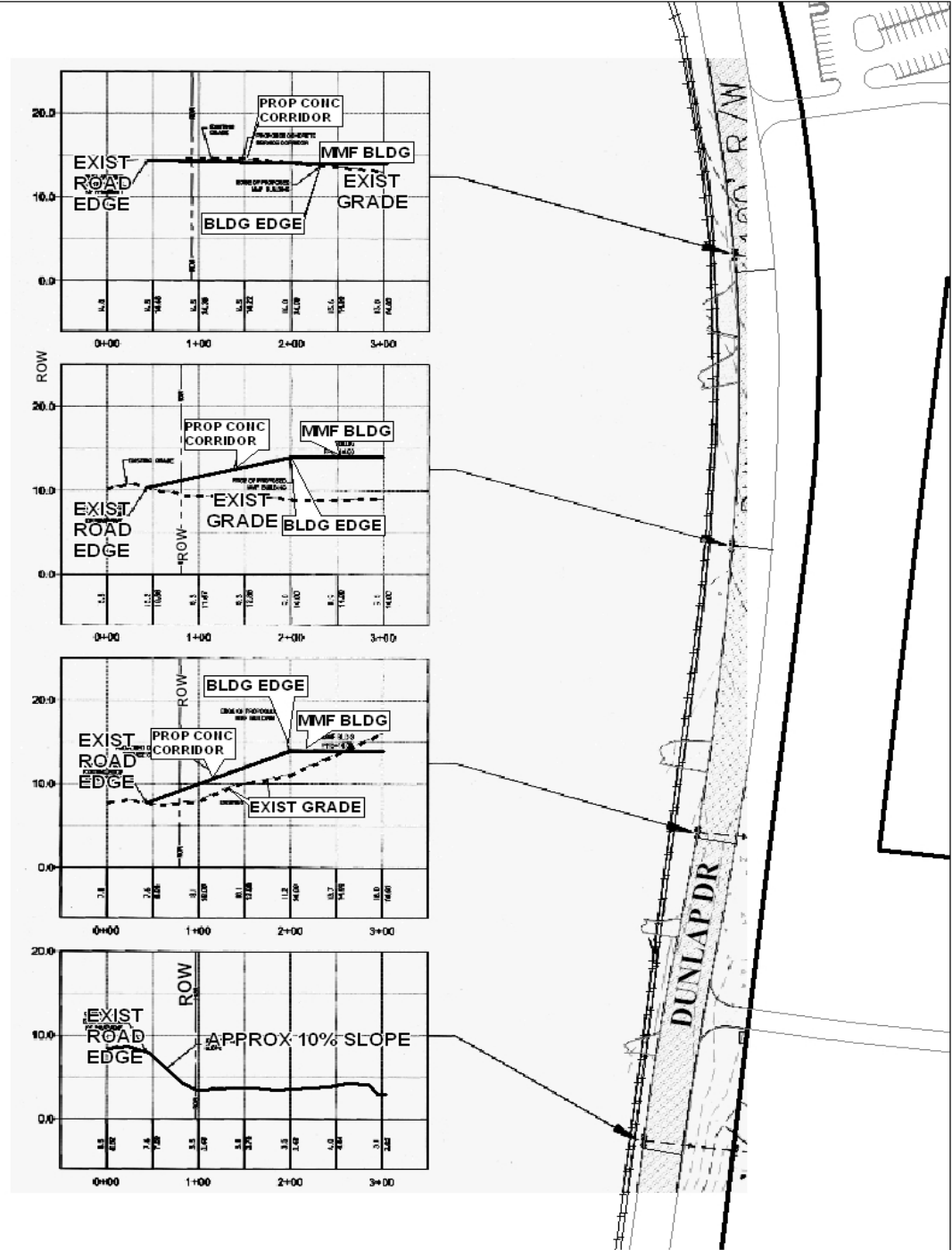
# DETAIL SITE PLAN



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# SIDEWALK CROSS SECTION DETAIL



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