

**PLANNED UNIT DEVELOPMENT  
& SUBDIVISION STAFF REPORT****March 1, 2018**

<b><u>DEVELOPMENT NAME</u></b>	The Shoppes at Rangeline Subdivision
<b><u>SUBDIVISION NAME</u></b>	The Shoppes at Rangeline Subdivision
<b><u>LOCATION</u></b>	4439 Rangeline Road Northwest corner of Rangeline Road and Halls Mill Road.
<b><u>CITY COUNCIL DISTRICT</u></b>	District 4
<b><u>AREA OF PROPERTY</u></b>	2 Lots / 2.0 ± acres
<b><u>CONTEMPLATED USE</u></b>	Planned Unit Development Approval to allow shared access and parking between multiple building sites, and to create two legal lots or record from one existing legal lot.
<b><u>TIME SCHEDULE FOR DEVELOPMENT</u></b>	None given.

**ENGINEERING****COMMENTS**

**Subdivision:** FINAL PLAT COMMENTS (should be addressed prior to submitting the FINAL PLAT for review and/or signature by the City Engineer):

- A. Provide all of the required information on the SUBDIVISION PLAT (i.e. signature blocks, signatures, certification statements, written legal description, required notes, legend, scale, bearings and distances) that is required by the current Alabama State Board of Licensure for Professional Engineers and Land Surveyors.
- B. Provide and label the monument set or found at each subdivision corner.
- C. Add a signature block for the Owner, Notary Public, Planning Commission, Traffic Engineer, and City Engineer.
- D. Provide the Surveyor's Certificate and Signature.
- E. Provide the Surveyor's, Owner's (notarized), Planning Commission, and Traffic Engineering signatures.
- F. Add a note to the SUBDIVISION PLAT stating that as shown on the 1984 aerial photos Lots A & B will receive historical credit of impervious area towards stormwater detention requirement per Mobile City Code, Chapter 17, Storm Water Management and Flood Control. (Coordinate with City Engineering Department staff to establish the exact amount prior to the submittal of the FINAL PLAT for signature.)
- G. Add a note to the SUBDIVISION PLAT stating that a Land Disturbance permit will be required for any land disturbing activity in accordance with Mobile City Code, Chapter 17, Storm Water Management and Flood Control); the City of Mobile, Alabama Flood Plain

Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control.

- H. Add a note to the Plat stating that the approval of all applicable federal, state, and local agencies (including all storm water runoff, wetland and floodplain requirements) will be required prior to the issuance of a Land Disturbance permit.
- I. Add a note that sidewalk is required to be constructed, and/or repaired, along the frontage of each lot, or parcel, at time of new development or construction, unless a sidewalk waiver is approved.
- J. Add a note to the plat stating that all proposed detention facilities, common areas, and wetlands shall be the responsibility of the Property Owner(s), and not the responsibility of the City of Mobile.
- K. Add a note to the plat stating that all easements shall remain in effect until vacated through the proper Vacation process.
- L. Provide a copy of the FINAL SUBDIVISION PLAT to the Engineering Dept. for review. No signatures are required on this drawing.
- M. After addressing all of the FINAL SUBDIVISION PLAT review comments by the Engineering Dept. provide the ORIGINAL (with all other signatures) and one (1) copy (signatures not required) of the revised Final Plat to the Engineering Department.

#### **Planned Unit Development**

1. Retain NOTES #1 - #5 as shown on the submitted site plan.

#### **TRAFFIC ENGINEERING**

##### **COMMENTS**

Rangeline Road and its Service Road are ALDOT maintained roadways. Site is limited to its existing shared curb cuts with any changes in size, location and design to be approved by ALDOT (where applicable) and Traffic Engineering and conform to AASHTO standards. Any new on-site parking, including ADA handicap spaces, shall meet the minimum standards as defined in Section 64-6 of the City's Zoning Ordinance. There are proposed changes to the intersection of Rangeline Service Road and Halls Mill Road to limit access from the service road to right-in, right-out only. Those changes in addition to the construction of an additional southbound left turn lane at the intersection of Halls Mill Road and Rangeline Road are the responsibility of a nearby development project.

#### **URBAN FORESTRY**

##### **COMMENTS**

Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 2015-116 and City Code Chapters 57 and 64).

#### **FIRE DEPARTMENT**

##### **COMMENTS**

All projects within the City Limits of Mobile shall comply with the requirements of the City of Mobile Fire Code Ordinance. (2012 International Fire Code).

#### **REMARKS**

The applicant is requesting Planned Unit Development Approval to allow shared access and parking between multiple building sites, and to create two legal lots of

record from one existing legal lot. The site is located in Council District 4, and according to the applicant is served by public water and sanitary sewer.

This site most recently appeared before the Planning Commission at its December 21, 2000 meeting where the Commission allowed a 7 lot subdivision.

The site has been given a District Center land use designation, per the recently adopted Future Land Use Plan and Map. The Future Land Use Plan and Map complements and provides additional detail to the Development Framework Maps in the Map for Mobile, adopted by the Planning Commission at its November 5, 2015 meeting.

This designation applies across the city to larger areas of existing mixed-use character or where such character is encouraged. These areas will include moderate to high-density residential (minimum densities of 6 dwelling units per acre0) in dynamic, horizontal or vertical mixed use environments, to provide a balance of housing and employment.

District Centers generally serve several surrounding neighborhoods and may even have a city wide or region-wide reach. As such, they are often anchored by a major commercial or institutional employer such as a shopping mall or a medical center.

Depending on location and assigned zoning, residential areas in District Centers may incorporate a mix of housing types, ranging from mid-rise multifamily buildings containing apartments and lofts, to townhouses and detached single-family homes. Major civic cultural institutions and public spaces provide regional and neighborhood destinations.

District Centers should be designed to induce pedestrian activity, with high quality streetscapes connecting the different components of a center as well as the center to its surrounding area. DC districts may be served by transit and include development of an intensity and design that supports transit use.

It should be noted that the Future Land Use Plan and Map components of the Map for Mobile Plan are meant to serve as a general guide, not a detailed lot and district plan. In many cases the designation on the new Future Land Use Map may match the existing use of land, but in others the designated land use may differ from what is on the ground today. As such, the Future Land Use Plan and Map allows the Planning Commission and City Council to consider individual cases based on additional information such as the classification request, the surrounding development, the timing of the request, and the appropriateness and compatibility of the proposed use and, where applicable, the zoning classification.

Subdivision review examines the site with regard to promoting orderly development, protecting general health, safety and welfare, and ensuring that development is correlated with adjacent developments and public utilities and services, and to ensure that the subdivision meets the minimum standards set forth in the Subdivision Regulations for lot size, road frontage, lot configuration, etc.

The proposed lots appear to meet the minimum size and frontage requirements as defined by Section V.D.2. of the Subdivision Regulations.

The preliminary plat depicts the lot sizes in square feet and acres. The lot sizes should be retained in square feet and acres on the Final Plat, if approved. The minimum building setback lines are depicted, and should be retained on the Final Plat, if approved.

The site fronts Halls Mill Road, which is identified as a minor arterial street, as shown on the Major Street Plan Component of the Comprehensive Plan. It appears that sufficient dedication has already been provided in previous subdivision applications. The site also fronts the Rangeline Road Service Road, a major street with curb and gutter with adequate right-of-way provided.

The site currently has 2 existing curb-cuts to an interior drive aisle and 1 existing curb-cut to the Rangeline Service Road. Thus, the site should be limited to the existing curb-cuts with size, location and design to be approved by Traffic Engineering, and ALDOT where appropriate, and conform to AASHTO standards.

The plat also depicts a 5' access easement along Halls Mills Road. A note should be placed on the Final Plat stating that no structures shall be placed or constructed within any easements.

Planned Unit Development review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating excess traffic along minor residential streets in residential districts outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD.

In pursuit of these purposes, the objectives to be met by a PUD are as follows: a) to encourage innovative and diversified design in building form and site development; b) flexibility, to permit greater flexibility in the location and arrangement of buildings and uses than is generally possible under district regulations; c) to encourage the most efficient and sustainable use of land, especially tracts in the inner part of the city that remain undeveloped or that are appropriate for re-development; d) to preserve and protect as urban amenities the natural features and characteristics of land; e) to encourage the provision of common open space through efficient site design; and, f) to encourage optimum use of available public utilities, streets and community facilities.

PUD approval is site-plan specific, thus any changes to the site plan / Subdivision plat will require approval by the Planning Commission. Also, PUD approvals expire within one (1) year if no permits for the development are obtained.

The site is currently commercially developed with an existing vacant commercial building. The site plan depicts the newly proposed Lot B as being a retail/ office building and restaurant. The

10,987 square foot building will consist of 9,787 square feet utilized as retail/ office and 1,200 square feet as restaurant. The site plan also depicts the proposed Lot A to be developed as a drive-thru restaurant with a 2,000 square foot building and outdoor seating space.

With regards to parking requirements, the retail/ office building and restaurant located on Lot B will require 45 parking spaces. Although the site plan does not depict the square footage of the outdoor dining area for the proposed restaurant on Lot A, staff was able to determine that an additional 380 square feet  $\pm$  of outside dining will be provided; thus requiring an estimated 24 parking spaces for drive-thru restaurant with outdoor dining area. Therefore a total of 89 parking spaces will be required for the site; the site plan depicts a total of 119 parking spaces. The site plan erroneously depicts the total parking spaces required as 66 parking spaces instead of the 89. The site plan should be revised to depict the total square footage of the outdoor seating space, and the above numbers listed for the parking requirements in the parking table on the site plan.

It should also be pointed noted that the parking spaces on Lot B closes to the building do not depict bumper stops on the last two parking spaces, thus encroachment into a pedestrian area may occur. The site plan should be revised to depict bumper stops on all the front parking spaces. It appears that curbing is provided on Lot A in front of the parking spaces adjacent to the drive-thru restaurant, however if curbing is not provided, the site plan should be revised to depict bumper stops in front of those parking spaces closest to the building as well.

With regards to tree and landscaping, the site plan does not actually depict the trees and landscaping on the site plan, but does provide a chart. As typically required by the Planning Commission for PUD's, the site will be required to provide frontage trees along the Rangeline Road Service Road and Halls Mill Road as well as parking trees for the new construction and compliant landscaping.

It should be noted that staff cannot determine if there is adequate space between the order menu board and the pick-up window on Lot A because the drive-thru window is not clearly identified. It does appear that sufficient room is providing for queuing spaces for the drive-thru, but without the location of the pick-up window being identified, staff cannot make a final determination. The site plan should be revised to depict the location of the pick-up window.

The minimum building setback lines are depicted on the site plan and should be retained on future on any site plans.

A sidewalk is not depicted on the site plan. The placement of a sidewalk along Halls Mill Road and the Rangeline Road Service Road or the submission of a sidewalk waiver will be required.

## **RECOMMENDATION**

**Subdivision:** Based upon the preceding, the application is recommended for Tentative Approval, subject to the following conditions:

- 1) retention of the lot sizes in square feet and acres;

- 2) retention of the 25' minimum building setback line;
- 3) placement of a note on the Final Plat stating that no structures shall be placed or constructed within any easements;
- 4) placement of a note on the Final Plat stating the site should be limited to the existing curb-cuts with size, location and design to be approved by Traffic Engineering, and ALDOT where appropriate, and conform to AASHTO standards;
- 5) compliance with Engineering comments: *"FINAL PLAT COMMENTS (should be addressed prior to submitting the FINAL PLAT for review and/or signature by the City Engineer): A. Provide all of the required information on the SUBDIVISION PLAT (i.e. signature blocks, signatures, certification statements, written legal description, required notes, legend, scale, bearings and distances) that is required by the current Alabama State Board of Licensure for Professional Engineers and Land Surveyors. B. Provide and label the monument set or found at each subdivision corner. C. Add a signature block for the Owner, Notary Public, Planning Commission, Traffic Engineer, and City Engineer. D. Provide the Surveyor's Certificate and Signature. E. Provide the Surveyor's, Owner's (notarized), Planning Commission, and Traffic Engineering signatures. F. Add a note to the SUBDIVISION PLAT stating that as shown on the 1984 aerial photos Lots A & B will receive historical credit of impervious area towards stormwater detention requirement per Mobile City Code, Chapter 17, Storm Water Management and Flood Control. (Coordinate with City Engineering Department staff to establish the exact amount prior to the submittal of the FINAL PLAT for signature.) G. Add a note to the SUBDIVISION PLAT stating that a Land Disturbance permit will be required for any land disturbing activity in accordance with Mobile City Code, Chapter 17, Storm Water Management and Flood Control; the City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control. H. Add a note to the Plat stating that the approval of all applicable federal, state, and local agencies (including all storm water runoff, wetland and floodplain requirements) will be required prior to the issuance of a Land Disturbance permit. I. Add a note that sidewalk is required to be constructed, and/or repaired, along the frontage of each lot, or parcel, at time of new development or construction, unless a sidewalk waiver is approved. J. Add a note to the plat stating that all proposed detention facilities, common areas, and wetlands shall be the responsibility of the Property Owner(s), and not the responsibility of the City of Mobile. K. Add a note to the plat stating that all easements shall remain in effect until vacated through the proper Vacation process. L. Provide a copy of the FINAL SUBDIVISION PLAT to the Engineering Dept. for review. No signatures are required on this drawing. M. After addressing all of the FINAL SUBDIVISION PLAT review comments by the Engineering Dept. provide the ORIGINAL (with all other signatures) and one (1) copy (signatures not required) of the revised Final Plat to the Engineering Department."*;
- 6) compliance with Traffic Engineering comments: *"Rangeline Road and its Service Road are ALDOT maintained roadways. Site is limited to its existing shared curb cuts with any changes in size, location and design to be approved by ALDOT (where applicable) and Traffic Engineering and conform to AASHTO standards. Any new on-site parking, including ADA handicap spaces, shall meet the minimum standards as defined in Section 64-6 of the City's Zoning Ordinance. There are proposed changes to the intersection of Rangeline Service Road and Halls Mill Road to limit access from the service road to*

*right-in, right-out only. Those changes in addition to the construction of an additional southbound left turn lane at the intersection of Halls Mill Road and Rangeline Road are the responsibility of a nearby development project.”;*

- 7) compliance with Urban Forestry comments: *“Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 2015-116 and City Code Chapters 57 and 64).”;*
- 8) compliance with Fire comments: *“All projects within the City Limits of Mobile shall comply with the requirements of the City of Mobile Fire Code Ordinance. (2012 International Fire Code).”;*
- 9) full compliance with all other municipal codes and ordinances; and
- 10) provision of a revised PUD site plan prior to signing the Final Plat

**Planned Unit Development:** Staff recommends the following Findings of Fact for Approval for the Planned Unit Development:

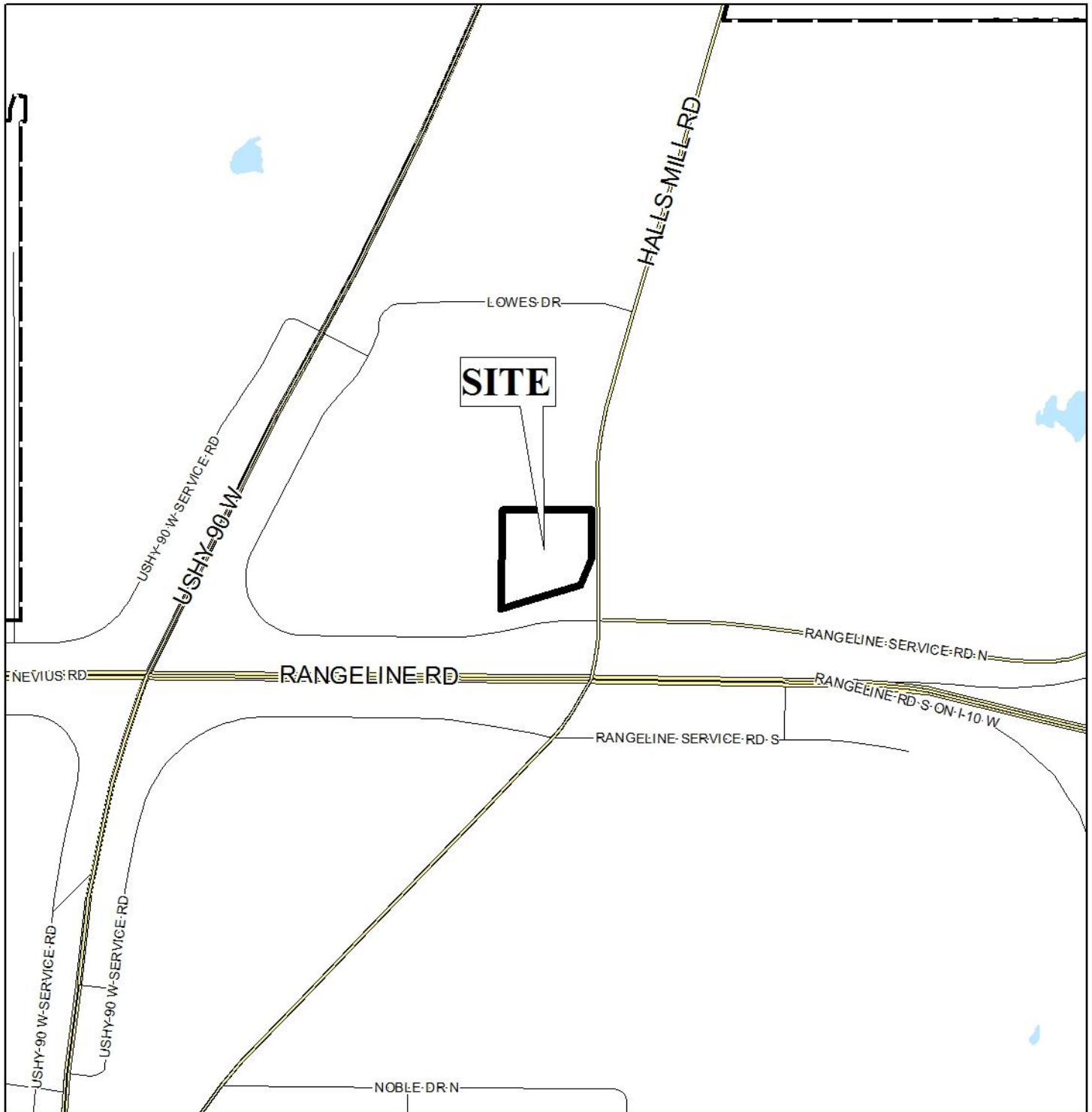
- a. the proposal promotes the objective of Creative design (to encourage innovative and diversified design in building form and site development), because it is reconfiguring a previous lot to allow more development;
- b. the proposal promotes the objective of Flexibility (to permit greater flexibility in the location and arrangement of buildings and uses than is generally possible under district regulations), because it is creating more business sites on an existing property;
- c. the proposal promotes the objective of Efficient land use (to encourage the most efficient and sustainable use of land, especially tracts in the inner part of the city that remain undeveloped or that are appropriate for redevelopment), because the site is being redeveloped, and this is revitalizing the property;
- d. the proposal promotes the objective of Environment (to preserve and protect as urban amenities the natural features and characteristics of the land), because it is minimizing new construction which will allow more undeveloped areas to remain undeveloped;
- e. the proposal promotes the objective of Open space (to encourage the provision of common open space through efficient site design), because it is reducing the requirement of more paved surfacing that would be required for a undeveloped lot adding new development;
- f. the proposal promotes the objective of Public services (to encourage optimum use of available public utilities, streets and community facilities), because it is utilizing a site that is already served with public infrastructure.

Based upon the preceding, this application is recommended for approval subject to the following:

- 1) revision of the site plan depict the total square footage of the outdoor seating space, and correction of the numbers listed for the parking requirements in the parking table on the site plan;
- 2) revision of the site plan to depict bumper stops on the last two parking spaces on Lot B;
- 3) revision of the site plan to depict bumper stops along the parking spaces adjacent to the drive-thru restaurant on Lot A if curbing is not provided;
- 4) revision of the site plan to depict the order pick-up window on Lot A;
- 5) revision of the site plan to depict a sidewalk on the property, or the submission of a sidewalk waiver;

- 6) compliance with frontage trees for the entire site, parking trees, and full landscaping compliance for the new construction;
- 7) retention of the lot sizes in square feet and acres;
- 8) retention of the 25' minimum building setback line;
- 9) placement of a note on the site plan stating that no structures shall be placed or constructed within any easements;
- 10) placement of a note on the site plan stating the site should be limited to the existing curb-cuts with size, location and design to be approved by Traffic Engineering, and ALDOT where appropriate, and conform to AASHTO standards;
- 11) compliance with Engineering comments: *"Retain NOTES #1 - #5 as shown on the submitted site plan."*;
- 12) compliance with Traffic Engineering comments: *"Rangeline Road and its Service Road are ALDOT maintained roadways. Site is limited to its existing shared curb cuts with any changes in size, location and design to be approved by ALDOT (where applicable) and Traffic Engineering and conform to AASHTO standards. Any new on-site parking, including ADA handicap spaces, shall meet the minimum standards as defined in Section 64-6 of the City's Zoning Ordinance. There are proposed changes to the intersection of Rangeline Service Road and Halls Mill Road to limit access from the service road to right-in, right-out only. Those changes in addition to the construction of an additional southbound left turn lane at the intersection of Halls Mill Road and Rangeline Road are the responsibility of a nearby development project."*;
- 13) compliance with Urban Forestry comments: *"Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 2015-116 and City Code Chapters 57 and 64)."*
- 14) compliance with Fire comments: *"All projects within the City Limits of Mobile shall comply with the requirements of the City of Mobile Fire Code Ordinance. (2012 International Fire Code)."*;
- 15) full compliance with all other municipal codes and ordinances; and
- 16) provision of a revised PUD site plan prior to signing the Final Plat.

# LOCATOR MAP



APPLICATION NUMBER 14 DATE March 1, 2018

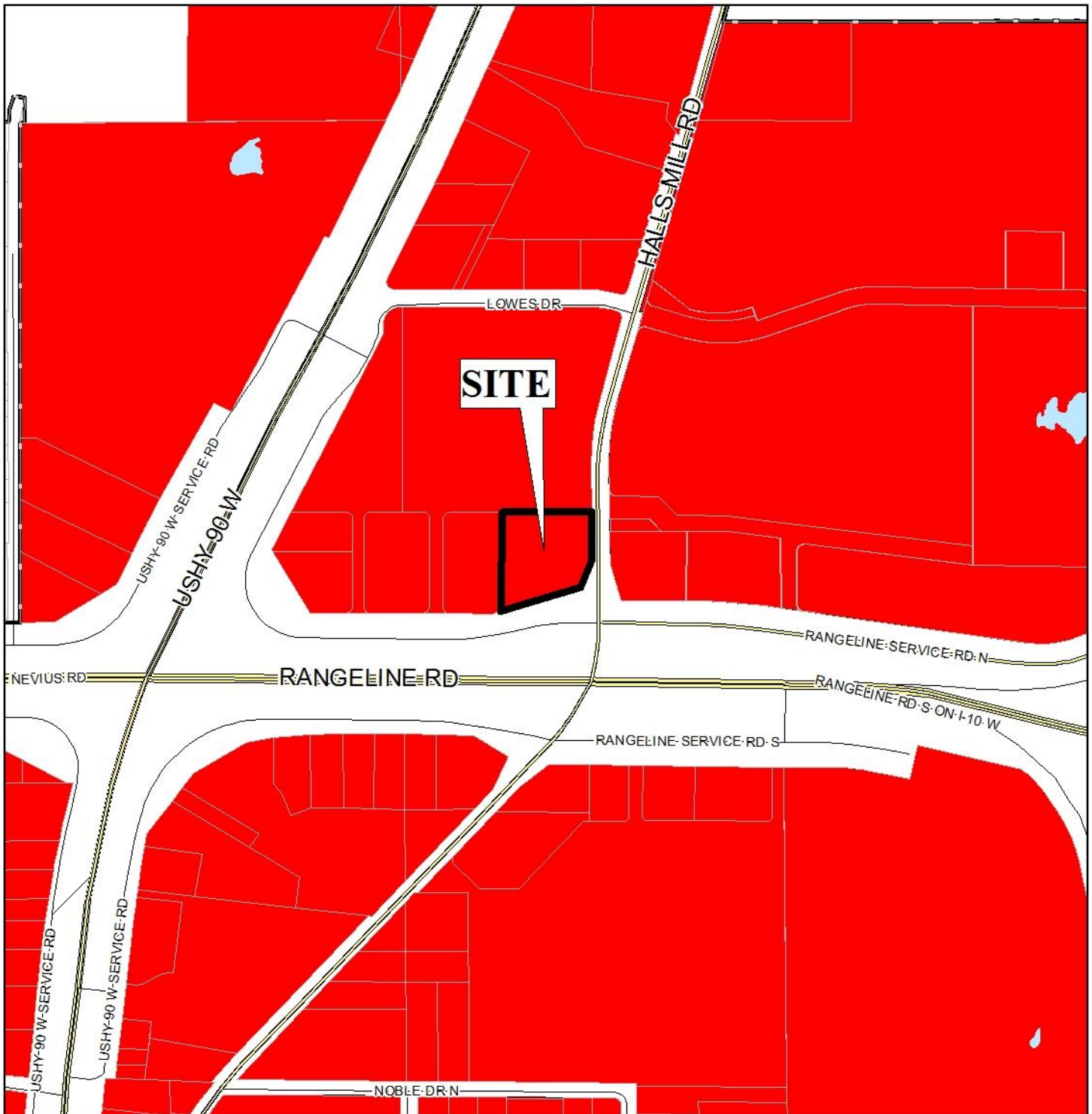
APPLICANT The Shoppes at Rangeline Subdivision

REQUEST Subdivision, Planned Unit Development



NTS

# LOCATOR ZONING MAP



APPLICATION NUMBER 14 DATE March 1, 2018

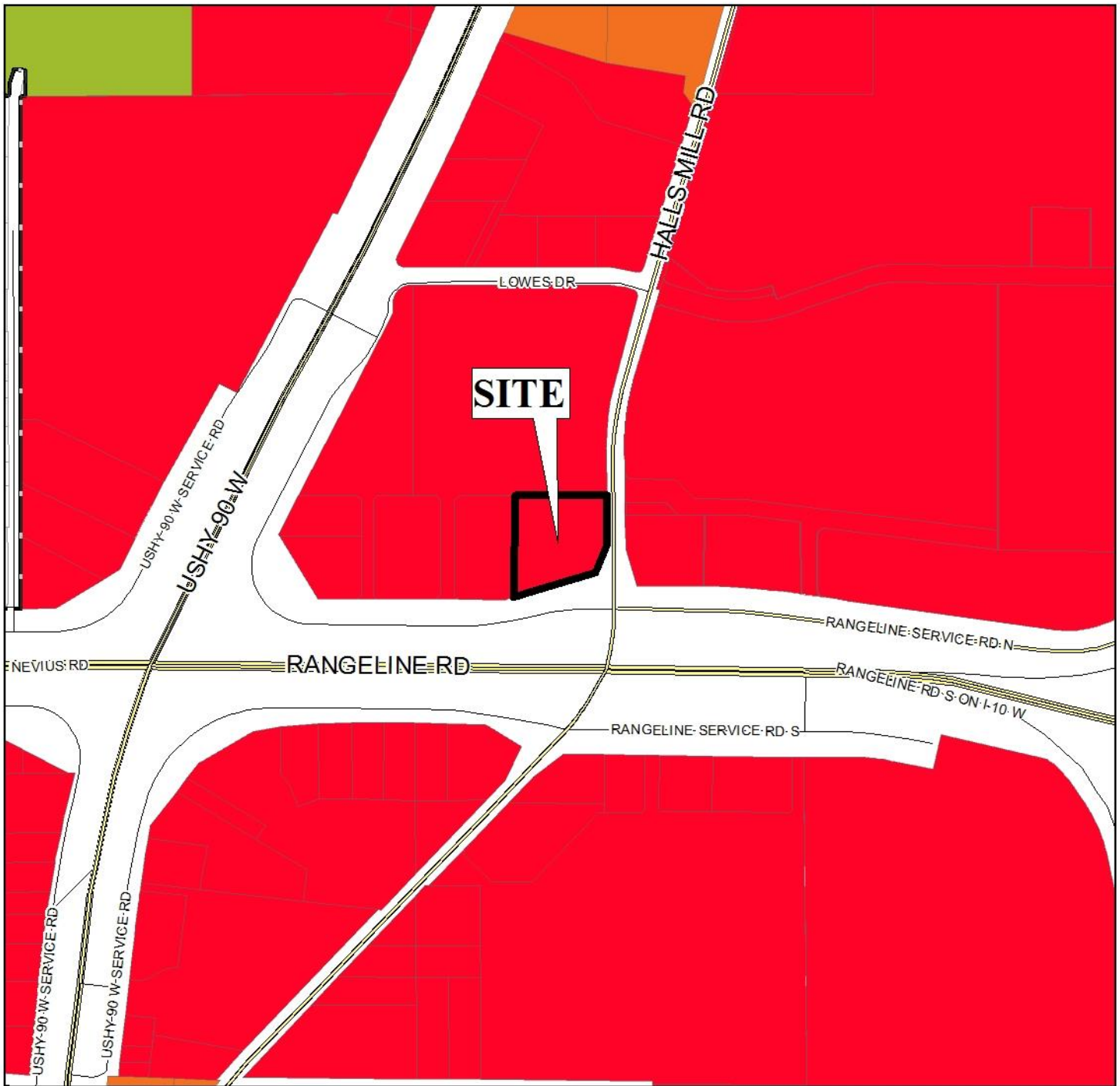
APPLICANT The Shoppes at Rangeline Subdivision

REQUEST Subdivision, Planned Unit Development



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# FLUM LOCATOR MAP



APPLICATION NUMBER 14 DATE March 1, 2018

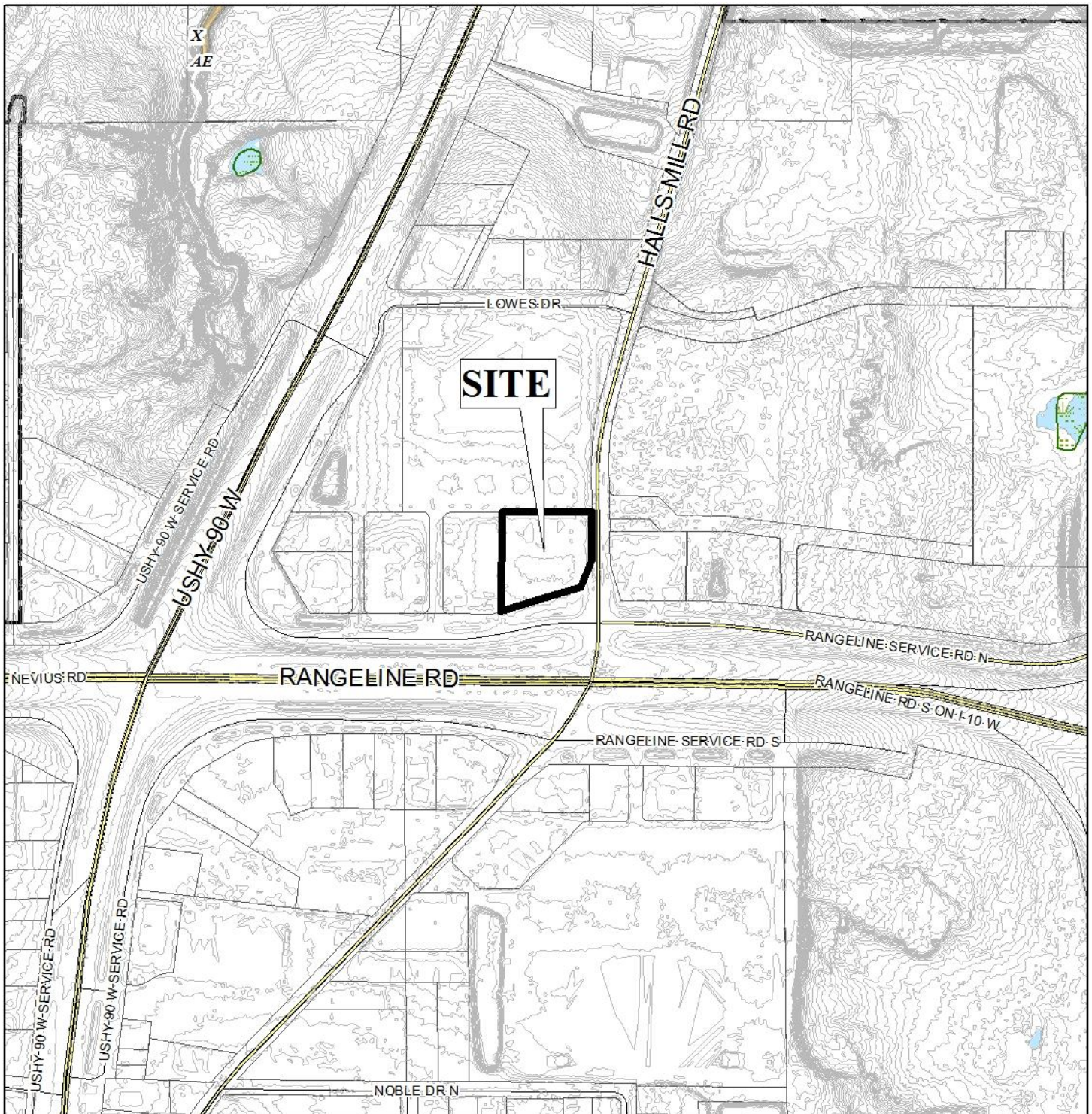
APPLICANT The Shoppes at Rangeline Subdivision

REQUEST Subdivision, Planned Unit Development

Low Density Residential	Neighborhood Center - Traditional	Downtown Waterfront	Parks & Open Space
Mixed Density Residential	Neighborhood Center - Suburban	Light Industry	Water Dependent
Downtown	Traditional Corridor	Heavy Industry	
District Center	Mixed Commercial Corridor	Institutional	



# ENVIRONMENTAL LOCATOR MAP



APPLICATION NUMBER 14 DATE March 1, 2018

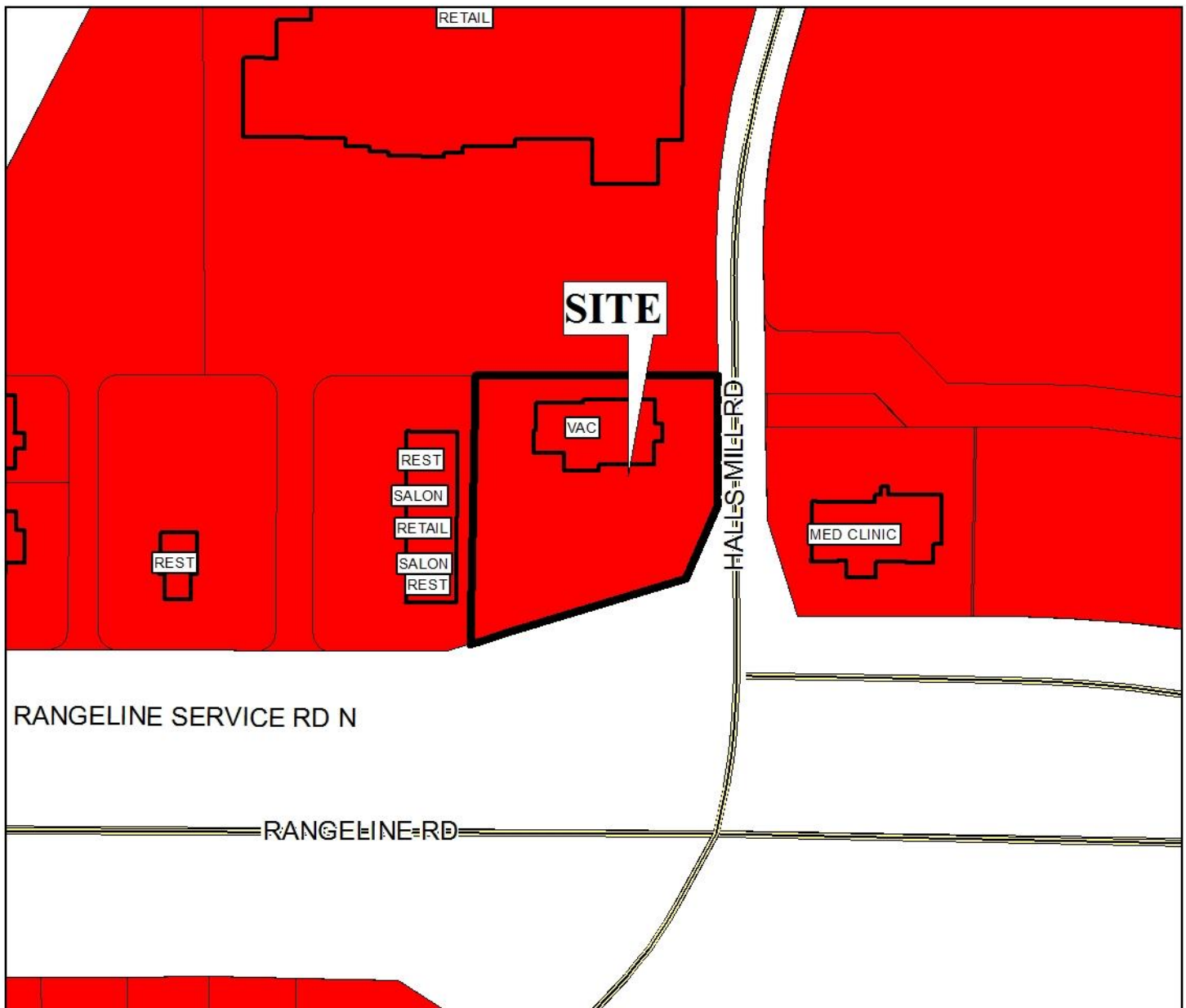
APPLICANT The Shoppes at Rangeline Subdivision

REQUEST Subdivision, Planned Unit Development



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# PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The site is surrounded by commercial units.

APPLICATION NUMBER 14 DATE March 1, 2018

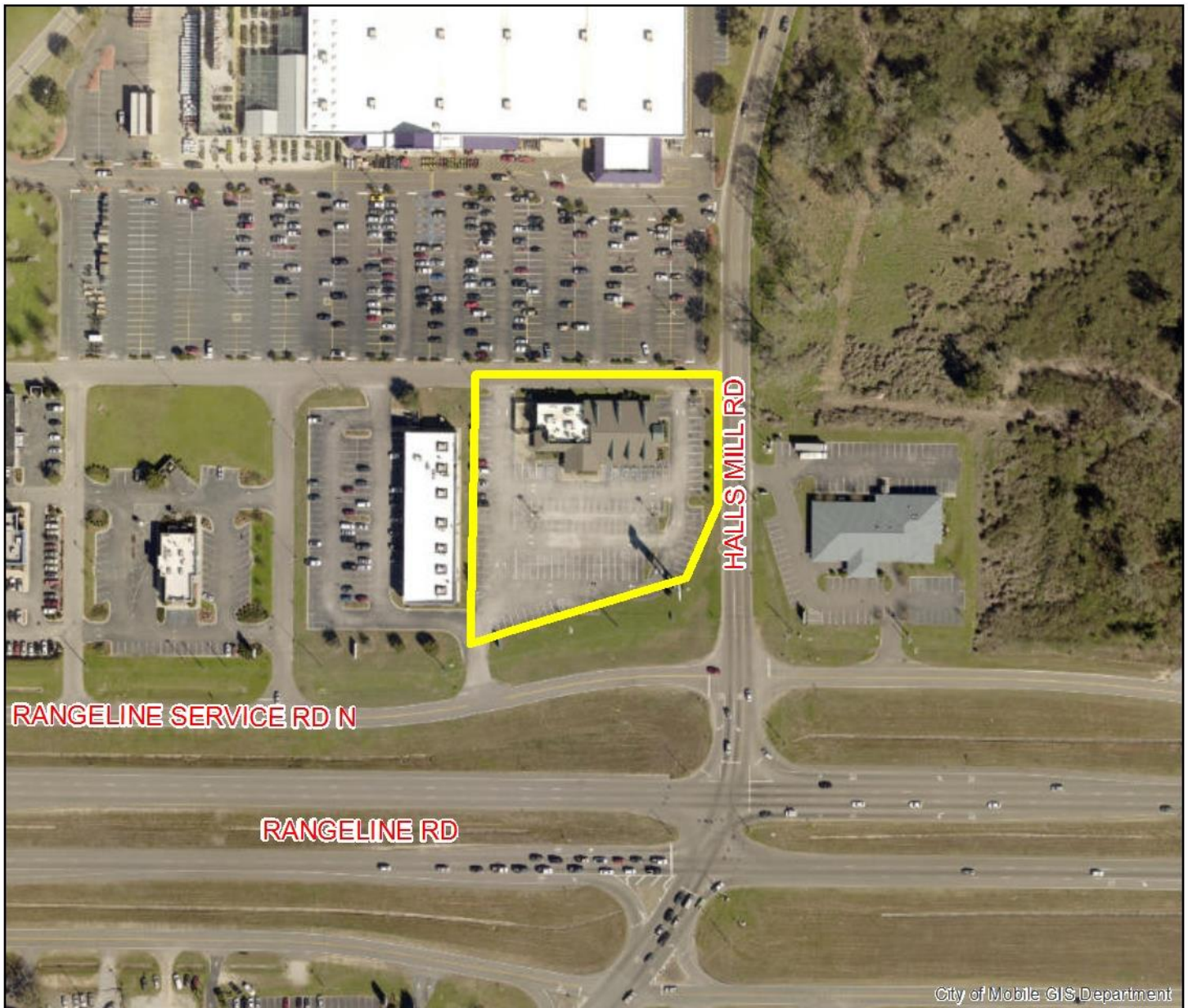
APPLICANT The Shoppes at Rangeline Subdivision

REQUEST Subdivision, Planned Unit Development

R-A	R-3	T-B	B-2	B-5	MUN	SD-WH	T5.1
R-1	R-B	B-1	B-3	I-1	OPEN	T3	T5.2
R-2	H-B	LB-2	B-4	I-2	SD	T4	T6



# PLANNING COMMISSION VICINITY MAP - EXISTING AERIAL



City of Mobile GIS Department

The site is surrounded by commercial units.

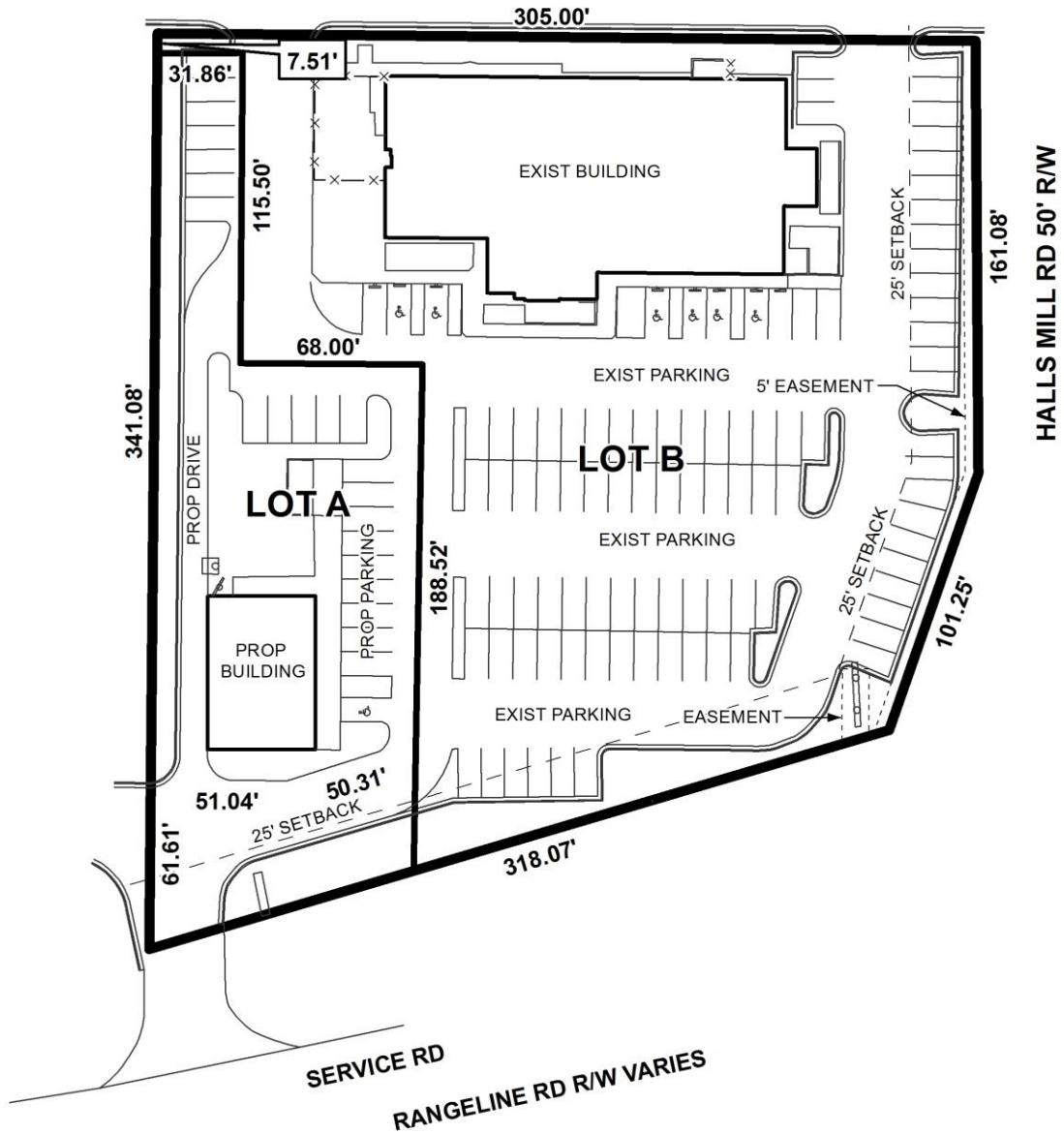
APPLICATION NUMBER 14 DATE March 1, 2018

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REQUEST Subdivision, Planned Unit Development



# SITE PLAN



The site plan illustrates the existing building, existing parking, setbacks, easements, proposed building, and proposed parking.

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