

**PLANNED UNIT DEVELOPMENT  
& SUBDIVISION STAFF REPORT****Date: May 18, 2017****DEVELOPMENT NAME**

Ryerson Subdivision

**SUBDIVISION NAME**

Ryerson Subdivision

**LOCATION**

363 South Royal Street & 378 Old Water Street  
(Southeast corner of South Royal Street and Canal Street,  
extending to the Southwest corner of Canal Street and Old  
Water Street, extending to the Northwest corner of  
Palmetto Street and Old Water Street)

**CITY COUNCIL  
DISTRICT**

District 2

**AREA OF PROPERTY**

1 Lot / 3.5 ± Acres

**CONTEMPLATED USE**

Planned Unit Development Approval to allow multiple  
buildings on a single building site with reduced setbacks,  
and Subdivision approval to create one legal lot of record.

**TIME SCHEDULE  
FOR DEVELOPMENT**

Not provided.

**ENGINEERING  
COMMENTS****Subdivision**

**FINAL PLAT COMMENTS** (should be addressed prior to submitting the FINAL PLAT for  
review and/or signature by the City Engineer):

- A. Provide all of the required information on the SUBDIVISION PLAT (i.e. signature blocks, signatures, certification statements, written legal description, required notes, legend, scale, bearings and distances) that is required by the current Alabama State Board of Licensure for Professional Engineers and Land Surveyors.
- B. Show and label the proposed MFFE (Minimum Finished Floor Elevation).
- C. Provide and label the monument set or found at each subdivision corner.
- D. Revise the signature block from "CITY OF MOBILE ENGINEERING" to "CITY ENGINEER".
- E. Provide the Surveyor's, Owner's (notarized), Planning Commission, and Traffic Engineering signatures.
- F. Add a note to the SUBDIVISION PLAT stating that a Land Disturbance permit will be required for any land disturbing activity in accordance with Mobile City Code, Chapter 17, Storm Water Management and Flood Control); the City of Mobile, Alabama Flood Plain

Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control.

- G. Add a note to the Plat stating that the approval of all applicable federal, state, and local agencies (including all storm water runoff, wetland and floodplain requirements) will be required prior to the issuance of a Land Disturbance permit.
- H. Add a note that sidewalk is required to be constructed, and/or repaired, along the frontage of each lot, or parcel, at time of new development or construction, unless a sidewalk waiver is approved.
- I. Provide a copy of the FINAL SUBDIVISION PLAT to the Engineering Dept. for review. No signatures are required on this drawing.
- J. After addressing all of the FINAL SUBDIVISION PLAT review comments by the Engineering Dept. provide the ORIGINAL (with all other signatures) and one (1) copy (signatures not required) of the revised Final Plat to the Engineering Department.

### **Planned Unit Development**

#### **ADD THE FOLLOWING NOTES TO THE PUD SITE PLAN:**

- 1. Any work performed in the existing ROW (right-of-way) such as driveways, sidewalks, utility connections, grading, drainage, irrigation, or landscaping will require a ROW permit from the City of Mobile Engineering Department (208-6070) and must comply with the City of Mobile Right-of-Way Construction and Administration Ordinance (Mobile City Code, Chapter 57, Article VIII).
- 2. A Land Disturbance Permit application shall be submitted for any proposed land disturbing activity with the property. A complete set of construction plans including, but not limited to, drainage, utilities, grading, storm water detention systems, paving, and all above ground structures, will need to be included with the Land Disturbance permit. This Permit must be submitted, approved, and issued prior to beginning any of the construction work.
- 3. Any and all proposed land disturbing activity within the property will need to be submitted for review and be in conformance with Mobile City Code, Chapter 17, Storm Water Management and Flood Control; the City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control.
- 4. The approval of all applicable federal, state, and local agencies (including all storm water runoff, wetland and floodplain requirements) will be required prior to the issuance of a Land Disturbance permit. The Owner/Developer is responsible for acquiring all of the necessary permits and approvals.
- 5. The proposed development must comply with all Engineering Department design requirements and Policy Letters.

### **TRAFFIC ENGINEERING**

#### **COMMENTS**

Site is limited to its existing curbs cuts with size, location and design to be approved by Traffic Engineering and conform to AASHTO standards. Any new on-site parking, including ADA handicap spaces, shall meet the minimum standards as defined in Section 64-6 of the City's Zoning Ordinance. The handicap parking as shown does not meet minimum width requirements for a van accessible space (16' minimum).

**URBAN FORESTRY****COMMENTS**

Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 2015-116 and City Code Chapters 57 and 64).

**FIRE DEPARTMENT****COMMENTS**

All projects within the City Limits of Mobile shall comply with the requirements of the City of Mobile Fire Code Ordinance. (2012 International Fire Code).

**MAWSS COMMENTS**

No comments.

**REMARKS**

The applicant is seeking Planned Unit Development Approval to allow multiple buildings on a single building site with reduced setbacks, and Subdivision approval to create one legal lot of record. The site is located in Council District 2, and according to the applicant is served by public water and sanitary sewer.

The applicant proposes to locate a modular office building on the site set back approximately 8' from the front property line along South Royal Street and also create one legal lot of record containing multiple buildings from four metes-and-bounds parcels and one existing legal lot of record.

The site is zoned I-2, Heavy Industry, which allows a wide range of commercial uses. The applicant's current use as a metal processing and distribution facility is allowed by right in this zoning district.

A small area of the total site consists of one legal lot of record, Dean's Industrial Subdivision, approved by the Commission in 2000. The remainder consists of various metes-and-bounds parcels.

Pertaining to the PUD, the applicant states:

*"The subject property is owned entirely by Joseph T. Ryerson & Son, Inc. An application for subdivision is being submitted to bring the multiple parcels into a new legal lot of record. The purpose of this PUD is to allow multiple buildings with shared access on the newly created lot. A modular building with a platform landing and handicap ramp is proposed and will violate a 25 foot minimum front building setback along Royal Street. The handicap ramp is exempt from the building setback. We are requesting the setback be placed at 8 feet as shown to accommodate the proposed building and platform"*

According to the recently adopted Map for Mobile Plan, the site is located within a "Waterfront" Development Area. The intent of this Development Area includes:

- Increased and improved public access to waterfront

- More passive park space and trails/open space that allows public access but is sensitive to the preservation of the natural shorelines, wetlands, etc.
- Minimize impacts of development
- Better streetscaping and aesthetics in waterfront/industrial areas

It should be noted that the Map for Mobile Plan is meant to serve as a general guide, not a detailed lot and district plan or mandate for development. Thus, as this request is solely related to the subdivision of property, not the use of the property, application of the Map for Mobile principles may not be appropriate.

The site is bounded to the North across Canal Street by vacant I-2 property; to the West across South Royal Street by I-2, Light Industry, used by the Mobile County Corrections facility and vacant land; to the South across Palmetto Street by I-2 property used for manufacturing; and to the East by the CSX Railroad right-of-way.

Planned Unit Development review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating excess traffic along minor residential streets in residential districts outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD. PUD approval is site plan specific, thus any changes to the site plan / Subdivision plat will require approval by the Planning Commission.

Please note that a Planned Unit Development approval by the Planning Commission expires after one year if no permits are obtained.

Subdivision review examines the site with regard to promoting orderly development, protecting general health, safety and welfare, and ensuring that development is correlated with adjacent developments and public utilities and services, and to ensure that the subdivision meets the minimum standards set forth in the Subdivision Regulations for lot size, road frontage, lot configuration, etc.

The site fronts South Royal Street, a component of the Major Street Plan with a planned 80' right of way. As the current right-of-way width along the site is 64', dedication to provide 40' from the centerline normally would be required. However, staff has determined that, due to pending revisions to the Major Street Plan, additional right-of-way will no longer be requested along South Royal Street. Therefore, no dedication should be required along that street frontage. As the PUD requests the setback along South Royal Street be modified to only 8', and as similar modification was recently granted by the Commission for the Mobile Community Corrections Subdivision directly across South Royal Street, such would be appropriate in this instance. This setback should be retained along the South Royal Street frontage. No minimum building setback line is shown along the North end of the property along Canal Street. As the existing warehouse building was constructed encroaching into the required setback, the plat should be revised to illustrate a minimum building setback line along the North face of the existing building and

expanding to 25' to the East and West of the building footprint. As on the preliminary plat, the 25' minimum building setback line should be retained along Palmetto Street but should be adjusted to be along the face of the existing metal Quonset hut building. To the East along the Railroad right-of-way, a zero or 5'+ minimum building setback line should be indicated.

As Canal Street and Palmetto Street each have a compliant 50' right-of-way, no frontage dedication would be required. However, a 25' corner radius should be dedicated at the intersection of South Royal Street and Canal Street. The proposed lot meets the minimum size requirements of the Subdivision Regulations. The plat labels the lot size in both square feet and acres. As corner radius dedication should be required, the lot label should be revised to indicate the lot size in both square feet and acres after any required dedication, or a table should be furnished on the Final Plat providing the same information.

As a means of access management, a note should be required on the Final Plat stating that the lot is limited to its existing curb cuts, with the size, location and design to be approved by Traffic Engineering and conform to AASHTO standards.

The preliminary plat indicates two easements on the property. A large easement granted to Alabama Power Company allows for the relocation of transmission towers, poles and other appliances in the event of construction or improvement of any public rights-of-way in proximity to its power lines. Most of this recorded easement is within the footprint of the existing large warehouse building. The other easement is much smaller and restricts the use of its area for any activities associated with the construction or repair of any water craft of any type. Most of that easement is also within the footprint of the existing large warehouse building. As portions of each easement are within the undeveloped area of the site, a note should be required on the Final Plat stating that no structure may be placed or constructed within any easement unless the holder of the easement grants written permission for such.

As the site is zoned I-2, the current use is allowed by right. Furthermore, this district allows up to 75% building site coverage and a 25' front yard setback and zero or 5'+ side and rear yard setbacks (when abutting other commercial districts). The site plan indicates one existing modular office building to be removed and replaced with a larger one encroaching into the 25' front setback along South Royal Street. With the proposed modular building added and one existing modular building removed, the site would have a compliant total site coverage of approximately 47%. With the site being in an older industrial area with many existing noncompliant setbacks, and with the recently-approved setbacks approved by the Commission directly across South Royal Street, the allowance of the requested reduced setbacks would not be out of character for the area. The site plan for the PUD should be revised to reflect the setbacks as allowed for the Subdivision.

Parking calculations on the site plan are for office space only and indicate a sufficient number of spaces provided. However, no calculations are provided for warehouse employees. Therefore, the site plan parking calculations should be revised to provide sufficient parking for warehouse employees also and to indicate such parking spaces on site. It should be noted that, if the site is required to have more than 25 parking spaces, a photometric lighting plan for the parking area will be required at the time of submittal for development permits. As the site is within an I-2

district and as the Zoning Ordinance was revised to not require paved parking in I-2, all parking spaces can be gravel as indicated, with the exception of any provided/required accessible parking.

Landscaping calculations indicate compliance with the ratio requirements. However, as only a small portion of the site is proposed for re-development, staff has determined that tree plantings can be based on only the affected 170' of South Royal Street frontage, thereby requiring only five frontage trees.

A note should be required on the site plan stating that the site is limited to its existing curb cuts with the size, location and design of all curb cuts are to be approved by Traffic Engineering and conform to AASHTO standards.

The site plan does not indicate a dumpster nor is there a note stating that a dumpster will not be utilized. Therefore, the site plan should be revised to indicate a compliant dumpster or a note should be placed on the site plan stating that refuse collection will either be via private can collection or curb-side service.

The site plan should be revised to indicate any required corner radius dedication at the intersection of South Royal Street and Canal Street as per the Subdivision. The lot area label should also be revised to reflect the lot size in both square feet and acres after any required dedication, or a table should be furnished on the site plan providing the same information.

The site plan indicates public sidewalks along most of the public street frontages. However, no sidewalks are indicated along the Eastern ends of the site along Canal Street or Palmetto Street. Therefore, the site plan should be revised to indicate sidewalks along all public street frontages, or a Sidewalk Waiver should be approved prior to signing the Final Plat for the Subdivision.

## RECOMMENDATION

**Subdivision:** The plat meets the minimum requirements of the Subdivision Regulations and is recommended for tentative approval, subject to the following conditions:

- 1) retention of the 8' minimum building setback line along the South Royal Street frontage;
- 2) revision of the plat to illustrate a minimum building setback line along the North face of the existing building and expanding to 25' to the East and West of the building footprint;
- 3) retention of the 25' minimum building setback line along Palmetto Street adjusted to be along the face of the existing Quonset hut building within that encroachment;
- 4) revision of the plat to illustrate a zero or 5'+ building setback line along the East side of the site adjacent to the railroad right-of-way;
- 5) revision of the plat to provide a 25' corner radius dedication at the corner of South Royal Street and Canal Street;
- 6) revision of the plat to label the lot size in both square feet and acres after any required dedication, or the furnishing of a table on the Final Plat providing the same information;

- 7) placement of a note on the Final Plat stating that the lot is limited to its existing curb cuts, with the size, location and design to be approved by Traffic Engineering and conform to AASHTO standards;
- 8) placement of a note on the Final Plat stating that no structure may be placed or constructed within any easement unless the holder of the easement grants written permission for such;
- 9) subject to the Engineering comments: *[FINAL PLAT COMMENTS (should be addressed prior to submitting the FINAL PLAT for review and/or signature by the City Engineer):*  
A. *Provide all of the required information on the SUBDIVISION PLAT (i.e. signature blocks, signatures, certification statements, written legal description, required notes, legend, scale, bearings and distances) that is required by the current Alabama State Board of Licensure for Professional Engineers and Land Surveyors.* B. *Show and label the proposed MFEE (Minimum Finished Floor Elevation).* C. *Provide and label the monument set or found at each subdivision corner.* D. *Revise the signature block from "CITY OF MOBILE ENGINEERING" to "CITY ENGINEER".* E. *Provide the Surveyor's, Owner's (notarized), Planning Commission, and Traffic Engineering signatures.* F. *Add a note to the SUBDIVISION PLAT stating that a Land Disturbance permit will be required for any land disturbing activity in accordance with Mobile City Code, Chapter 17, Storm Water Management and Flood Control; the City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control.* G. *Add a note to the Plat stating that the approval of all applicable federal, state, and local agencies (including all storm water runoff, wetland and floodplain requirements) will be required prior to the issuance of a Land Disturbance permit.* H. *Add a note that sidewalk is required to be constructed, and/or repaired, along the frontage of each lot, or parcel, at time of new development or construction, unless a sidewalk waiver is approved.* I. *Provide a copy of the FINAL SUBDIVISION PLAT to the Engineering Dept. for review. No signatures are required on this drawing.* J. *After addressing all of the FINAL SUBDIVISION PLAT review comments by the Engineering Dept. provide the ORIGINAL (with all other signatures) and one (1) copy (signatures not required) of the revised Final Plat to the Engineering Department.];*
- 10) subject to the Traffic Engineering comments: *[(Site is limited to its existing curbs cuts with size, location and design to be approved by Traffic Engineering and conform to AASHTO standards. Any new on-site parking, including ADA handicap spaces, shall meet the minimum standards as defined in Section 64-6 of the City's Zoning Ordinance. The handicap parking as shown does not meet minimum width requirements for a van accessible space (16' minimum).];*
- 11) subject to the Urban Forestry comments: *[Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 2015-116 and City Code Chapters 57 and 64).];*
- 12) subject to the Fire Department comments: *[All projects within the City Limits of Mobile shall comply with the requirements of the City of Mobile Fire Code Ordinance. (2012 International Fire Code)];* and
- 13) submission and approval of two (2) copies of a revised PUD site plan prior to signing the Final Plat.

**Planned Unit Development:** Based upon the preceding, this application is recommended for approval, subject to the following conditions:

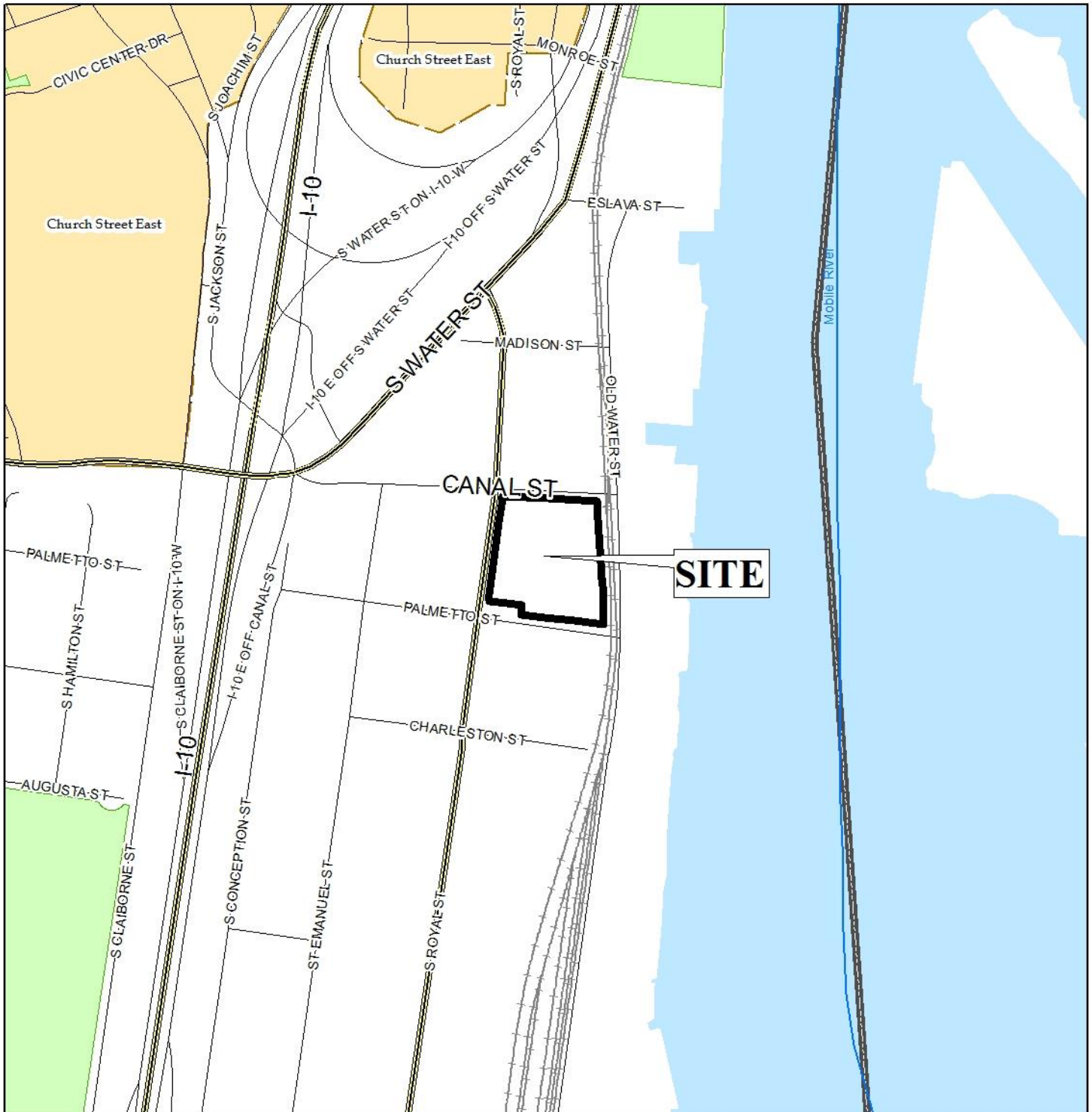
- 1) retention of the 8' minimum building setback line along the South Royal Street frontage;
- 2) revision of the site plan to illustrate a minimum building setback line along the North face of the existing building and expanding to 25' to the East and West of the building footprint;
- 3) retention of the 25' minimum building setback line along Palmetto Street adjusted to be along the face of the existing Quonset hut building within that encroachment;
- 4) revision of the site plan to illustrate a zero or 5'+ building setback line along the East side of the site adjacent to the railroad right-of-way;
- 5) revision of the parking calculations to provide sufficient parking for warehouse employees and provision of additional parking spaces on the site plan, if needed;
- 6) revision of the site plan to provide five frontage trees based on the 170' frontage of the proposed affected area instead of the entire Royal Street frontage, with the location to be coordinated with the Planning and Zoning staff;
- 7) placement of a note on the site plan stating that the lot is limited to its existing curb cuts, with the size, location and design to be approved by Traffic Engineering and conform to AASHTO standards;
- 8) revision of the site plan to indicate a compliant dumpster or the placement of a note on the site plan stating that refuse collection will either be via private can collection or curb-side service;
- 9) revision of the site plan to provide a 25' corner radius dedication at the corner of South Royal Street and Canal Street;
- 10) revision of the site plan to label the lot size in both square feet and acres after any required dedication, or the furnishing of a table on the site plan providing the same information;
- 11) revision of the site plan to indicate sidewalks along all public street frontages, or the approval of a Sidewalk Waiver prior to signing the Final Plat for the Subdivision;
- 12) subject to the Engineering comments: *[ADD THE FOLLOWING NOTES TO THE PUD SITE PLAN: 1. Any work performed in the existing ROW (right-of-way) such as driveways, sidewalks, utility connections, grading, drainage, irrigation, or landscaping will require a ROW permit from the City of Mobile Engineering Department (208-6070) and must comply with the City of Mobile Right-of-Way Construction and Administration Ordinance (Mobile City Code, Chapter 57, Article VIII). 2. A Land Disturbance Permit application shall be submitted for any proposed land disturbing activity with the property. A complete set of construction plans including, but not limited to, drainage, utilities, grading, storm water detention systems, paving, and all above ground structures, will need to be included with the Land Disturbance permit. This Permit must be submitted, approved, and issued prior to beginning any of the construction work. 3. Any and all proposed land disturbing activity within the property will need to be submitted for review and be in conformance with Mobile City Code, Chapter 17, Storm Water Management and Flood Control); the City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control. 4. The approval of all applicable federal, state, and local*



*agencies (including all storm water runoff, wetland and floodplain requirements) will be required prior to the issuance of a Land Disturbance permit. The Owner/Developer is responsible for acquiring all of the necessary permits and approvals. 5. The proposed development must comply with all Engineering Department design requirements and Policy Letters.];*

- 13) *subject to the Traffic Engineering comments: [Site is limited to its existing curbs cuts with size, location and design to be approved by Traffic Engineering and conform to AASHTO standards. Any new on-site parking, including ADA handicap spaces, shall meet the minimum standards as defined in Section 64-6 of the City's Zoning Ordinance. The handicap parking as shown does not meet minimum width requirements for a van accessible space (16' minimum).];*
- 14) *subject to the Urban Forestry comments: [Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 2015-116 and City Code Chapters 57 and 64).];*
- 15) *subject to the Fire Department comments: [All projects within the City Limits of Mobile shall comply with the requirements of the City of Mobile Fire Code Ordinance. (2012 International Fire Code)];*
- 16) *full compliance with all municipal codes and ordinances; and*
- 17) *submission and approval of two (2) copies of a revised PUD site plan prior to signing the Final Plat for the Subdivision.*

# LOCATOR MAP



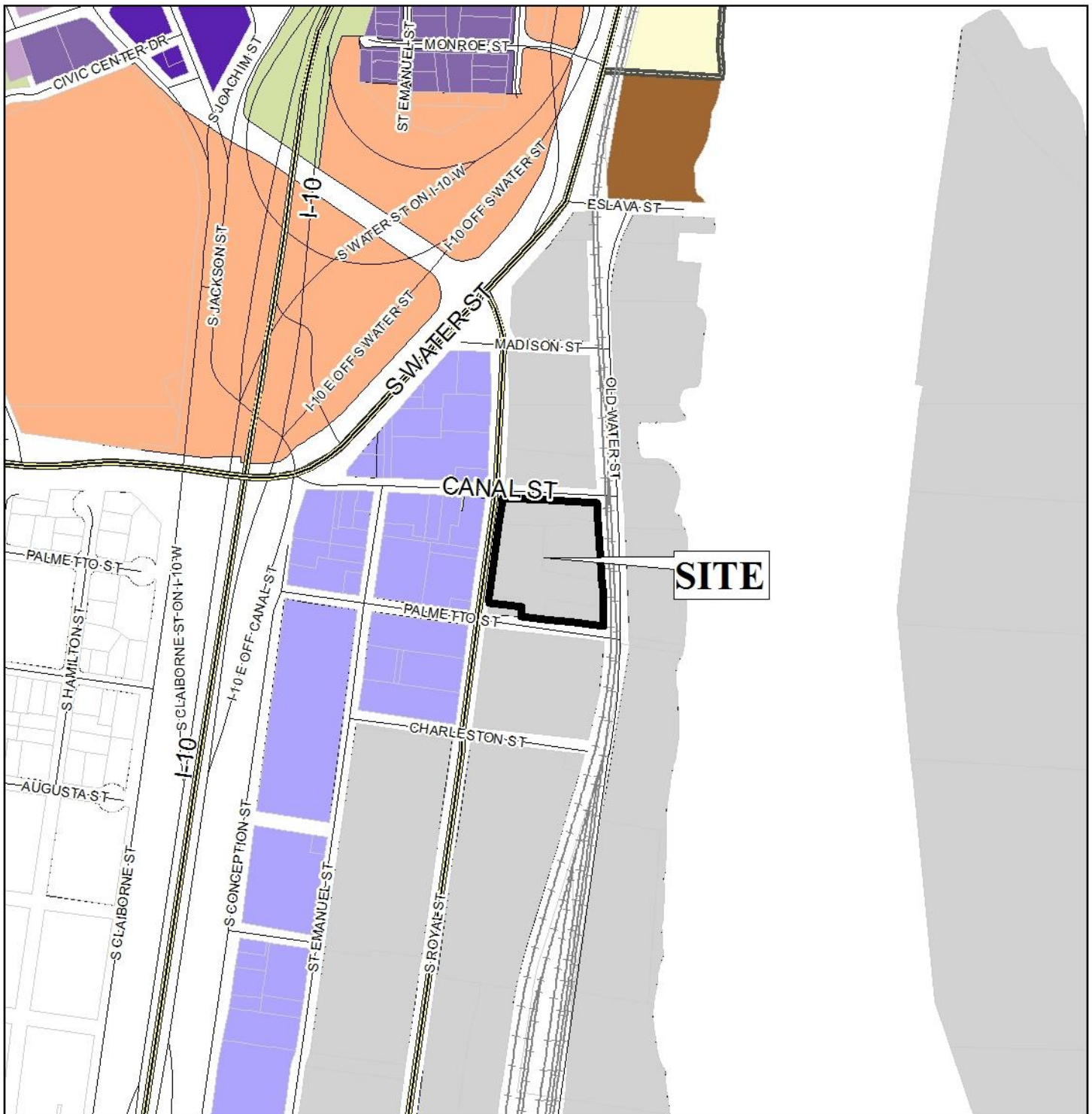
APPLICATION NUMBER 14 DATE May 18, 2017

APPLICANT Ryerson Subdivision

REQUEST Subdivision, Planned Unit Development



# LOCATOR ZONING MAP



APPLICATION NUMBER 14 DATE May 18, 2017

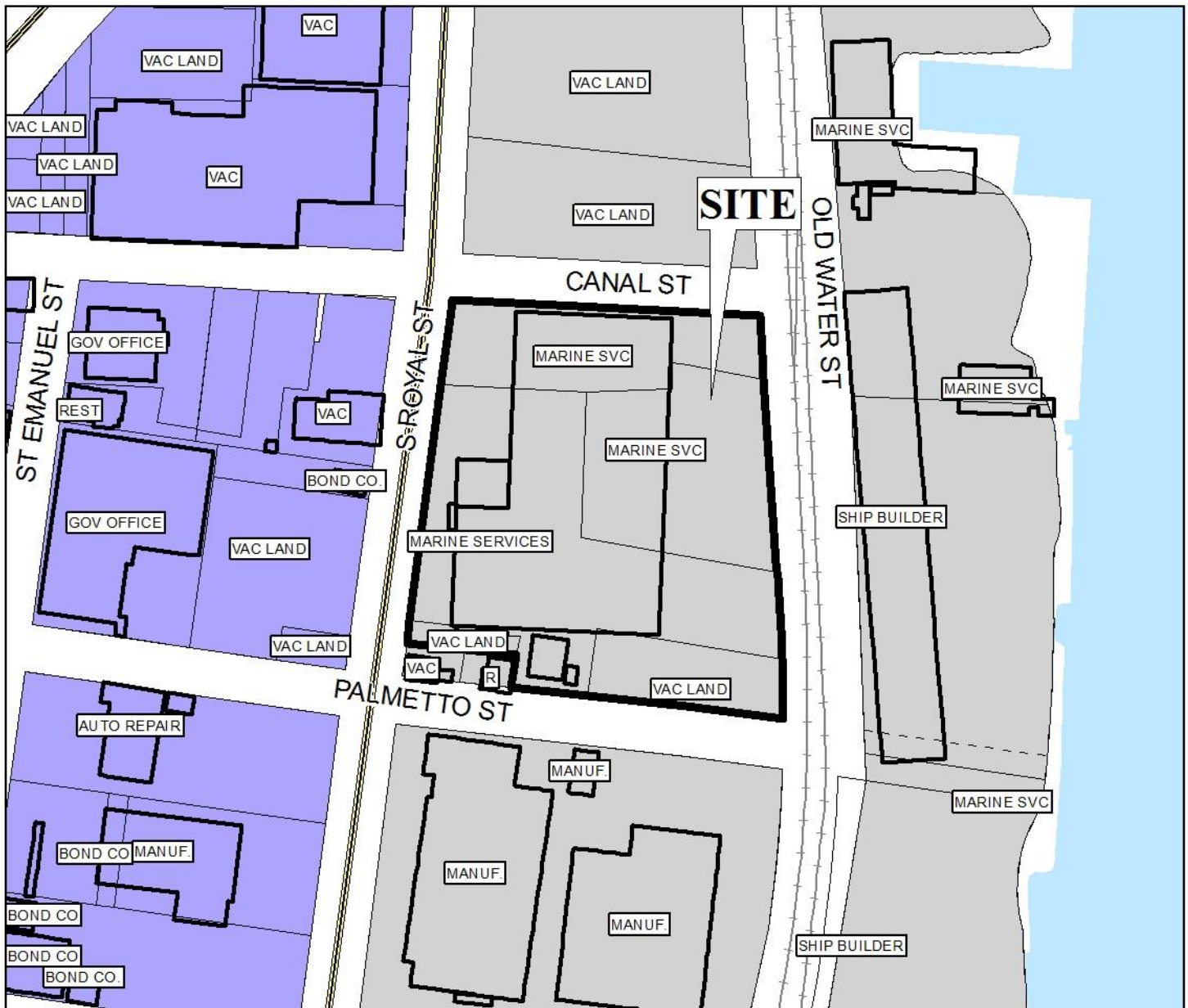
APPLICANT Ryerson Subdivision

REQUEST Subdivision, Planned Unit Development





# PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The site is surrounded by commercial units to the west and industrial units to the east.

APPLICATION NUMBER 14 DATE May 18, 2017

APPLICANT Ryerson Subdivision

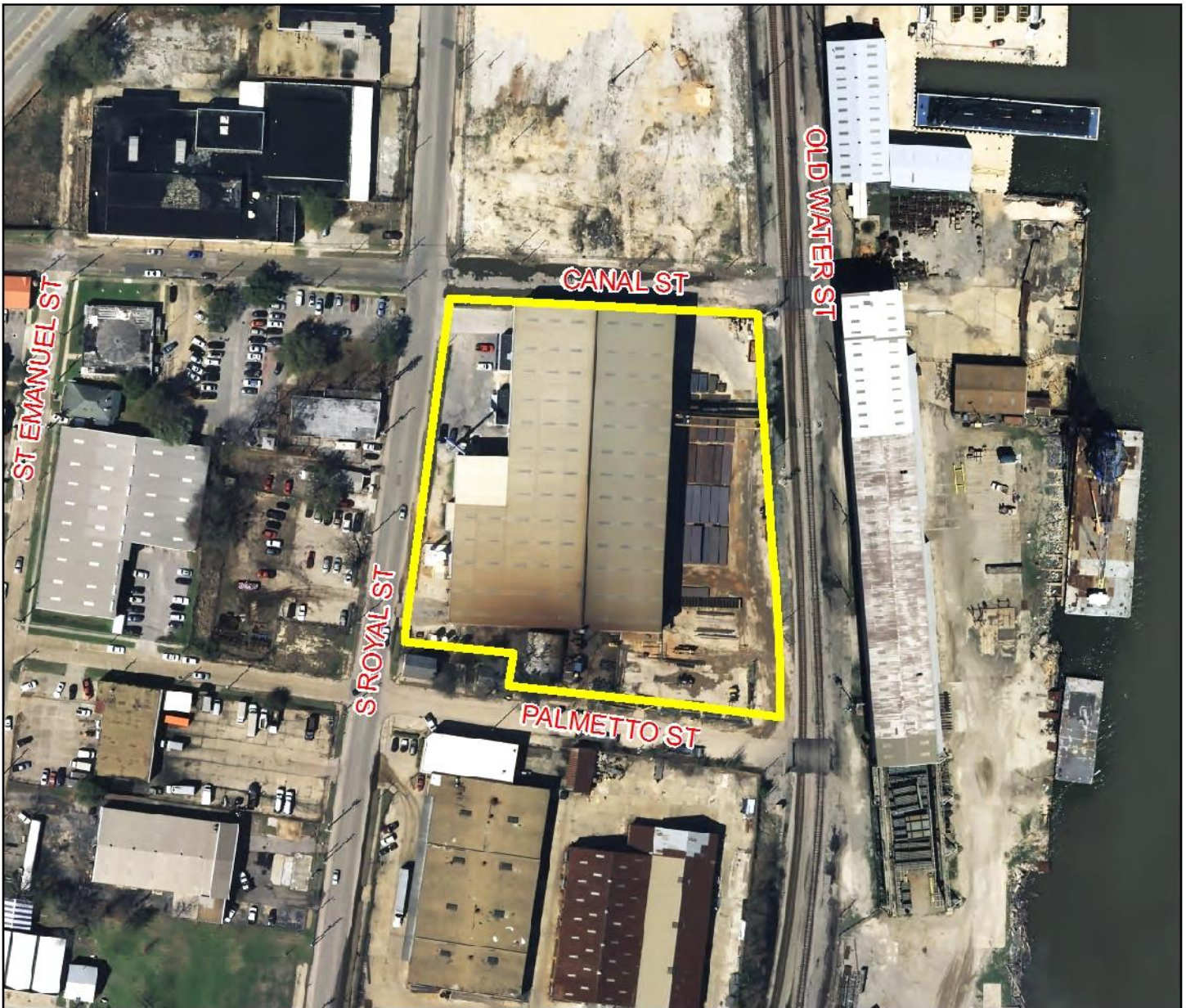
REQUEST Subdivision, Planned Unit Development

R-A	R-3	T-B	B-2	B-5	MUN	SD-WH	T5.1
R-1	R-B	B-1	B-3	I-1	OPEN	T3	T5.2
R-2	H-B	LB-2	B-4	I-2	SD	T4	T6





# PLANNING COMMISSION VICINITY MAP - EXISTING AERIAL



The site is surrounded by commercial units to the west and industrial units to the east.

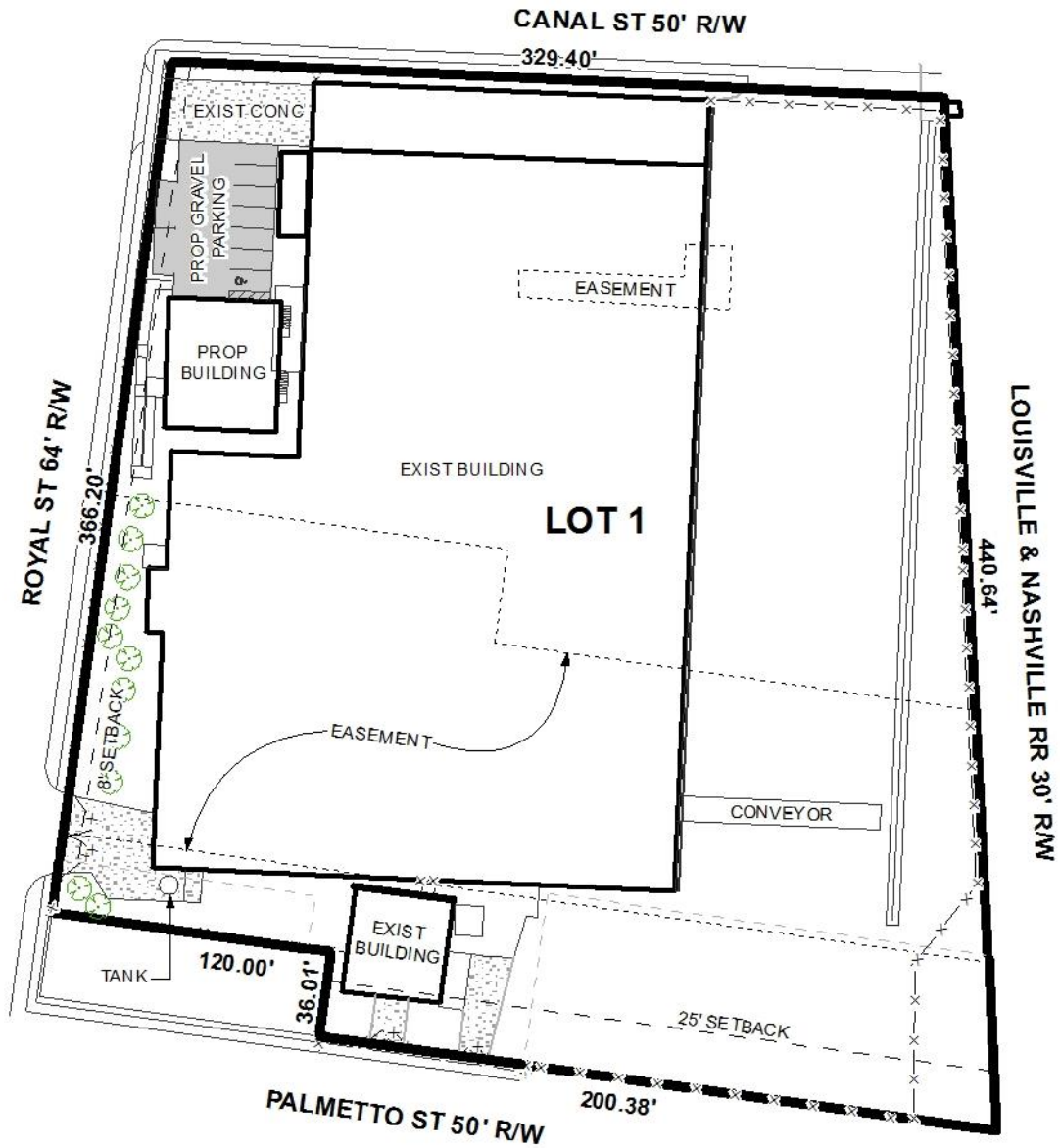
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# SITE PLAN



The site plan illustrates the existing building, setbacks, proposed buildings, and proposed parking.

APPLICATION NUMBER 14 DATE May 18, 2017

APPLICANT Ryerson Subdivision

REQUEST Subdivision, Planned Unit Development



NTS