

**PUD &  
SUBDIVISION STAFF REPORT****Date: March 18, 2004**

**DEVELOPMENT NAME** Sollie Road Development Subdivision

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**LOCATION** East side of Sollie Road, 400'± North of the East terminus of Isle of Palms Drive, extending to the East terminus of Raleigh Boulevard.

**PRESENT ZONING** R-1, Single-Family Residential

**AREA OF PROPERTY** 70.3± Acres 152 Lots

**CONTEMPLATED USE** Single-Family Residential Subdivision with Alleyway Access

**TIME SCHEDULE** Immediate

**ENGINEERING****COMMENTS**

All alleys must be specified as private and to be maintained by the property owners. Must comply with all stormwater and flood control ordinances. Any work performed in the right of way will require a right of way permit.

**TRAFFIC ENGINEERING****COMMENTS**

Driveway number, sizes, location and design to be approved by Traffic Engineering and conform to AASHTO standards.

**URBAN FORESTRY****COMMENTS**

Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (City Code Chapters 57 and 64 and State Act 61-929).

**REMARKS**

The applicant is proposing development of the site as a 152 lot, single-family residential subdivision with alley (shared) access. Planned Unit Development Approval is required to allow the alleys.

Applications for this development were presented to the Commission at the February 19<sup>th</sup> meeting. After discussion of several points of concern (including density, flood zone, access, alley widths, and fencing), the applications were denied. The applicant has resubmitted the plan and is seeking reconsideration of the proposal.

As the proposal is the same, much of the staff report remains unchanged. However, to address concerns raised at the meeting by both the Commission and opposition, more detailed information is provided in this report.

Sollie Road, which has a variable right-of-way, is a major street as shown on the Major Street Plan and as such requires a minimum right-of-way of 100' and may require either a 40' parallel service road or some alternative method of access management.

Based on the plat submitted, it appears that some dedications have been acquired along Sollie Road. However, there is not a continuous 50' from centerline right-of-way; therefore, dedications necessary to provide 50' from centerline for the entire site should be provided.

Planned Unit Development review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating excess traffic along minor residential streets in residential districts outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD.

The lot sizes proposed range from 7,200 sq.ft., which meet the minimum size requirements of the Subdivision Regulations and the Zoning Ordinance for single-family residential properties, to lots that are 10,800 sq. ft., which exceed the minimum. There are several parcels that are designated as common areas which will provide open space/playground areas for the residents of the subdivision. Therefore, the development as proposed is in compliance with density standards for single-family residential developments. Additionally, the plat reflects a large area (approximately 8+ acres) of open space along Second Creek. Given the amount of open space/common area provided (over 1/3 of the site), the density of the development is well below that which would be allowed by the Subdivision Regulations and Zoning Ordinance.

The site is bisected by Second Creek and therefore is considered environmentally sensitive. While portions of several lots include areas designated as flood zone AE, the difference between flood plain and floodway should be noted. The floodway is the stream channel and must remain open to permit passage of the base flood. Flood waters are generally deepest and swiftest in the floodway, which is a portion of the floodplain. The 100 yr floodplain (designated Flood zone AE) is usually wider and is inundated during the 100 year flood. A 100-year flood is one that has a 1% chance of happening in any 1 year. Construction in the floodway is not permitted; however, construction in the flood zone may be permitted, but will have to meet certain requirements as specified in the City's Flood Plain Land Use Ordinance. Additionally, many subdivisions within the Commission's jurisdiction contain lots that include areas designated as a flood zone. Furthermore, because the site is considered environmentally sensitive, approvals from federal, state and local agencies will be required prior to the issuance of any permits.

It should be noted that to lessen the impact on the creek and floodplain, significant areas outside the floodplain are being left undeveloped and only one bridge across the creek is to be constructed.

The plat indicates that there is to be only one point of access to Sollie Road. Typically, subdivisions with this number of lots provide more than one point of access. However, in this instance, the site does not lend itself to a second access point; a second bridge would be needed at the South end of the property, otherwise the access would be used by only approximately 30% of the lots or fewer. Furthermore, the plat provides two street stubs to the East, allowing for future development of that property, as well as allowing for an additional access point to Sollie Road when the adjacent property is developed.

Comments were made at the previous meeting relating to streets exceeding the maximum length for a closed end street. The Subdivision Regulations state that ordinarily, closed end (cul-de-sac) streets shall be no longer than 600'. Two of the streets within the subdivision technically exceed this length; however, the wording of the requirement allows the Commission to consider unique circumstances related to site features or design features, and allow longer streets where appropriate. In this instance, the creek and alleyways would allow the Commission to consider and allow the increased length.

The plan generally complies with the requirements for PUD approval.

The applicant's engineer has indicated that the construction standard for the alleys will comply with city standards with regard to base and paving material thickness. However, there will not be curb and gutter for the alleys; drainage inlets will be located in the middle of the paved alleyway. Maintenance of the alleys, as with all common areas (including the undeveloped land adjacent along Second Creek) is to be the responsibility of the property owners association.

As indicated above, the development proposes alleys. The Subdivision Regulations state that in general, alleys will not be approved for residential districts of subdivisions, except as part of an acceptable Traditional Neighborhood Development. A Traditional Neighborhood Development is defined as generally a large-scale development, comprehensively incorporating mixed uses, mixed densities, relatively small lots, a rectilinear street layout and pedestrian-oriented design.

While the proposed development does not have mixed uses, it could be argued that it does have some degree of variety in density (a majority of lots at 7200 sq.ft. and an area of lots that are 33% larger). Given the constraints created by the creek, a true rectilinear street layout is not practical. While the proposed development may not meet a strict interpretation of a TND, the statement is not a mandate but a general qualification, leaving the Commission latitude in cases where a strict interpretation may not be practical.

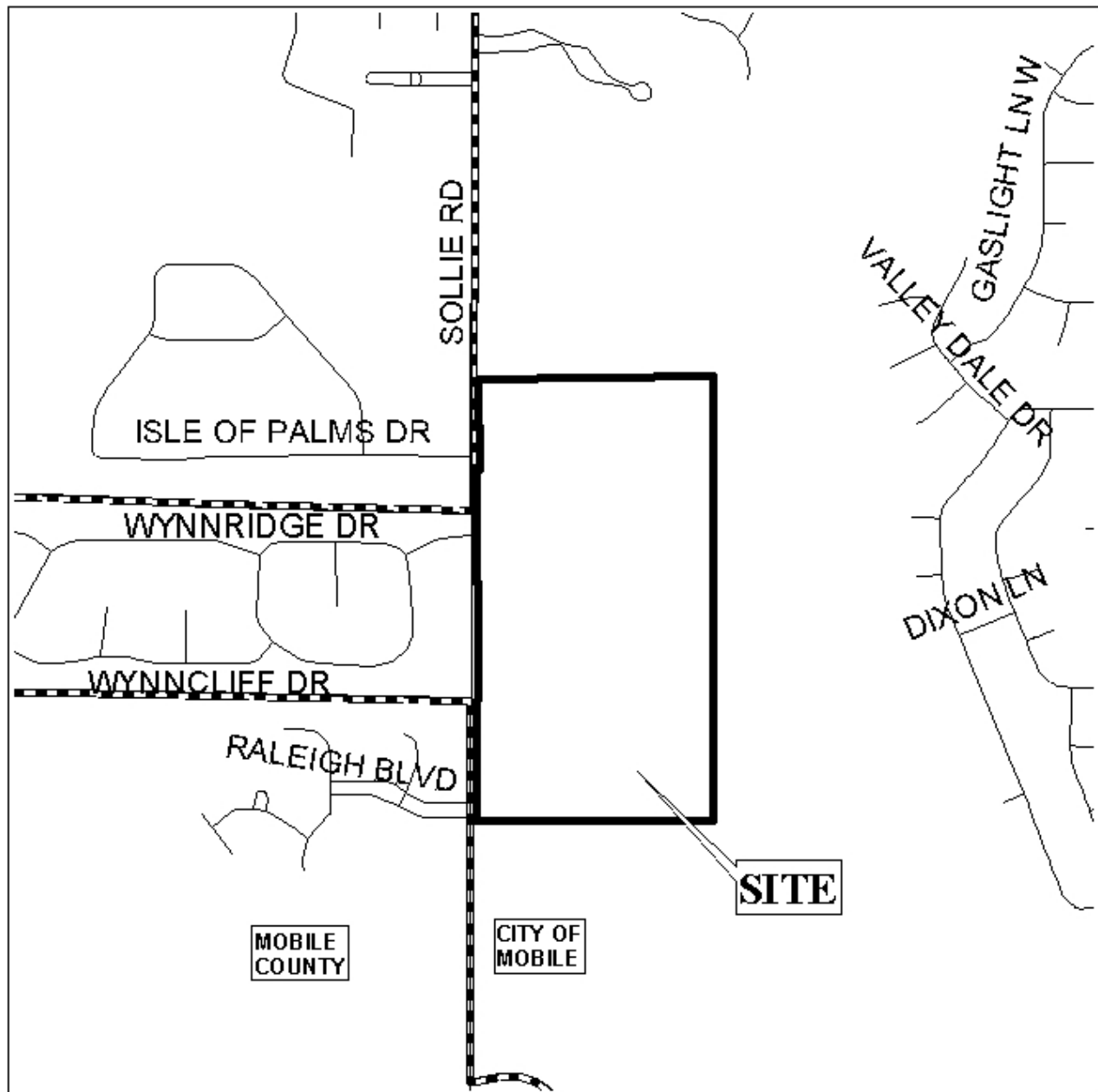
### **RECOMMENDATION**

**Planned Unit Development:** based on the preceding, it is recommended that this application be approved subject to the following conditions: 1) dedication of sufficient right-of-way along Sollie Road to provide a minimum of 50' from centerline; 2) construction standards for the alleys to comply with

city standards with regard to base and paving material, as indicated by the applicant's engineer; 3) developer to obtain all necessary federal, state and local approvals prior to the issuance of any permits; 4) maintenance of the alleys, as with all common areas (including the undeveloped land adjacent along Second Creek) is to be the responsibility of the property owners association; 5) provision of a minimum 6' privacy fence along the Sollie Road frontage, to be setback a sufficient distance from the entrance road to not create a line of sight problem (exact location to be approved by Traffic Engineering); and 6) full compliance with all municipal codes and ordinances.

**Subdivision:** based on the preceding, it is recommended that this application be approved subject to the following conditions: 1) dedication of sufficient right-of-way along Sollie Road to provide a minimum of 50' from centerline; 2) construction standards for the alleys to comply with city standards with regard to base and paving material, as indicated by the applicant's engineer; 3) developer to obtain all necessary federal, state and local approvals prior to the issuance of any permits; 4) placement of a note on the final plat stating that maintenance of the alleys, and all common areas (including the undeveloped land adjacent along Second Creek) is to be the responsibility of the property owners association; and 5) placement of a note on the final plat stating that there shall be no direct access from the alley to Sollie Road.

## LOCATOR MAP



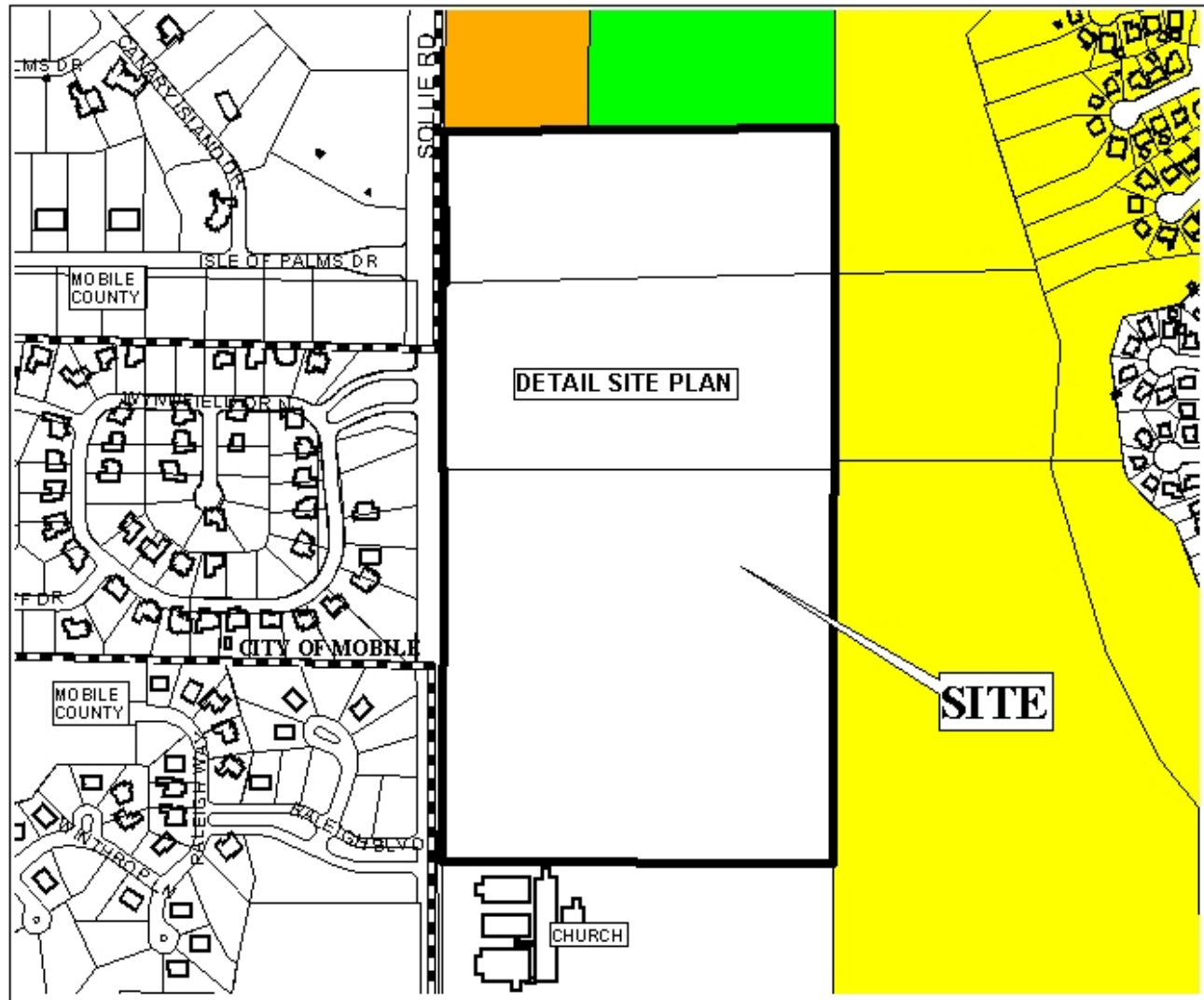
APPLICATION NUMBER 4 & 5 DATE March 18, 2004

APPLICANT Sollie Road Development Subdivision

REQUEST Planned Unit Development, Subdivision



# PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



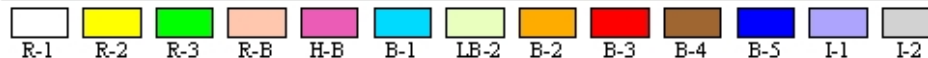
The site is surrounded by single family residential dwellings,  
with a church located to the South.

APPLICATION NUMBER 4 & 5 DATE March 18, 2004

APPLICANT Sollie Road Development Subdivision

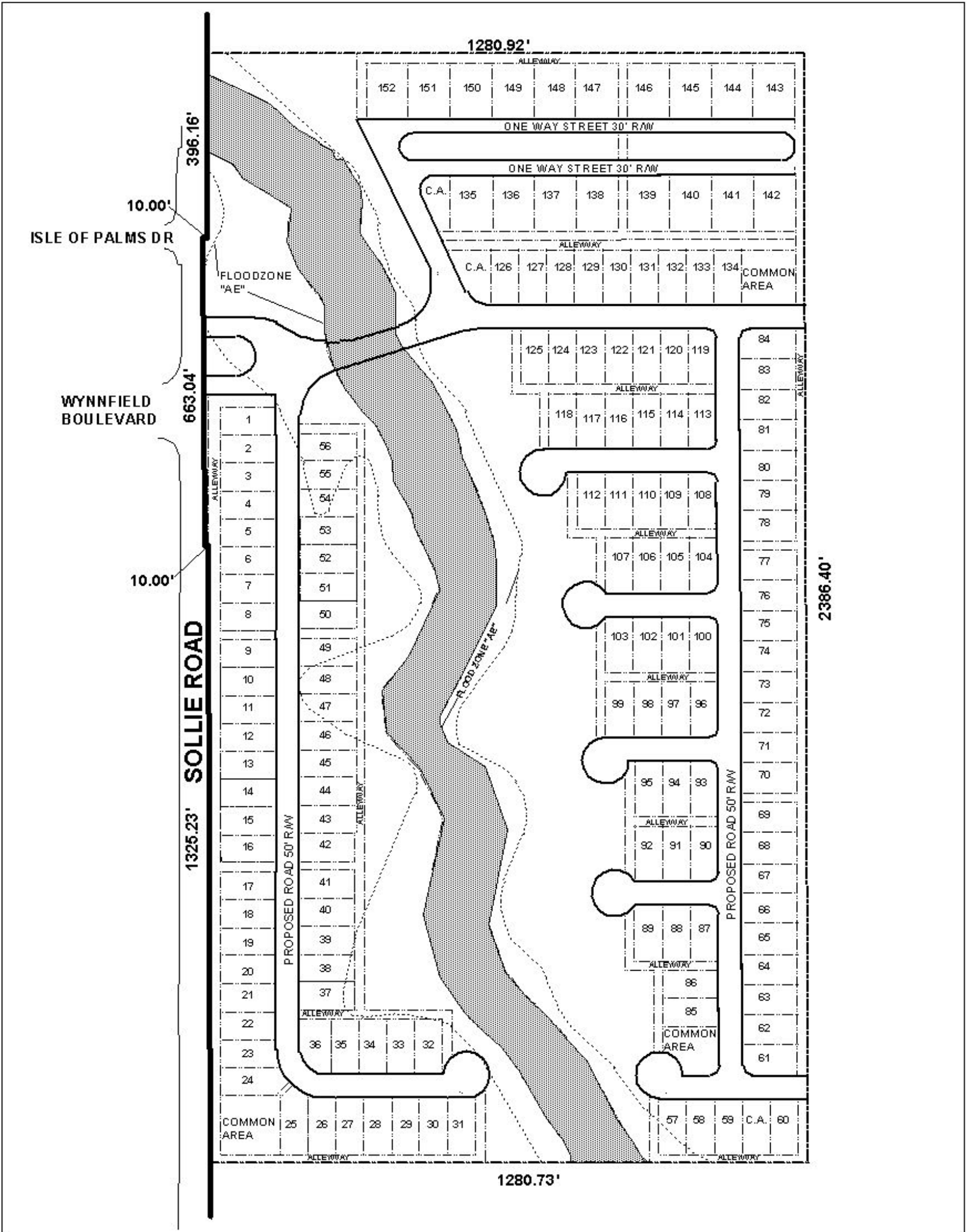
REQUEST Planned Unit Development, Subdivision

LEGEND



NTS

SITE PLAN



The site is located on the East side of Sollie Road, 400' North of the East terminus of Isle of Palms Drive, extending to the East terminus of Raleigh Boulevard. The plan illustrates the subdivision layout.

APPLICATION NUMBER 4 & 5 DATE March 18, 2004  
APPLICANT Sollie Road Development Subdivision  
USE/REQUEST Planned Unit Development, Subdivision

