

**PLANNED UNIT DEVELOPMENT
& SUBDIVISION STAFF REPORT****Date: November 2, 2017****DEVELOPMENT NAME**Rangeline Crossing III Subdivision, Resubdivision of Lots
4 & 5**SUBDIVISION NAME**Rangeline Crossing III Subdivision, Resubdivision of Lots
4 & 5**LOCATION**(North side of Rangeline Road Service Road 904'± East of
Halls Mill Road)**CITY COUNCIL
DISTRICT**

District 4

AREA OF PROPERTY2 Lots / 2.1 ± acres (Subdivision)
7 Lots / 78.3 ± acres (PUD)**CONTEMPLATED USE**Planned Unit Development Approval to amend a
previously approved Planned Unit Development to allow shared access between multiple lots to
accommodate a proposed paint warehouse and retail store.**TIME SCHEDULE
FOR DEVELOPMENT**

None given.

**ENGINEERING
COMMENTS****Subdivision**

A. Provide all of the required information on the SUBDIVISION PLAT (i.e. signature blocks, signatures, certification statements, written legal description, required notes, legend, scale, bearings and distances) that is required by the current Alabama State Board of Licensure for Professional Engineers and Land Surveyors.

B. Provide a note that a 25' riparian buffer may be required, during development, along the edge of anything considered by ADEM to be a water of the state.

C. Provide the recording information for Rangeline Crossing Subdivision III.

D. Provide the Surveyor's, Owner's (notarized), Planning Commission, and Traffic Engineering signatures.

E. Add a note to the plat stating that all easements shall remain in effect until vacated through the proper Vacation process.

F. Provide a copy of the FINAL SUBDIVISION PLAT to the Engineering Dept. for review. No signatures are required on this drawing.

G. After addressing all of the FINAL SUBDIVISION PLAT review comments by the Engineering Dept. provide the ORIGINAL (with all other signatures) and one (1) copy (signatures not required) of the revised Final Plat to the Engineering Department.

Planned Unit Development

1. Any existing or proposed detention facility shall be maintained as it was constructed and approved. The Land Disturbance Permit application for any proposed construction includes a requirement of a Maintenance and Inspection Plan (signed and notarized by the Owner) for the detention facility. This Plan shall run with the land and be recorded in the County Probate Office prior to the Engineering Department issuing their approval for a Final Certificate of Occupancy.

TRAFFIC ENGINEERING**COMMENTS**

A traffic impact study has been prepared for this site. The study includes off-site improvements along Halls Mill Road adjacent to the development and southward to the intersection of Rangeline Road. Any improvements within the Service Road right-of-way or the Rangeline Road right-of-way are subject to ALDOT approval and permitting. Development is contingent upon the completion of off-site improvements as required by the City of Mobile and ALDOT. Additional impact studies or updates to this study will be required as the development changes or more proposed uses are presented to the City. Driveway number, size, location and design are subject to the approved PUD for this site, and shall be approved by ALDOT (where applicable) and Traffic Engineering and conform to AASHTO standards. Required on-site parking, including ADA handicap spaces, shall meet the minimum standards as defined in Section 64-6 of the City's Zoning Ordinance.

The submitted site plan for the PUD does not include the site plan for the previously approved VA property. The proposed Lot 5A has less than 30' of frontage on the (private) Rangeline Crossing Drive. A standard driveway requires 64' of frontage otherwise the driveway will extend over the property line. The proposed layout of the driveway onto Rangeline Crossing Drive is substandard and not recommended. The plan should include the previously approved VA site and its driveway so that the two designs can be clarified with the PUD approval. Based on comments from ALDOT regarding access on the Service Road, the driveway to Rangeline Crossing should be considered as a shared driveway with Lot 4A and may be better suited to split the property line. The shift of the aisle can provide some relief for the driveway on the north side as well. Confirmation of ALDOT driveway approval will be required prior to permitting for site work on each lot. Enlarged Lot B does not clearly indicate whether the westernmost driveway on Rangeline Service Road is included and may conflict with the overall site layout.

Revised for December 7th meeting:

A traffic impact study has been prepared for this site and is an ongoing effort given the dynamics of this development. The study includes off-site improvements along Halls Mill Road adjacent to the development and southward to the intersection of Rangeline Road. Any improvements within the Service Road right-of-way or the Rangeline Road right-of-way are subject to ALDOT approval and permitting. Development is contingent upon the completion of off-site improvements as required by the City of Mobile and ALDOT. Additional impact studies or updates to this study will be required as the development changes or more proposed uses are presented to the City. As of the application, the most recent version of the study submitted to the City is dated August 21, 2017. Updates and changes that may have been made to the study since its submittal must be provided to the City for any final acceptance of the study findings. ALDOT (or its representative) is concurrently reviewing the study for final acceptance by both agencies. A development agreement may be required at ALDOT's request to further outline the terms of the approval of this development.

Driveway number, size, location and design are subject to the approved PUD for this site, and shall be approved by ALDOT (where applicable) and Traffic Engineering and conform to AASHTO standards. Required on-site parking, including ADA handicap spaces, shall meet the minimum standards as defined in Section 64-6 of the City's Zoning Ordinance.

The enlarged plan does not appear to be of the scale reflected on the plan sheet therefore dimensions of driveways, aisles, parking spaces, etc. could not be verified. The revised PUD site plan now appears to include a loading dock on the easternmost lot on Rangeline Service Road. The loading dock appears to obstruct the drive aisle from north to south adjacent to the building. The label on the Lot 4 Specialty Retail should be relocated so that the site layout can be clearly viewed. This arrangement of a drive-thru adjacent to the drive aisle is not desirable (northbound traffic "sandwiched" between two southbound aisles of traffic). Additional separation between the drive aisle and the drive thru is necessary. The westernmost lot appears to include parallel parking that is not clearly identified (looks to be extra long and wide). In conjunction with the drive-thru "bail-out" lane, this area needs to be addressed for right-of-way and functionality, especially if the specialty retail/apparel lot is to share the drive aisle.

With the development of the future parcels on the north side of the private road, the alignment of driveways is strongly encouraged and may be required based on the lot layout and intensity of the proposed development.

URBAN FORESTRY

COMMENTS

Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 2015-116 and City Code Chapters 57 and 64).

FIRE DEPARTMENT

COMMENTS

All projects within the City Limits of Mobile shall comply with the requirements of the City of Mobile Fire Code Ordinance. (2012 International Fire Code)

REMARKS

The applicant is seeking Planned Unit Development Approval to amend a previously approved Planned Unit Development to allow shared access between multiple lots to accommodate a proposed paint warehouse and retail store, and Subdivision Approval to resubdivide 2 existing lots by relocating an interior lot line from Lots 4 and 5 of the previously approved Rangeline Crossing Subdivision III. The site is located in Council District 4, and according to the applicant is served by public water and sanitary sewer.

The site has been given a District Center land use designation, per the recently adopted Future Land Use Plan and Map. The Future Land Use Plan and Map complements and provides additional detail to the Development Framework Maps in the Map for Mobile, adopted by the Planning Commission at its November 5, 2015 meeting.

This designation applies across the city to larger areas of existing mixed-use character or where such character is encouraged. These areas will include moderate to high-density residential (minimum densities of 6 du/ac) in dynamic, horizontal or vertical mixed use environments, to provide a balance of housing and employment.

District Centers generally serve several surrounding neighborhoods and may even have a city-wide or region-wide reach. As such, they are often anchored by a major commercial or institutional employer such as a shopping mall or a medical center.

Depending on location and assigned zoning, residential areas in District Centers may incorporate a mix of housing types, ranging from mid-rise multifamily buildings containing apartments and lofts, to townhouses and detached single-family homes. Major civic cultural institutions and public spaces provide regional and neighborhood destinations.

District Centers should be designed to induce pedestrian activity, with high quality streetscapes connecting the different components of a center as well as the center to its surrounding area. DC districts may be served by transit and include development of an intensity and design that supports transit use.

It should be noted that the Future Land Use Plan and Map components of the Map for Mobile Plan are meant to serve as a general guide, not a detailed lot and district plan. In many cases the designation on the new Future Land Use Map may match the existing use of land, but in others the designated land use may differ from what is on the ground today. As such, the Future Land Use Plan and Map allows the Planning Commission and City Council to consider individual cases based on additional information such as the classification request, the surrounding development, the timing of the request, and the appropriateness and compatibility of the proposed use and, where applicable, the zoning classification.

The purpose of this application, according to the applicant, is as follows:

“The contemplated use of Rangeline Crossing PUD is for a proposed 4,000 sf commercial paint warehouse and retail store.”

Planned Unit Development review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating excess traffic along minor residential streets in residential districts outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD. PUD approval is site plan specific, thus any changes to the site plan / Subdivision plat will require approval by the Planning Commission.

Please note that a Planned Unit Development approval by the Planning Commission expires after one year if no permits are obtained.

Subdivision review examines the site with regard to promoting orderly development, protecting general health, safety and welfare, and ensuring that development is correlated with adjacent developments and public utilities and services, and to ensure that the subdivision meets the minimum standards set forth in the Subdivision Regulations for lot size, road frontage, lot configuration, etc.

The site (or parts thereof), have been before the Planning Commission numerous times, most recently at the Commission's July 20th, May 4th, and February 16, 2017 meetings, where Subdivision and Planned Unit Development approvals were granted. The most recent approval was to allow shared access between multiple lots, and Subdivision of three lots. The applicant now wishes to resubdivide the original Lot 4 and Lot 5 from the Rangeline Crossing Subdivision, III by shifting an interior lot line to increase the acreage for the proposed Lot 4A and minimize acreage for the proposed Lot 5A. Lot 5A will still comply with the minimize lot size requirements.

The site is zoned B-3, Community District, which allows a wide range of commercial and residential uses. The PUD will contain a total of seven lots, two large common areas, and several smaller common areas. The two larger common areas will be used for storm water detention for the overall development. A large medical clinic and a fast-food restaurant are currently proposed, both of which will access a proposed private street. A public street is also proposed as part of the overall development, and will link Halls Mill Road to Demetropolis Road.

Because of the size of the overall PUD site and the proximity of the site to Rangeline Road, an ALDOT maintained facility, access management is a concern. The PUD site plan shows that the proposed fast-food lot will have one curb-cut to the Rangeline Service Road North, and two curb-cuts to the proposed private roads (Rangeline Crossing Drive) within the development. The proposed clinic is depicted as having two curb-cuts to Demetropolis Road and one curb-cut to the proposed private road. The clinic additionally abuts the proposed public street (Yogger Pass), and access to this street can be granted via one curb-cut once that street is built and accepted for maintenance by the City of Mobile; however, once access is obtained to Yogger Pass, the clinic's most northern access point to Demetropolis Road should be closed due to possible traffic conflicts. With the exception of future access by the clinic site to Yogger Pass, all other lots within the PUD should be limited in terms of the number of curb-cuts as depicted on an approved PUD site plan. Furthermore, the proposed curb-cuts for Lot 5A depict one curb-cut to Rangeline Road and one curb-cut to Rangeline Crossing Drive. The curb-cut depicted along Rangeline Crossing Drive is located in an undesired location and as it is extremely close to the clinic driveway and appears that it may cause traffic flow issues. It is staff's opinion that a shared curb-cut between Lot 4A and Lot 5A will be more functional and a more appropriate location.

The PUD site plan depicts Lot 5A as having a commercial paint warehouse and retail store developed on the site with what appears to be compliant parking; a dumpster in compliance with Section 64-4.D.9. is depicted on the site plan and the note should be retained on any future site plans. The note stating "full tree planting and landscape requirements will be meet and in compliance with Section 64-4.E of the Zoning Ordinance" should be retained on future site plans.

The 25' minimum building setback lines are depicted on the site plan and the plat; however, it appears that the setback line does not continue across the entire length of the proposed Lot 5A. the plat and site plan should be revised to depict the 25' minimum building lines for the entirety of both properties.

Please note all proposed signage will require separate permits, reviews, and approvals.

As for the Subdivision, both lots will meet the minimum size and area requirements for lots with access to public water and sewer.

Several drainage, access and utility easements are depicted on the preliminary plat and the PUD site plan. The note stating “structures that are not associated with the purpose of the easement shall not be placed within any easement” should be retained on both the plat and site plan.

Sidewalks are depicted on the site plan and should be retained on any future plans.

It should be pointed out that the site has been before the Commission several times with multiple changes to the overall layout of the development. Based on the fact that PUD’s are site plan specific, it does require new applications upon development; however multiple subdivisions reconfiguring the lot sizes as well as a number of changes to the overall site have occurred in very short periods of time. Therefore staff has determined that a Master Plan for the entire site should be submitted to allow a general guide for the overall development of the site. Therefore the current applications are recommended for holdover to allow the submission of an overall Master Plan for the entirety of the PUD and a revised site plan specific for this application to reflect a shared curb-cut and drive lane for Lot 4A and Lot 5A.

RECOMMENDATION

Subdivision: Based upon the preceding, this application is recommended for Holdover until the December 7th meeting, with revisions due by Tuesday November 14, 2017 to be heard in conjunction with the associated Planned Unit Development.

Planned Unit Development: Based upon the preceding, this application is recommended for Holdover until the December 7th meeting, with revisions due by Tuesday November 14, 2017 to address the following:

1. submission of a Master Plan for the entire PUD development;
2. submission of a revised site plan depicting a shared curb-cut between Lot 4A and Lot 5A to the private Rangeline Crossing and the public Rangeline Road Service Road;
3. revision of the site plan and subdivision plat to depict the 25’ minimum building setback lines across the entire property line of Lot 5A; and
4. revision to depict compliance with Traffic Engineering comments: *“A traffic impact study was has been prepared for this site. The study includes off-site improvements along Halls Mill Road adjacent to the development and southward to the intersection of Rangeline Road. Any improvements within the Service Road right-of-way or the Rangeline Road right-of-way are subject to ALDOT approval and permitting. Development is contingent upon the completion of off-site improvements as required by the City of Mobile and ALDOT. Additional impact studies or updates to this study will be required as the development changes or more proposed uses are presented to the City. Driveway number, size, location and design are subject to the approved PUD for this site, and shall be approved by ALDOT (where applicable) and Traffic Engineering and conform to AASHTO standards. Required on-site parking, including ADA handicap spaces, shall meet the minimum standards as defined in Section 64-6 of the City’s Zoning Ordinance. The submitted site plan for the PUD does not include the site plan for the previously approved VA property. The proposed Lot 5A has less than 30’ of frontage on*

the (private) Rangeline Crossing Drive. A standard driveway requires 64' of frontage otherwise the driveway will extend over the property line. The proposed layout of the driveway onto Rangeline Crossing Drive is substandard and not recommended. The plan should include the previously approved VA site and its driveway so that the two designs can be clarified with the PUD approval. Based on comments from ALDOT regarding access on the Service Road, the driveway to Rangeline Crossing should be considered as a shared driveway with Lot 4A and may be better suited to split the property line. The shift of the aisle can provide some relief for the driveway on the north side as well. Confirmation of ALDOT driveway approval will be required prior to permitting for site work on each lot. Enlarged Lot B does not clearly indicate whether the westernmost driveway on Rangeline Service Road is included and may conflict with the overall site layout."

Revised for the December 7th meeting:

The applications were heldover from the November 2nd meeting to allow the applicant to address the following: submission of a Master Plan for the entire PUD development, submission of a revised site plan depicting a shared curb-cut between Lot 4A and Lot 5A to the private Rangeline Crossing and the public Rangeline Road Service Road, and revision of the site plan and subdivision plat to depict the 25' minimum building setback lines across the entire property line of Lot 5A. A revised subdivision plat was not submitted with the revised site plan thus staff is uncertain if the corrections regarding the 25' minimum building setback line have been addressed on the plat, but they have been illustrated on the site plan.

The revised site plan submitted does depict a general Master Plan for the overall site. Although the site plan only illustrates Phase 1 and Phase 2, the narrative provided by the applicant breaks down the development into 4 phases. Phase 1 consist of the 80,000 square foot VA Clinic and is currently under development. Phase 2 includes the 4,000 square foot specialty retail development, an 11, 000 square foot specialty retail development, a 3,500 square foot fast food development, and a 3,800 square foot fast food and 6,000 square foot specialty retail/ apparel development. Phase 3 is a proposed 100,000 square foot shopping center; and Phase 4 is identified as "future development". PUD's are site plan specific and while staff understands that what is proposed may change, the proposed Master Plan helps guide development.

The revised site plan depicts a shared curb-cut between Lot 4A and Lot 5A to the private Rangeline Crossing and the public Rangeline Road Service Road; however the driveways are not 24' wide. In fact, none of the proposed driveways on Lot 4A are 24' wide, but they depict arrows for two way traffic. It should also be pointed out that Lot 5A appears to have a loading dock on the revised Master Plan, but drive aisle are not depicted. The site plan should be revised to reflect 24' wide driveways and access aisle on all lots.

The site plan also depicts a "site data table" that illustrates "Lot A 3,280 square feet and Lot B 79,040 square feet", however the lots are not clearly identified on the site plan. In fact, there appears to be old lot lines and lot numbers still placed on the site plan. The site plan should be revised to clearly depict, identify, and label all lots and lot lines.

Lastly, it appears that the layout of Lot B resub of Lot 2 has completely changed from all of the previously approved site plans. The parking configuration is different, the building type has

changed, the location of the curb-cuts to the private Rangeline Crossing and the public Rangeline Road Service Road have been altered, and the placement of the dumpster has been relocated and appears to be within the 25' minimum building setback line. The site plan must be revised to clearly depict the dumpster location and 25' minimum building setback line to verify the dumpster is not located in the setbacks.

Subdivision: *Based upon the preceding the application is recommended for Tentative Approval, subject to the following conditions:*

- 1) depiction of lots sizes is square feet and acre or the provision of a table illustrating that information;*
- 2) placement of the 25' minimum building setback lines along where the property lines abut all road frontages;*
- 3) placement of a note on the Final Plat stating the number, size, design and location of curb-cuts is subject to a Planning Commission approved Planned Unit Development;*
- 4) placement of a note on the Final Plat stating that structures that are not associated with the purpose of the easement shall not be placed within any easement;*
- 5) compliance with Engineering comments: "A. Provide all of the required information on the SUBDIVISION PLAT (i.e. signature blocks, signatures, certification statements, written legal description, required notes, legend, scale, bearings and distances) that is required by the current Alabama State Board of Licensure for Professional Engineers and Land Surveyors. B. Provide a note that a 25' riparian buffer may be required, during development, along the edge of anything considered by ADEM to be a water of the state. C. Provide the recording information for Rangeline Crossing Subdivision III. D. Provide the Surveyor's, Owner's (notarized), Planning Commission, and Traffic Engineering signatures. E. Add a note to the plat stating that all easements shall remain in effect until vacated through the proper Vacation process. F. Provide a copy of the FINAL SUBDIVISION PLAT to the Engineering Dept. for review. No signatures are required on this drawing. G. After addressing all of the FINAL SUBDIVISION PLAT review comments by the Engineering Dept. provide the ORIGINAL (with all other signatures) and one (1) copy (signatures not required) of the revised Final Plat to the Engineering Department.";*
- 6) compliance with Traffic Engineering comments: "A traffic impact study has been prepared for this site and is an ongoing effort given the dynamics of this development. The study includes off-site improvements along Halls Mill Road adjacent to the development and southward to the intersection of Rangeline Road. Any improvements within the Service Road right-of-way or the Rangeline Road right-of-way are subject to ALDOT approval and permitting. Development is contingent upon the completion of off-site improvements as required by the City of Mobile and ALDOT. Additional impact studies or updates to this study will be required as the development changes or more proposed uses are presented to the City. As of the application, the most recent version of the study submitted to the City is dated August 21, 2017. Updates and changes that may have been made to the study since its submittal must be provided to the City for any final acceptance of the study findings. ALDOT (or its representative) is concurrently reviewing the study for final acceptance by both agencies. A development agreement may be required at ALDOT's request to further outline the terms of the approval of this development. Driveway number, size, location and design are subject to the approved PUD for this site, and shall be approved by ALDOT (where applicable) and Traffic*

Engineering and conform to AASHTO standards. Required on-site parking, including ADA handicap spaces, shall meet the minimum standards as defined in Section 64-6 of the City's Zoning Ordinance. The enlarged plan does not appear to be of the scale reflected on the plan sheet therefore dimensions of driveways, aisles, parking spaces, etc. could not be verified. The revised PUD site plan now appears to include a loading dock on the easternmost lot on Rangeline Service Road. The loading dock appears to obstruct the drive aisle from north to south adjacent to the building. The label on the Lot 4 Specialty Retail should be relocated so that the site layout can be clearly viewed. This arrangement of a drive-thru adjacent to the drive aisle is not desirable (northbound traffic "sandwiched" between two southbound aisles of traffic). Additional separation between the drive aisle and the drive thru is necessary. The westernmost lot appears to include parallel parking that is not clearly identified (looks to be extra long and wide). In conjunction with the drive-thru "bail-out" lane, this area needs to be addressed for right-of-way and functionality, especially if the specialty retail/apparel lot is to share the drive aisle. With the development of the future parcels on the north side of the private road, the alignment of driveways is strongly encouraged and may be required based on the lot layout and intensity of the proposed development.";

- 7) compliance with Urban Forestry comments: "Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 2015-116 and City Code Chapters 57 and 64).";
- 8) compliance with Fire comments: "All projects within the City Limits of Mobile shall comply with the requirements of the City of Mobile Fire Code Ordinance. (2012 International Fire Code)"; and
- 9) completion of the PUD process prior to the signing of the Final Plat.

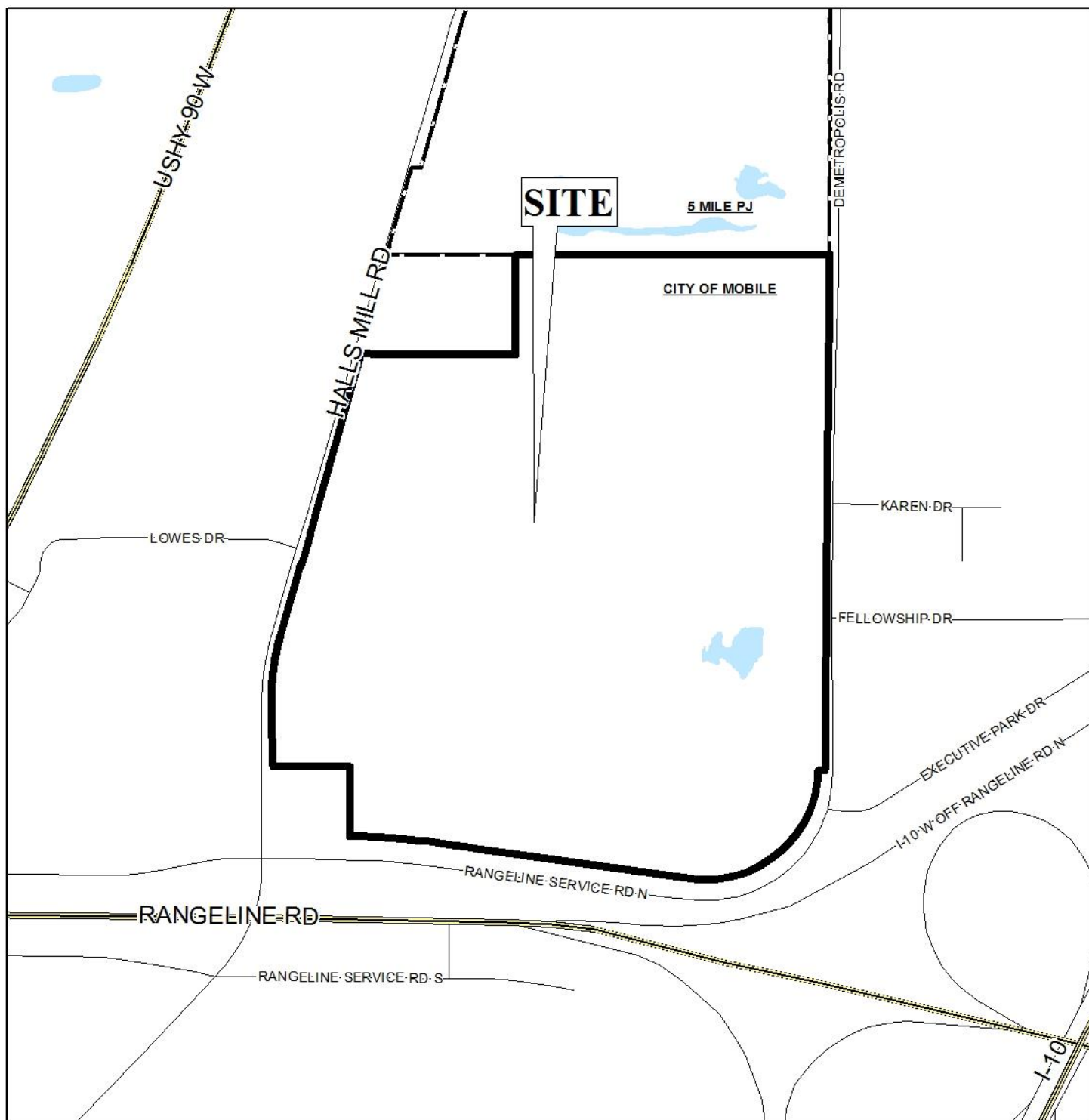
Planned Unit Development: Based upon the preceding, this application is recommended for Approval, subject to the following conditions:

- 1) revision of the site plan to identify and label all 4 phases on the site plan;
- 2) revision of the site plan to depict all driveways and drive aisles to be 24' wide;
- 3) revision of the site plan to remove all dumpsters out of the 25' minimum building setback line;
- 4) revise the site data table to illustrate each lot's size, the proposed use, square footage of the building, and proposed parking spaces;
- 5) placement of the 25' minimum building setback lines along where the property lines abut all road frontages;
- 6) placement of a note on the Final Plat stating the number, size, design and location of curb-cuts is subject to a Planning Commission approved Planned Unit Development;
- 7) placement of a note on the Final Plat stating that structures that are not associated with the purpose of the easement shall not be placed within any easement;
- 8) compliance with Engineering comments: "1. Any existing or proposed detention facility shall be maintained as it was constructed and approved. The Land Disturbance Permit application for any proposed construction includes a requirement of a Maintenance and Inspection Plan (signed and notarized by the Owner) for the detention facility. This Plan shall run with the land and be recorded in the County Probate Office prior to the Engineering Department issuing their approval for a Final Certificate of Occupancy.";
- 9) compliance with Traffic Engineering comments: "A traffic impact study has been prepared for this site and is an ongoing effort given the dynamics of this development.

The study includes off-site improvements along Halls Mill Road adjacent to the development and southward to the intersection of Rangeline Road. Any improvements within the Service Road right-of-way or the Rangeline Road right-of-way are subject to ALDOT approval and permitting. Development is contingent upon the completion of off-site improvements as required by the City of Mobile and ALDOT. Additional impact studies or updates to this study will be required as the development changes or more proposed uses are presented to the City. As of the application, the most recent version of the study submitted to the City is dated August 21, 2017. Updates and changes that may have been made to the study since its submittal must be provided to the City for any final acceptance of the study findings. ALDOT (or its representative) is concurrently reviewing the study for final acceptance by both agencies. A development agreement may be required at ALDOT's request to further outline the terms of the approval of this development. Driveway number, size, location and design are subject to the approved PUD for this site, and shall be approved by ALDOT (where applicable) and Traffic Engineering and conform to AASHTO standards. Required on-site parking, including ADA handicap spaces, shall meet the minimum standards as defined in Section 64-6 of the City's Zoning Ordinance. The enlarged plan does not appear to be of the scale reflected on the plan sheet therefore dimensions of driveways, aisles, parking spaces, etc. could not be verified. The revised PUD site plan now appears to include a loading dock on the easternmost lot on Rangeline Service Road. The loading dock appears to obstruct the drive aisle from north to south adjacent to the building. The label on the Lot 4 Specialty Retail should be relocated so that the site layout can be clearly viewed. This arrangement of a drive-thru adjacent to the drive aisle is not desirable (northbound traffic "sandwiched" between two southbound aisles of traffic). Additional separation between the drive aisle and the drive thru is necessary. The westernmost lot appears to include parallel parking that is not clearly identified (looks to be extra long and wide). In conjunction with the drive-thru "bail-out" lane, this area needs to be addressed for right-of-way and functionality, especially if the specialty retail/apparel lot is to share the drive aisle. With the development of the future parcels on the north side of the private road, the alignment of driveways is strongly encouraged and may be required based on the lot layout and intensity of the proposed development."

- 10) compliance with Urban Forestry comments: "Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 2015-116 and City Code Chapters 57 and 64).";*
- 11) compliance with Fire comments: "All projects within the City Limits of Mobile shall comply with the requirements of the City of Mobile Fire Code Ordinance. (2012 International Fire Code)";*
- 12) submission of a revised PUD site plan prior to the signing of the Final Plat.*
- 13) provision of three revised PUD site plans for the Planning and Zoning Department prior to the prior to the signing of the Final; and*
- 14) compliance with all other municipal codes and ordinances.*

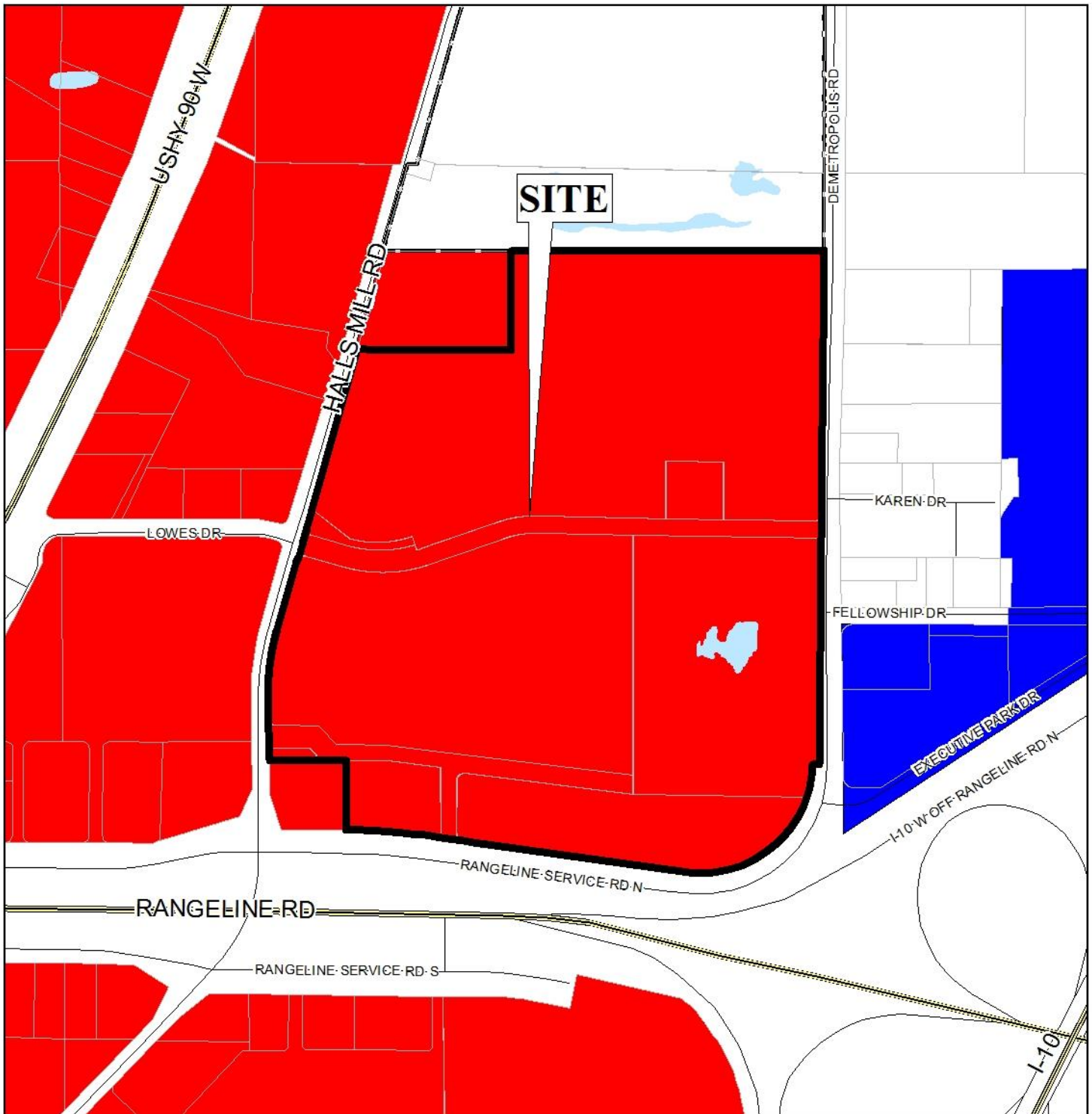
LOCATOR MAP



APPLICATION NUMBER 3 DATE December 7, 2017
 APPLICANT Rangeline Crossing Subdivision III, Resubdivision of Lots 4 & 5
 REQUEST Subdivision, Planned Unit Development



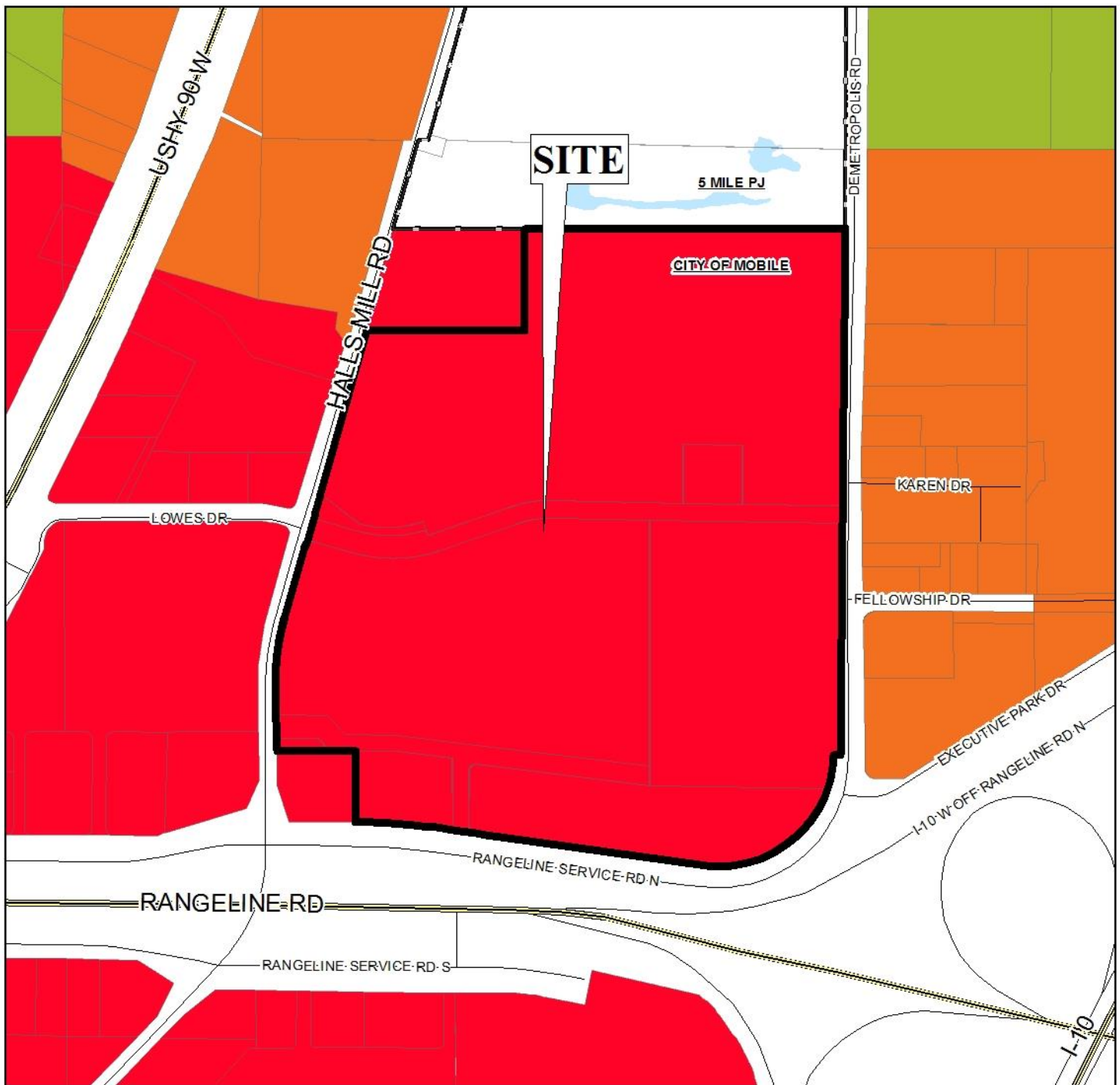
LOCATOR ZONING MAP



APPLICATION NUMBER 3 DATE December 7, 2017
APPLICANT Rangeline Crossing Subdivision III, Resubdivision of Lots 4 & 5
REQUEST Subdivision, Planned Unit Development



FLUM LOCATOR MAP



APPLICATION NUMBER 3 DATE December 7, 2017

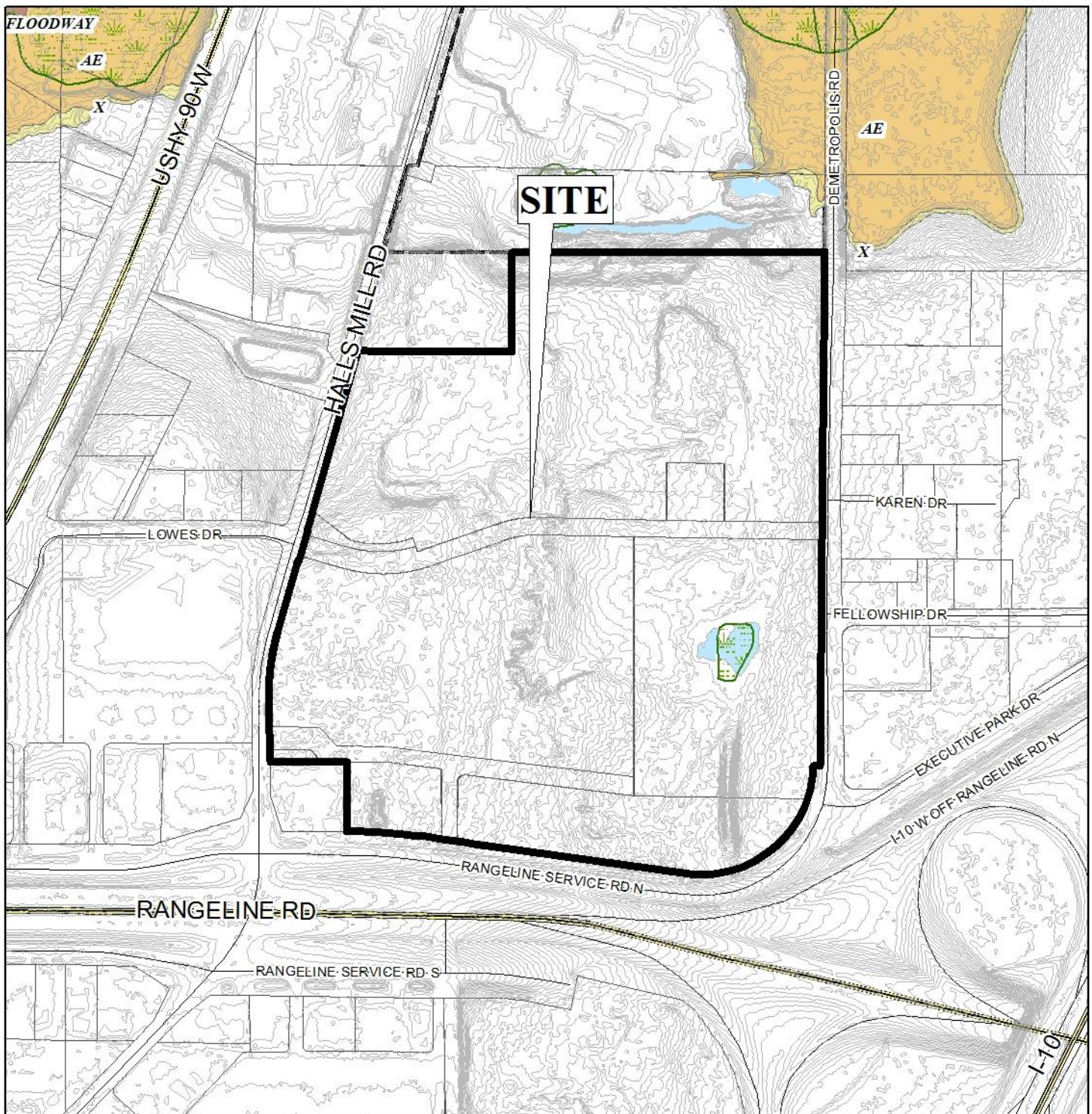
APPLICANT Rangeline Crossing Subdivision III, Resubdivision of Lots 4 & 5

REQUEST Subdivision, Planned Unit Development

Low Density Residential	Neighborhood Center- Traditional	Downtown Waterfront	Water Dependent
Mixed Density Residential	Traditional Corridor	Light Industry	Parks & Open Space
Downtown	Neighborhood Center- Suburban	Heavy Industry	
District Center	Mixed Commercial Corridor	Institutional	



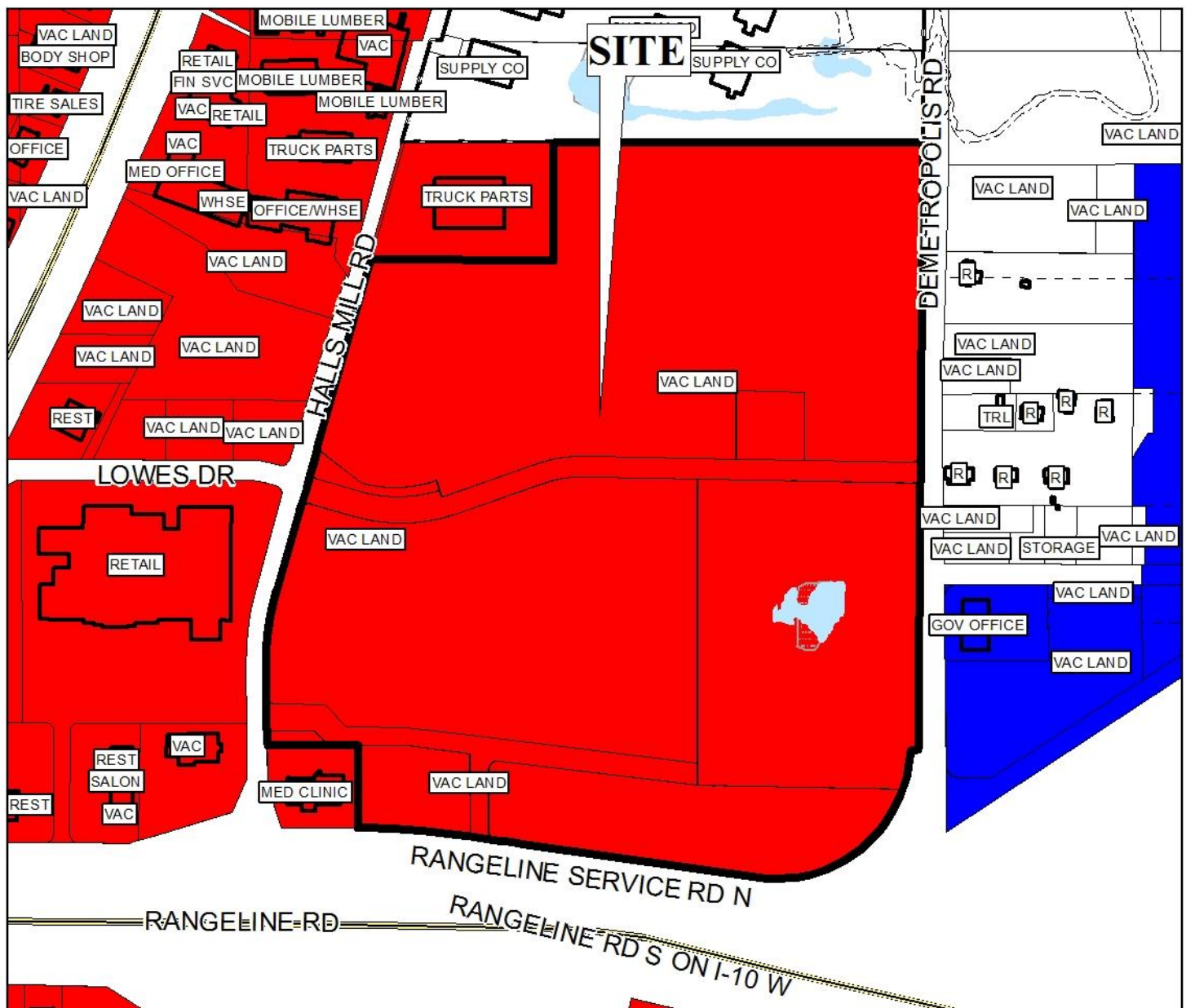
ENVIRONMENTAL LOCATOR MAP



APPLICATION NUMBER 3 DATE December 7, 2017
APPLICANT Rangeline Crossing Subdivision III, Resubdivision of Lots 4 & 5
REQUEST Subdivision, Planned Unit Development



PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The site is surrounded by commercial units. Residential units are located to the northeast.

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 APPLICANT Rangeline Crossing Subdivision III, Resubdivision of Lots 4 & 5
 REQUEST Subdivision, Planned Unit Development

R-A	R-3	T-B	B-2	B-5	MUN	SD-WH	T5.1
R-1	R-B	B-1	B-3	I-1	OPEN	T3	T5.2
R-2	H-B	LB-2	B-4	I-2	SD	T4	T6



PLANNING COMMISSION VICINITY MAP - EXISTING AERIAL

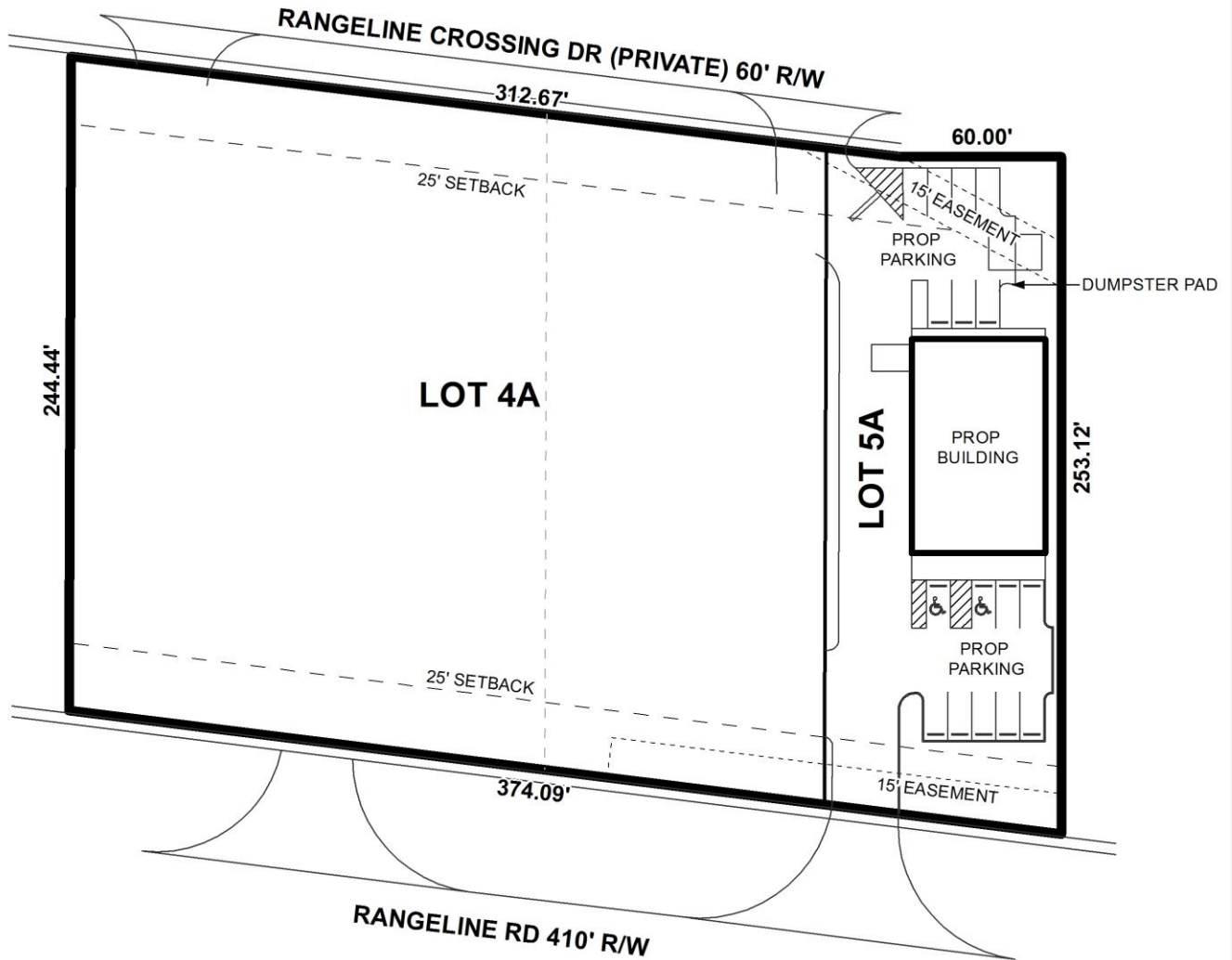


The site is surrounded by commercial units. Residential units are located to the northeast.

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 REQUEST Subdivision, Planned Unit Development



SITE PLAN

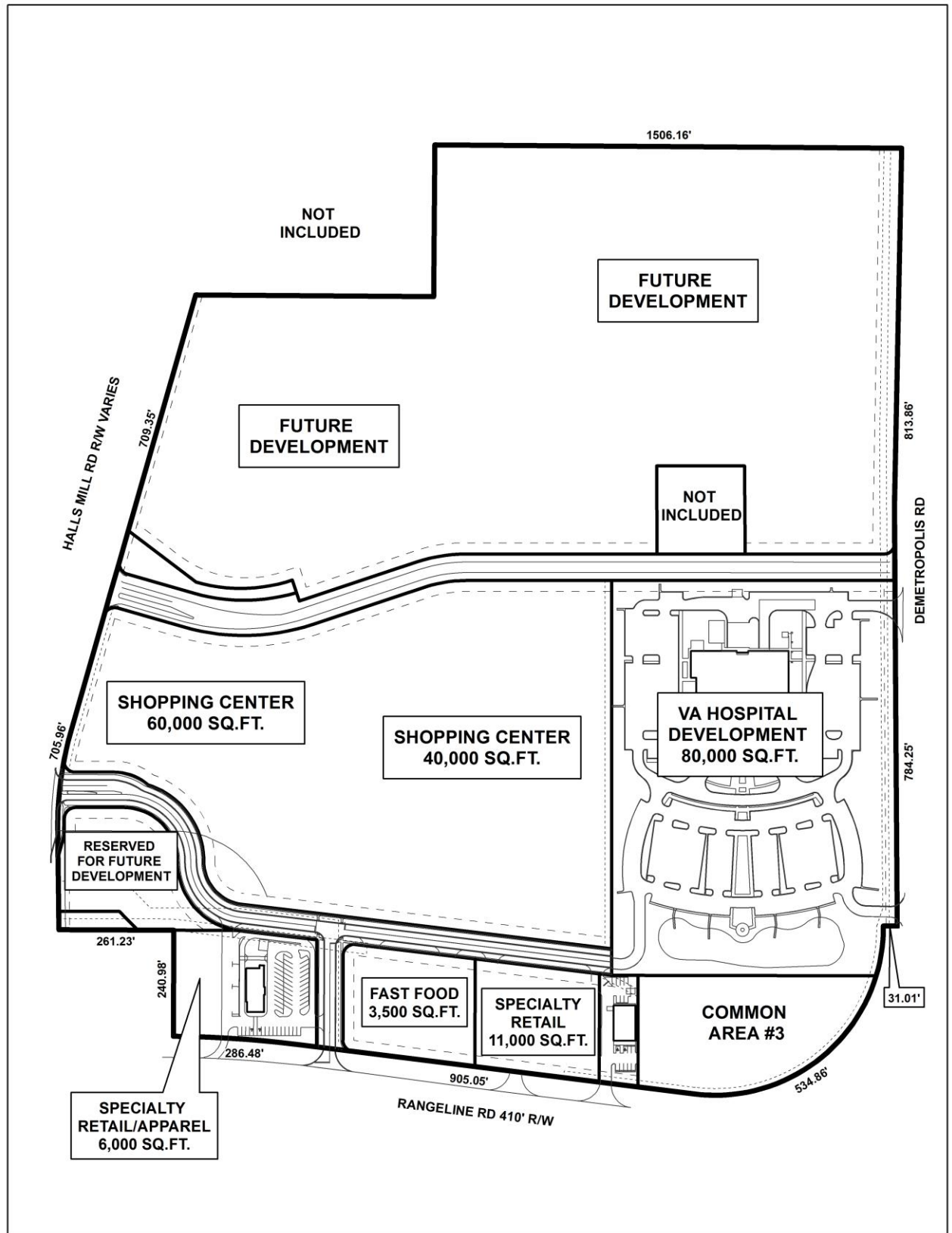


The site plan illustrates the proposed lots, proposed building, proposed parking, easements, and setbacks.

APPLICATION NUMBER 3 DATE December 7, 2017
 APPLICANT Rangeline Crossing Subdivision III, Resubdivision of Lots 4 & 5
 REQUEST Subdivision, Planned Unit Development



DETAIL SITE PLAN



APPLICATION NUMBER 3 DATE December 7, 2017

APPLICANT Rangeline Crossing Subdivision III, Resubdivision of Lots 4 & 5

REQUEST Subdivision, Planned Unit Development



NTS