

DAVE'S FIRST ADDITION SUBDIVISION

Engineering Comments: It is the responsibility of the applicant to look up the site in the City of Mobile (COM) GIS system and verify if NWI wetlands are depicted on the site. If the COM GIS shows wetlands on the site, it is the responsibility of the applicant to confirm or deny the existence of wetlands on-site. If wetlands are present, they should be depicted on plans and/or plat, and no work/disturbance can be performed without a permit from the Corps of Engineers. Must comply with all storm water and flood control ordinances. Any work performed in the right of way will require a right of way permit.

Traffic Engineering Comments: Driveway number, size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards. Final approval for all driveways, proposed and existing will be given upon submittal of final plans.

Urban Forestry Comments: Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

Fire-Rescue Department Comments: Shall comply with Section 508.5.1 of the 2003 IFC.

MAWSS Comments: MAWSS has water and sewer services available, but a Capacity Assurance application for **additional** sewer service has not been applied for. MAWSS cannot guarantee **additional** sewer service until a Capacity application is approved by Volkert Engineering, Inc.

This application is a re-submittal of an application approved in December, 2006, which expired.

The plat illustrates the proposed 2-lot, 0.9± acre subdivision which is located on the North side of Lloyd Station Road, 360'± East of Interstate 10, in Council District 4. The subdivision is served by public water and sanitary services.

The purpose of this application is to subdivide two metes and bounds parcels into two legal lots of record. Currently, one of the metes and bonds parcels is technically landlocked, having no frontage on a public maintained road. The proposed subdivision would provide real property frontage for the landlocked parcel. While this frontage would create a flag lot, and Section V.D.1. of the Subdivision Regulations has been amended to include a general prohibition of flag lots, other flag lots existing in the immediate area would provide justification for this subdivision. Additionally, as stated above, it would be providing real property frontage on a public road alleviating the landlocked situation.

Tax Assessor's records indicate that the parcels have been in this configuration since the 1960's and have changed hands several times.

Both lots would have frontage on Lloyds Station Road with a compliant 60' right-of-way, therefore, no dedication would be required. As a means of access management, a note should be

required on the final plat stating that each lot is limited to one curb cut to Lloyd Station Road, with the size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards. Since both lots have rear frontage on Interstate 10, a note should be required on the final plat stating that access to Interstate 10 is denied.

The 25' minimum building setback line is shown on the plat along Lloyd Station Road for both proposed lots. This should also be shown on the final plat as it is on Lot 1, but should be removed for Lot 2 and only illustrated as it is "boxed" from the point where the "pole" meets the lot, providing 25' clear in all directions from that point.

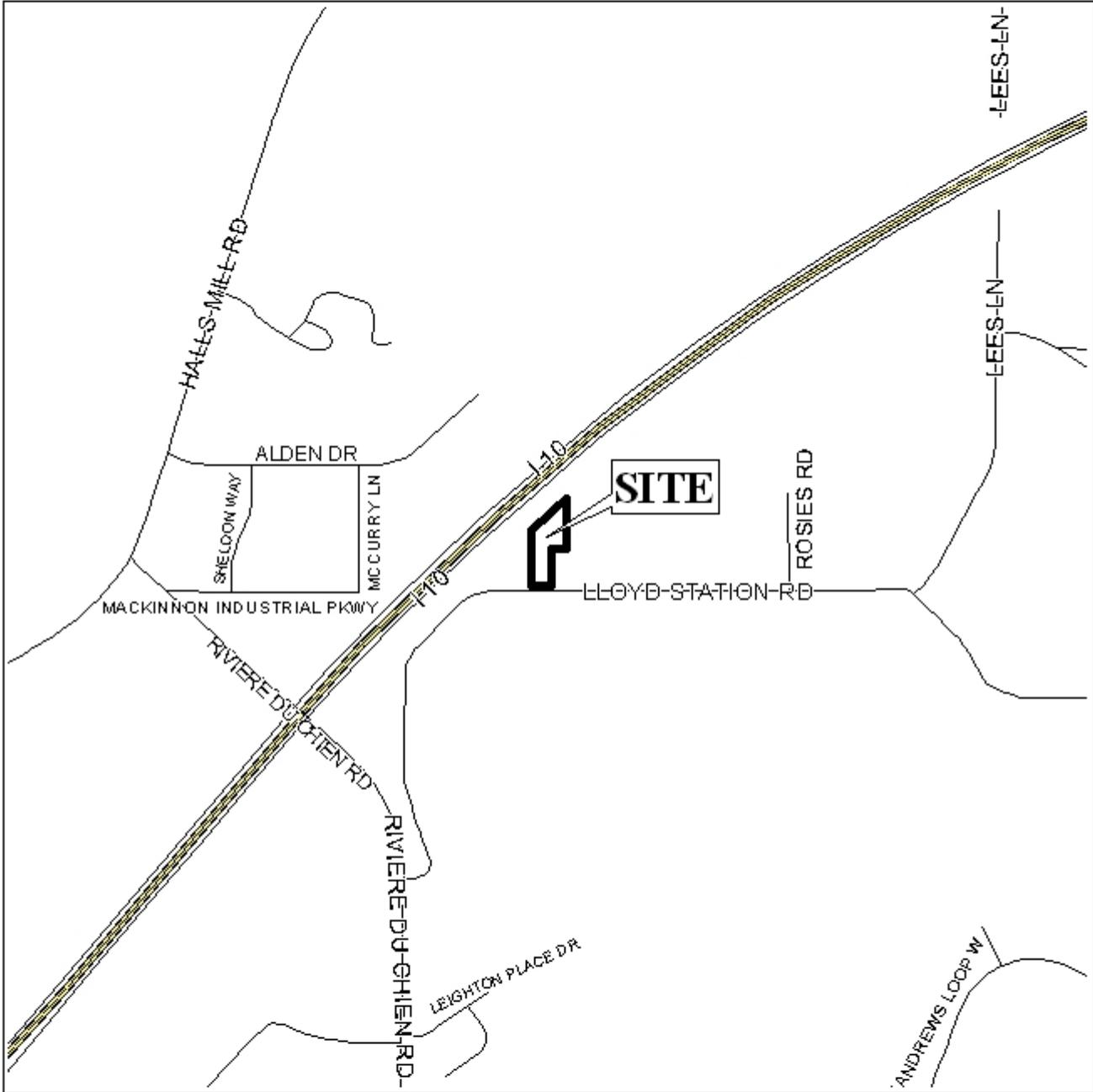
Each lot is labeled on the plat with its size in square feet. This should be revised on the final plat so that each lot is labeled with its size in acres and square feet, or a table should be provided furnishing the same information.

The geographic area defined by the City of Mobile and its planning jurisdiction, including this site, may contain Federally-listed threatened or endangered species as well as protected non-game species. Development of this site must be undertaken in compliance with all local, state and Federal regulations regarding endangered, threatened or otherwise protected species.

Based on the preceding, it is recommended that Section V.D.3. be waived and the plat be granted Tentative approval, subject to the following conditions:

- 1) placement of a note on the Final Plat stating that each lot is limited to one curb cut to Lloyd Station Road, with the size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards;
- 2) placement of a note on the Final Plat stating that both lots are denied access to Interstate 10;
- 3) illustration of the 25' building setback line along the road frontage of Lot 1;
- 4) illustration of the 25' building setback line for Lot 2 "boxed" from the point where the "pole" meets the lot, providing 25' clear in all directions from that point;
- 5) labeling of each lot with its size in acres and square feet, or the provision of a table on the plat furnishing the same information; and
- 6) placement of a note on the Final Plat stating that development of this site must be undertaken in compliance with all local, state and Federal regulations regarding endangered, threatened or otherwise protected species; and
- 7) subject to the Engineering Comments: *(It is the responsibility of the applicant to look up the site in the City of Mobile (COM) GIS system and verify if NWI wetlands are depicted on the site. If the COM GIS shows wetlands on the site, it is the responsibility of the applicant to confirm or deny the existence of wetlands on-site. If wetlands are present, they should be depicted on plans and/or plat, and no work/disturbance can be performed without a permit from the Corps of Engineers. Must comply with all storm water and flood control ordinances. Any work performed in the right of way will require a right of way permit).*

LOCATOR MAP



APPLICATION NUMBER 12 DATE May 1, 2008

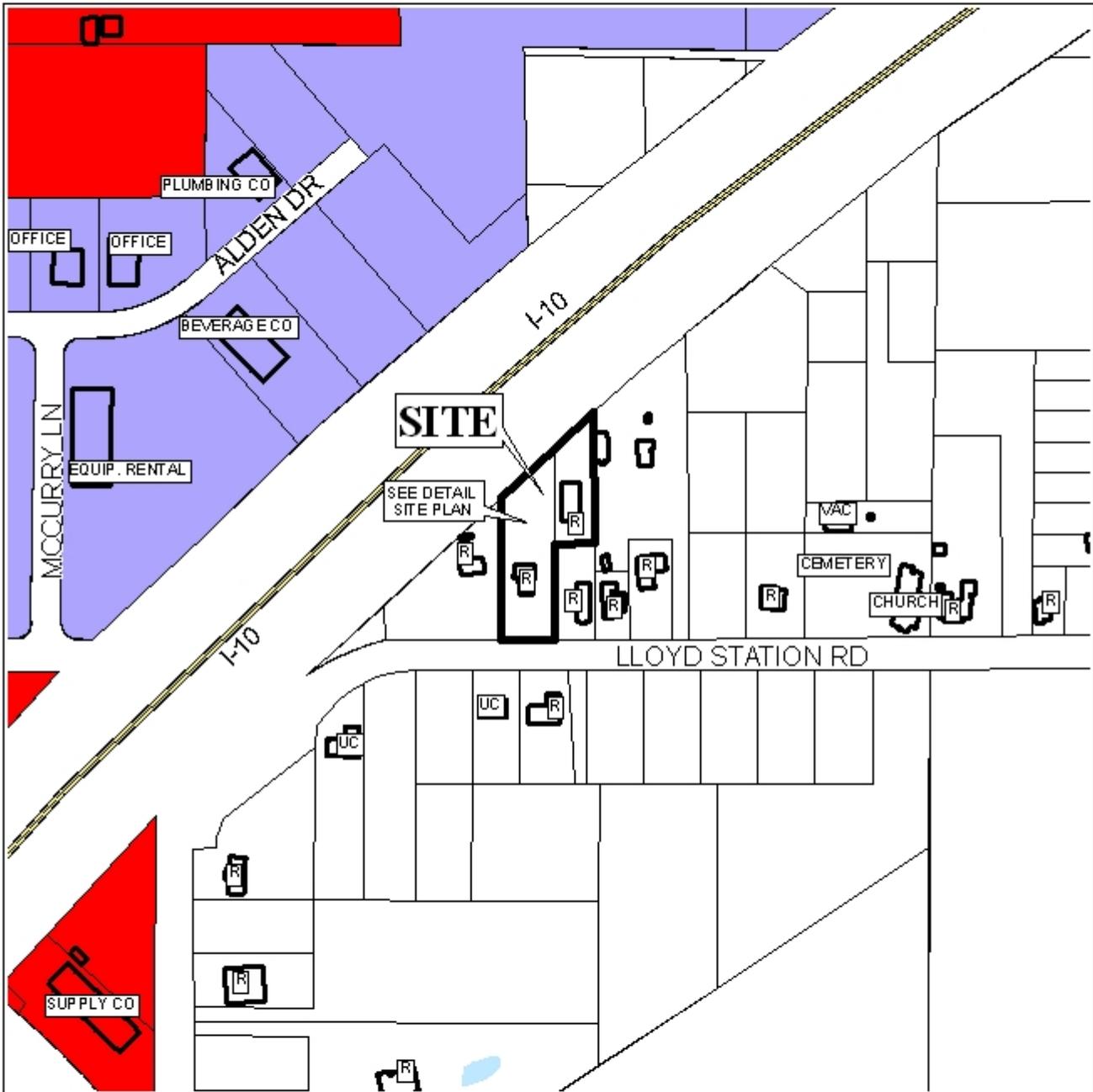
APPLICANT Dave's First Addition Subdivision

REQUEST Subdivision



NTS

DAVE'S FIRST ADDITION SUBDIVISION



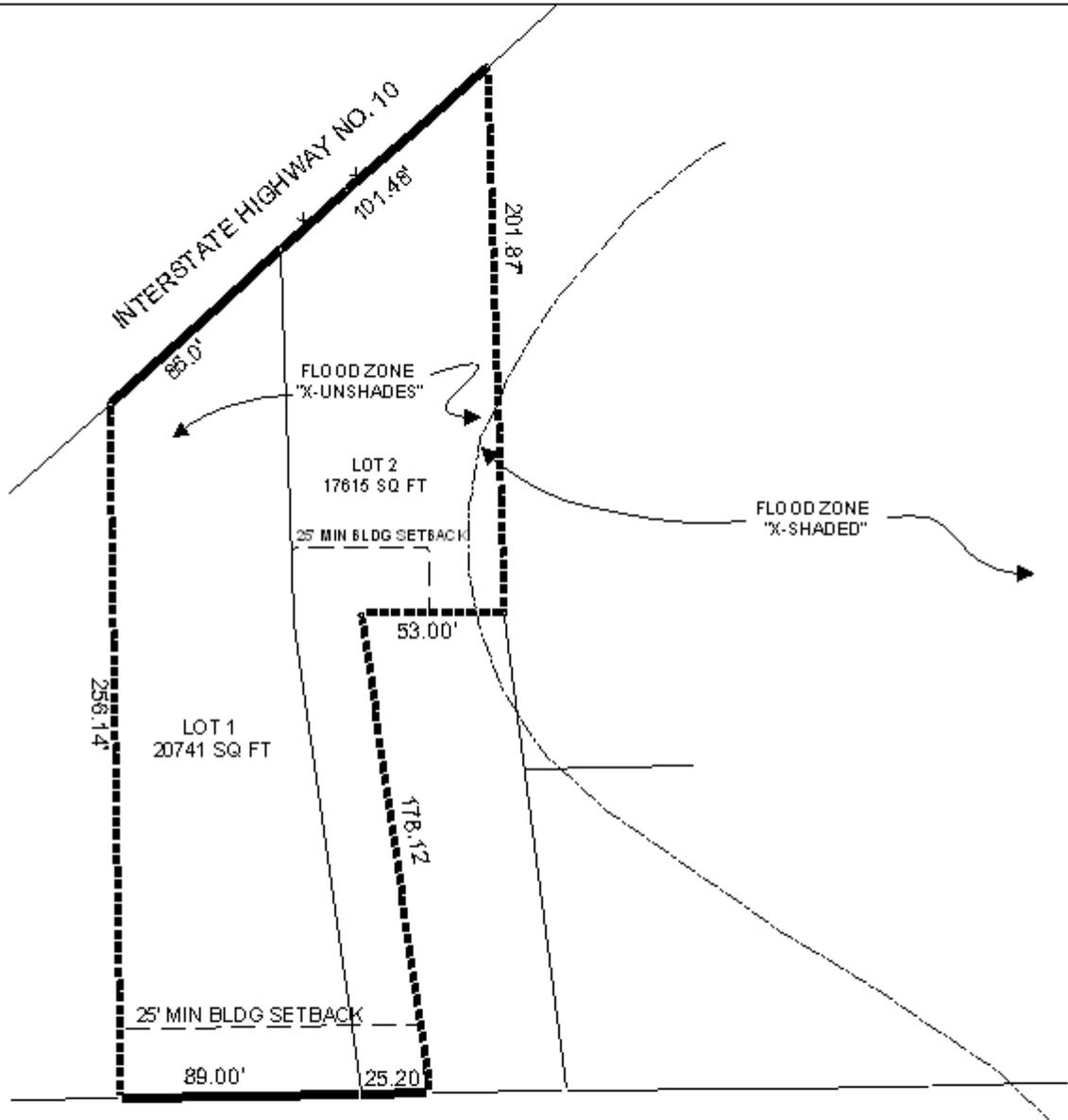
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LEGEND

R-1	R-2	R-3	R-A	R-B	H-B	T-B	B-1	LB-2	B-2	B-3	B-4	B-5	I-2



DETAIL SITE PLAN



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