

HOLDOVER
REVISED
COASTAL WATERS FEDERAL
CREDIT UNION SUBDIVISION

Engineering Comments: Development of property must comply with all stormwater management and flood control ordinances. Any work performed in the right-of-way will require a right-of-way permit.

Traffic Engineering Comments: Driveway number, size, location and design to be approved by Traffic Engineering and conform to AASHTO standards. *The handicap parking shown on the site plan should be relocated to prevent vehicles from backing directly into Springhill Avenue and to allow the drive-thru traffic access to the proposed twenty-four foot wide driveway. This would also eliminate the proposed twelve foot wide driveway which is only twenty feet east of the main driveway.*

The plat illustrates the proposed 1-lot, 0.4 ± acre subdivision which is located on the North side of Spring Hill Avenue, 100'± West of Pine Street. The site is served by city water and sanitary facilities.

The purpose of this application is to create a one-lot subdivision from two metes and bounds parcels.

As a means of access management, a note should be placed on the final plat stating that the development is limited to one curb cut to Spring Hill Avenue with the size, location and design to be approved by Traffic Engineering.

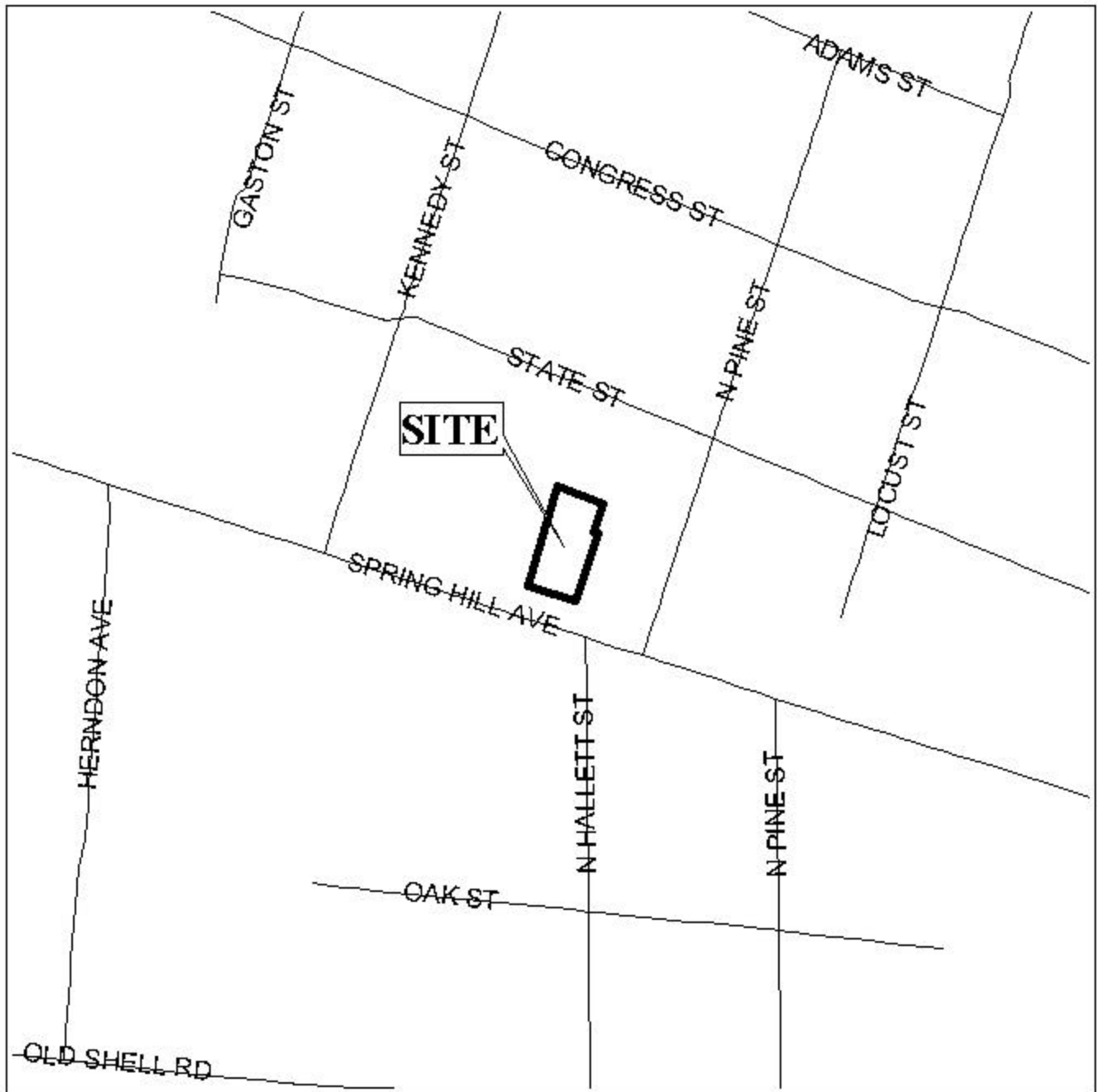
This is a commercial site which abuts residentially zoned property; therefore, the provision of a buffer in compliance of Section V.A.7 of the Subdivision Regulations should be required.

The 25-foot minimum building setback line is not shown but would be required on the final plat.

The Commission heldover this application to allow the applicant time to resubmit a traffic plan that illustrates the circulation and parking associated with the proposed addition.

The plat meets the minimum requirements of the Subdivision Regulations and is recommended for Tentative Approval subject to the following condition: 1) the placement of a note on the final plat stating that the development is limited to one curb to Spring Hill Avenue, with the size, location and design to be approved by Traffic Engineering; 2) the placement of a note on the final plat stating that a buffer, in compliance with Section V.A.7. will be provided where the site adjoins residentially developed property; and 3) placement of the required 25-foot minimum building setback line on the final plat.

LOCATOR MAP



APPLICATION NUMBER 11 DATE October 2, 2003

APPLICANT Coastal Waters Federal Credit Union Subdivision

REQUEST Subdivision



NTS

COASTAL WATERS FEDERAL CREDIT UNION SUBDIVISION



APPLICATION NUMBER 11 DATE October 2, 2003

LEGEND



NTS