

**PLANNED UNIT DEVELOPMENT
& SUBDIVISION STAFF REPORT****Date: March 6, 2014****DEVELOPMENT NAME** Mock Subdivision**SUBDIVISION NAME** Mock Subdivision**LOCATION** 6377 Old Shell Road
(Southeast corner of Old Shell Road and Hillcrest Road)**CITY COUNCIL
DISTRICT** District 6**AREA OF PROPERTY** 2 Lots / 2.4± Acres**CONTEMPLATED USE** Planned Unit Development Approval to allow multiple buildings on a single building site and shared access between two building sites, and Subdivision approval to create two legal lots of record from two existing metes and bounds parcels.**TIME SCHEDULE
FOR DEVELOPMENT** Upon approval.**ENGINEERING**

COMMENTS **Subdivision:** The following comments should be addressed prior to acceptance and signature by the City Engineer: A. Provide all of the required information on the SUBDIVISION PLAT (i.e. signature blocks, signatures, certification statements, written legal description, required notes, legend, scale, bearings and distances). B. Add a note to the SUBDIVISION PLAT stating that a Land Disturbance permit will be required for any land disturbing activity in accordance with the of the Storm Water Management and Flood Control Ordinance (Mobile City Code, Chapter 17 , Ordinance #65-007 & #65-045), latest edition. Storm water detention will be required for any proposed land disturbing activity. C. Add a note to the SUBDIVISION PLAT stating that as shown on the 1984 aerial photo (FLIGHT 19 - # 72) this lot will receive historical credit of impervious area towards storm water detention requirement per the Storm Water Management and Flood Control Ordinance (Mobile City Code, Chapter 17, Ordinance #65-007 & #65-045), latest edition. Coordinate exact amount with City Engineering Department staff prior to submitting the Land Disturbance Permit application. D. Add a note to the Plat stating that the approval of all applicable federal, state, and local agencies (including all storm water runoff, wetland and floodplain requirements) will be required prior to the issuance of a Land Disturbance permit. E. Applicant must complete the ROW vacation process prior to receiving the City and Traffic Engineer's signatures on the Subdivision Plat. F. Show the recording information for the vacated public ROW. G. Provide a written legal description for the subdivision, and matching bearing and distance labels, conforming to the approved Vacation of ROW. H. Provide and label the monument set or found at each subdivision corner. I. Add a signature block for the Owner, Notary Public, Planning Commission, Traffic Engineer, City Engineer, and County Engineer. J. Provide the Surveyor's Certificate and Signature. K. Provide the Surveyor's, Owner's (notarized), Planning

Commission, and Traffic Engineering signatures. L. Add a note that sidewalk is required to be constructed along the frontage of each lot, or parcel, at time of development, unless a sidewalk waiver is approved.

Planned Unit Development: 1. Applicant must complete the ROW vacation process prior to receiving Engineering approval for any Land Disturbance Permit or ROW Permit. 2. Any work performed in the existing ROW (right-of-way) such as driveways, sidewalks, utility connections, grading, drainage, irrigation, or landscaping will require a ROW permit from the City of Mobile Engineering Department (208-6070) and must comply with the City of Mobile Right-of-Way Construction and Administration Ordinance (Mobile City Code, Chapter 57, Article VIII). 3. Any and all proposed land disturbing activity within the property will need to be in conformance with the Storm Water Management and Flood Control Ordinance (Mobile City Code, Chapter 17, Ordinance #65-007 & #65-045); the City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control. 4. Add a note to the PUD Plan stating that the proposed development must comply with all Engineering Department Policy Letters: i. 5-13-2009 Policy Letter (Car wash drains and dumpster pads to drain to Sanitary Sewer System) ii. 8-4-2004 Policy Letter (Video inspection of new Storm Sewer System Piping)

TRAFFIC ENGINEERING

COMMENTS

A traffic impact study will need to be conducted for this site, based on its development intensity and location at a high volume intersection. The site is limited to one curb-cut on Old Shell Road (right-in, right-out only), and one curb-cut to Hillcrest Road, with size, location and design to be approved by Traffic Engineering and conform to AASHTO standards. Right-of-way vacation should be modified, as discussed with the Traffic Engineering Director, to address the need for access to traffic signal equipment. All on-site parking, including ADA handicap spaces, shall meet the minimum standards as defined in Section 64-6 of the City's Zoning Ordinance.

URBAN FORESTRY

COMMENTS

Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

Preservation status is to be given to the 72", 51" and 45" Live Oak Trees located on the center of the proposed development. Any work on or under these trees is to be permitted and coordinated with Urban Forestry; removal to be permitted only in the case of disease or impending danger.

FIRE DEPARTMENT

COMMENTS

All projects within the City of Mobile Fire Jurisdiction must comply with the requirements of the 2009 International Fire Code, as adopted by the City of Mobile.

REMARKS

The applicant is seeking Planned Unit Development Approval to allow multiple buildings on a single building site and shared access between two building sites, and Subdivision approval to create two legal lots of record from two existing metes and bounds

parcels. The site is located in Council District 6, and according to the applicant is served by public water and sanitary sewer.

Planned Unit Development review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating excess traffic along minor residential streets in residential districts outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD. PUD approval is site plan specific, thus any changes to the site plan / Subdivision plat will require approval by the Planning Commission.

Subdivision review examines the site with regard to promoting orderly development, protecting general health, safety and welfare, and ensuring that development is correlated with adjacent developments and public utilities and services, and to ensure that the subdivision meets the minimum standards set forth in the Subdivision Regulations for lot size, road frontage, lot configuration, etc.

The site currently consists of two metes-and-bounds parcels. The Eastern parcel was rezoned from R-1, Single-Family Residential, to B-2 in 1987. Along with that rezoning, dedication was required to provide 50' from the centerline of Old Shell Road in compliance with the Major Street Plan, and dedication to provide an additional 40 feet along Old Shell Road for a parallel service road. 60' was actually dedicated for the service road. In 1994, the Western parcel was rezoned from R-1 to B-2, and dedication was required resulting in the current compliant rights-of-way along Old Shell Road and Hillcrest Road, according to the Major Street Plan. The current 136' radius at the intersection of Old Shell Road and Hillcrest Road was also acquired via required dedication of that rezoning. Other conditions of that rezoning were the restriction of ingress and egress to one driveway on each thoroughfare to be located at the Easternmost and Southernmost boundaries of the property and that the current subdivision at that time be vacated resulting in one lot and any future subdivision requiring service road dedication. In 1997, rezoning from B-2 to B-3, Community Business, was requested, but was recommended for denial by the Planning Commission, and the denial was affirmed by the City Council.

The proposed lots, as depicted, meet the minimum size and frontage requirements as regulated by the Subdivision Regulations. The 25-foot minimum building setback lines are not depicted; therefore, the plat should be revised to depict the 25' minimum building setback line along both street frontages. As vacation of right-of-way of the previously dedicated service road and corner radius are proposed, the 25' building setback line should be measured from the right-of-way following vacation. The lots are labeled with their areas in square feet and acres and this should also be done on the Final Plat, or a table should be furnished on the Final Plat providing the same information.

Lot B would exceed the depth-to-width ratio allowed by Section V.D.3. of the Subdivision Regulations; however, as this is entirely an existing metes-and-bounds parcel, a waiver of that section would be in order.

As dedications were previously made along both street frontages to meet the Major Street Plan requirements, no further dedication would be required. And as vacations of right-of-way are proposed, the Final Plat should not be signed until the Vacation process for each have been completed.

The condition of the 1994 rezoning for the Western parcel restricting its ingress and egress to one driveway on each thoroughfare to be located at the Easternmost and Southernmost boundaries of the property still stands. Due to changing traffic conditions in the area, Traffic Engineering has determined that Hillcrest Road should be limited to the one Southernmost curb cut to Hillcrest Road. Traffic Engineering has also determined that the Old Shell Road curb cut to Lot B should be restricted to right-in, right-out traffic only. Notes reflecting those restrictions, with the size, design, and location to be approved by Traffic Engineering and conform to AASHTO standards, should be required on the Final Plat, if approved. Traffic Engineering has also determined that a Traffic Impact Study would be required for the intersection of Old Shell Road and Hillcrest Road and for the site driveways.

The site was never developed following the two rezonings. The applicant now proposes to construct a convenience store and restaurant, fuel island canopy, and tunnel car wash on Lot A, and a vehicle vacuuming facility under awning on Lot B. The two lots would share access with each other and the two access lanes for the car wash would be on Lot B.

The site plan submitted indicates two proposed accesses from Hillcrest Road onto Lot A. As previously mentioned, the 1994 rezoning for that portion of the site restricted access along Hillcrest Road to the Southernmost boundary of the property, and Traffic Engineering has also limited the Hillcrest Road access to that one drive. The Old Shell Road drive is indicated to be one-lane ingress and two-lane egress indicating that exiting traffic could turn left or right onto Old Shell Road. However, Traffic Engineering has determined that traffic should be limited to right-turn-in and right-turn-exit only for this drive.

The site plan indicates several large trees on the site, but does not identify types and indicates that all trees on the site are to be removed. Urban Forestry has determined that Preservation status is to be given to the 72", 51" and 45" Live Oak Trees located on the center of the proposed development. Any work on or under these trees is to be permitted and coordinated with Urban Forestry; removal to be permitted only in the case of disease or impending danger. Two of the trees are within proposed paved access aisles, and one is within the area of the fuel island canopy. The site plan provides total landscaping calculations but no frontage landscaping calculations. Also, tree planting calculations do not provide for parking trees. The site coverage calculations are based on the store and car wash only and do not include the fuel island canopy, vacuuming facility awning or the car wash entrance awning. Parking calculations for the store/restaurant are one space short. A compliant dumpster is indicated as is a City-standard sidewalk within the right-of-way.

The site plan includes those areas proposed to be vacated along Old Shell Road and Hillcrest Road. As Traffic Engineering has expressed concerns relating to the scope of the proposed rights-of-way vacation, further coordination with Traffic Engineering should be made to revise the preliminary plat and site plan.

RECOMMENDATION

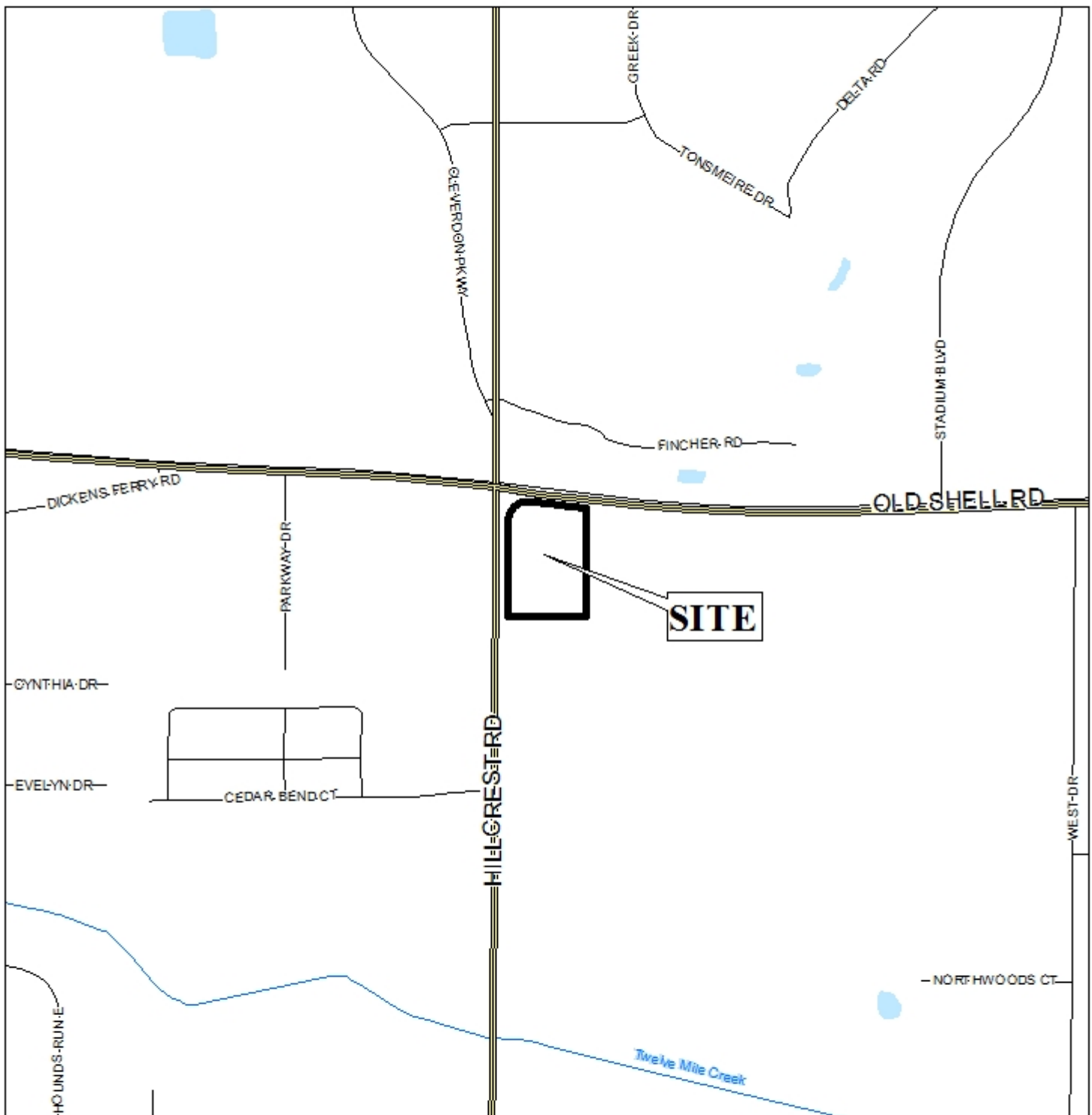
Subdivision: Due to the concerns expressed by Traffic Engineering, this application is recommended for holdover to the April 3rd meeting, with revisions due no later than March 17th, to allow the applicant to address the following issues:

- 1) revision of the plat to indicate modified right-of-way vacations which would be acceptable to Traffic Engineering to address the need for access to traffic signal equipment; and
- 2) submission of a Traffic Impact Study based on the site's development intensity and location at a high volume intersection.

Planned Unit Development: Due to the concerns expressed by Traffic Engineering and the numerous revisions required of the site plan, this application is recommended for holdover to the April 3rd meeting, with revisions due no later than March 17th, to allow the applicant to address the following issues:

- 1) revision of the site plan to include modified right-of-way vacations which would be acceptable to Traffic Engineering to address the need for access to traffic signal equipment;
- 2) submission of a Traffic Impact Study based on the site's development intensity and location at a high volume intersection;
- 3) revision of the site plan to one curb-cut on Old Shell Road (right-in, right-out only), and one curb-cut to Hillcrest Road, with size, location and design to be approved by Traffic Engineering and conform to AASHTO standards;
- 4) revision of the site plan to specify handicap parking meeting ADA compliance;
- 5) revision of the site plan to allow Preservation status for the 72", 51" and 45" Live Oak Trees located on the center of the proposed development, as per the Urban Forestry comments;
- 6) revision of the site plan to identify the species of all existing trees on the site;
- 7) revision of the landscaping calculations to also include frontage landscaping required, and revision of the site plan to indicate such areas;
- 8) revision of the tree planting requirement calculations to provide required parking area trees;
- 9) revision of the site coverage calculations to include all proposed structures (store/restaurant, car wash, fuel island canopy, vacuum facility awning and car wash entrance awning); and
- 10) revision of the parking calculations for the store/restaurant to 20 spaces required.

LOCATOR MAP



APPLICATION NUMBER 12 & 13 DATE March 6, 2014

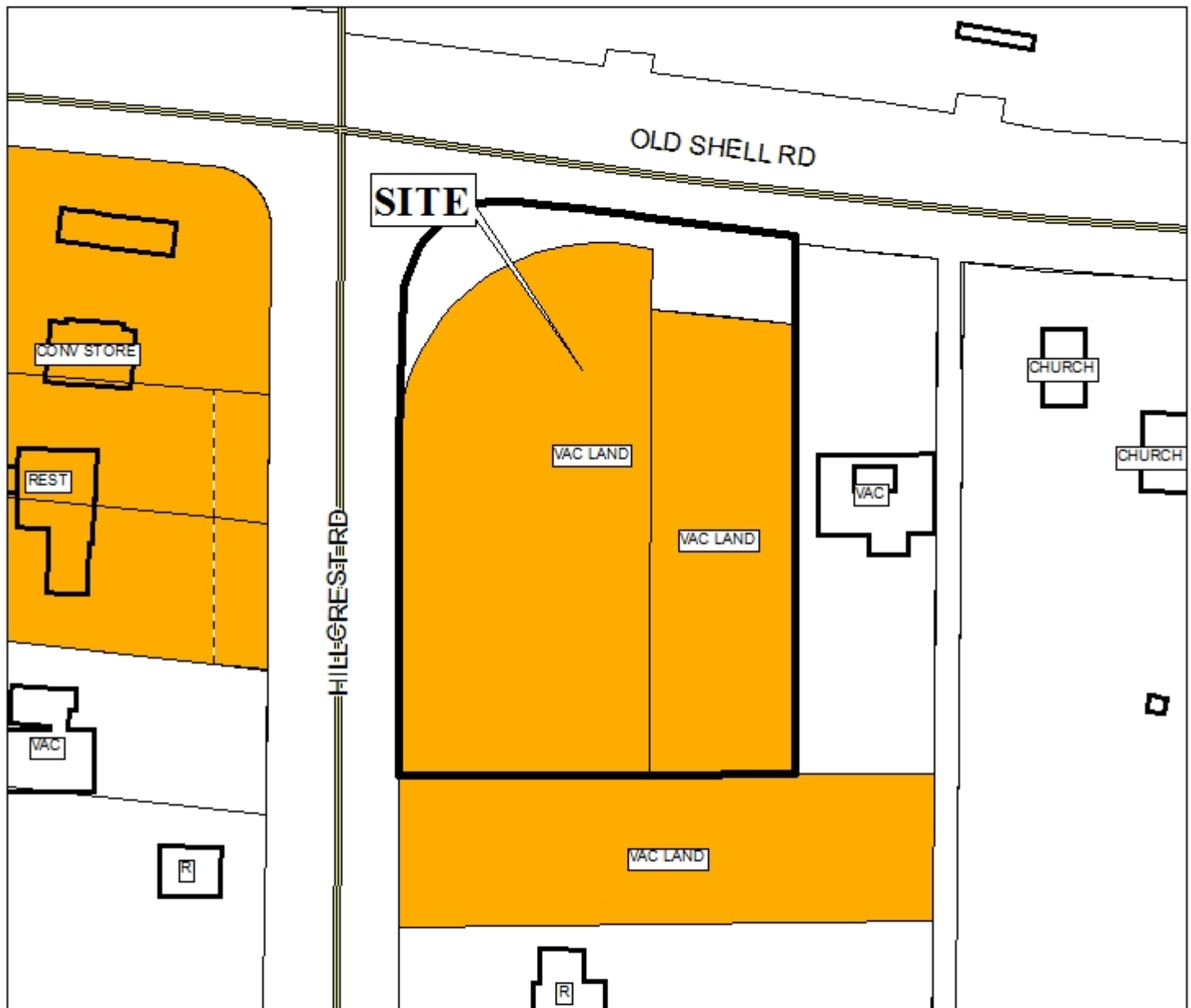
APPLICANT Mock Subdivision

REQUEST Subdivision, Planned Unit Development



NTS

PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The site is surrounded by miscellaneous commercial and residential units.

APPLICATION NUMBER 12 & 13 DATE March 6, 2014

APPLICANT Mock Subdivision

REQUEST Subdivision, Planned Unit Development

LEGEND

R-1	R-2	R-3	R-A	R-B	H-B	T-B	B-1	LB-2	B-2	B-3	B-4	B-5	I-1
												I-2	



PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The site is surrounded by miscellaneous commercial and residential units.

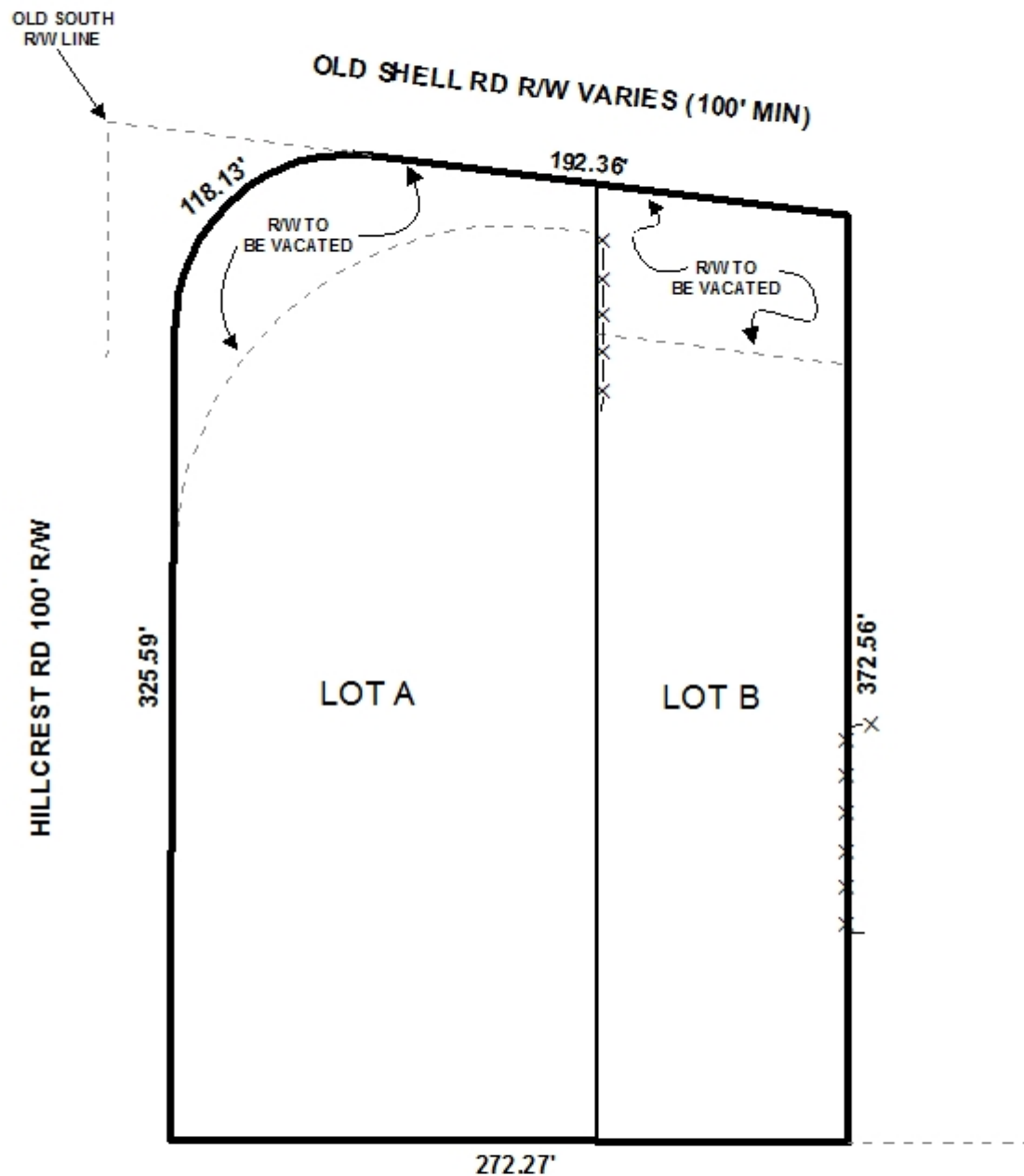
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APPLICANT Mock Subdivision

REQUEST Subdivision, Planned Unit Development



DETAIL SITE PLAN



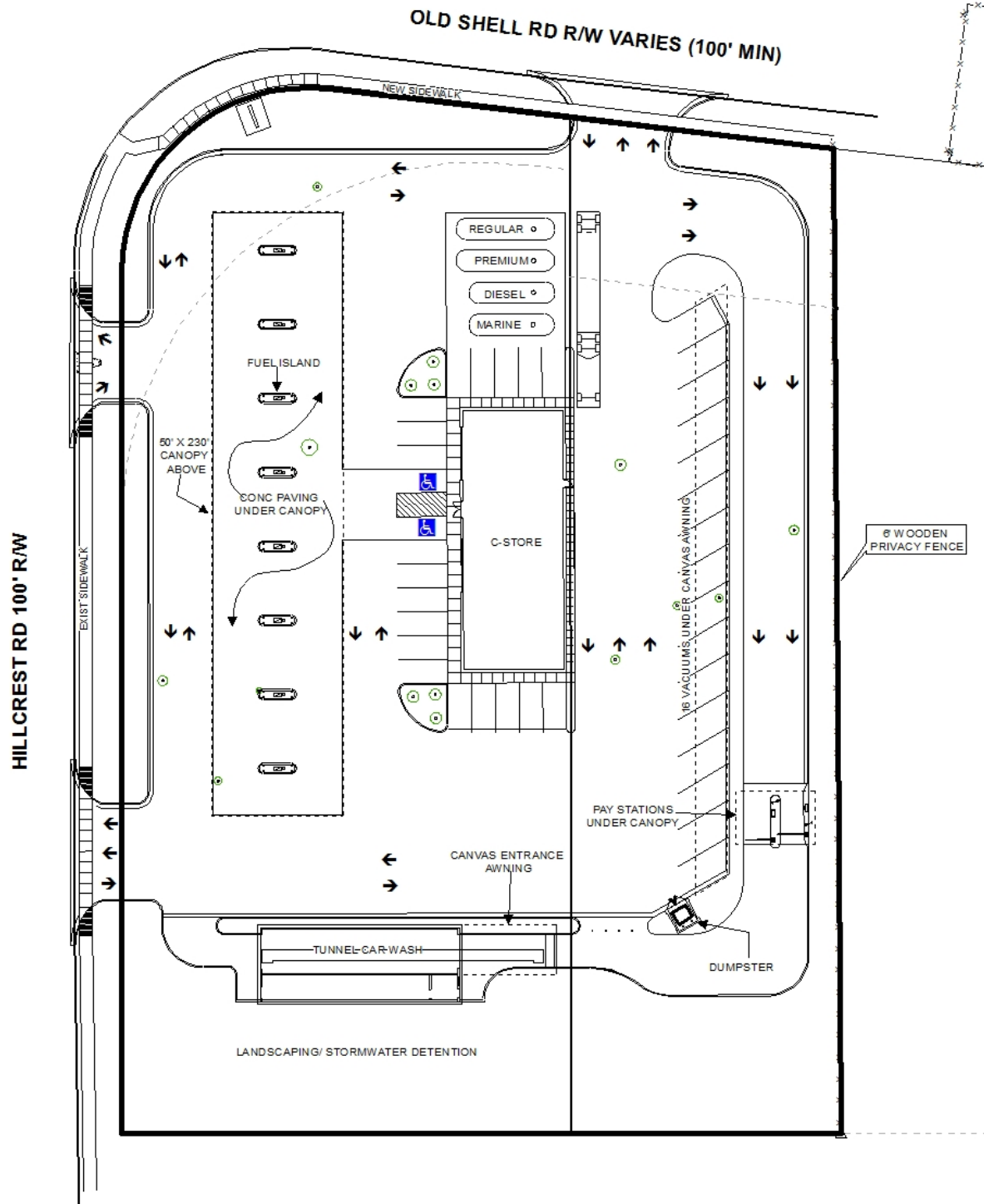
APPLICATION NUMBER 12 & 13 DATE March 6, 2014

APPLICANT Mock Subdivision

REQUEST Subdivision, Planned Unit Development



SITE PLAN



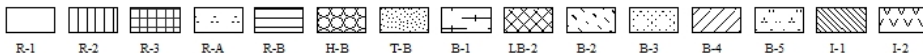
The site plan illustrates the proposed convenience store, new sidewalk, fuel tanks, canopies, and car wash area.

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REQUEST Subdivision, Planned Unit Development

LEGEND



NTS