

SPRINGHILL PLACE SUBDIVISION

Engineering Comments: Stormwater detention is required for this development and should be constructed via the Land Disturbance permitting process prior to submittal of the final plat. The common area for stormwater detention should be shown on the plat. In addition, public services will only be available to those lots that front on an opened public Rights of Way. Must comply with all stormwater and flood control ordinances. Any work performed in the right of way will require a right of way permit.

Traffic Engineering Comments: Driveway number, sizes, location and design to be approved by Traffic Engineering and conform to AASHTO standards.

Urban Forestry Comments: Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64). Preservation status is to be given to the 55" Live Oak Tree located between the proposed property line of Lot 6, Lot 7 and Cul-de-sac for Private Road. Any work on or under this tree is to be permitted and coordinated with Urban Forestry; removal to be permitted only in the case of disease or impending danger. Preservation of this tree will require a redesign of the private road near the tree.

The plat illustrates the proposed 2.8± acre, 8-lot subdivision located at the Northwest corner of Bishop Lane North and Broadway Drive. The site is served by public water and sanitary sewer.

The purpose of the application is the creation of an eight-lot private street subdivision from a metes and bounds parcel. The plat also illustrates reduced setbacks for one lot. Subdivision applications that include private streets and reduced setbacks are required to be accompanied by a Planned Unit Development (PUD) application, if the site is located in the city. The applicant had submitted an application for an Administrative PUD, but this was not accepted, since it involves multiple dwellings and the creation of a new private street; the applicant was informed at that time that a full PUD application was required to accompany the subdivision application.

The site fronts Bishop Lane North, with a 50' right-of-way, and Broadway Drive, which is not open at the site, and the right-of-way of which varies from 33 feet to 41.5'. As this is substandard in width, dedication of sufficient right-of-way to provide 25 feet from the centerline of Broadway Drive would be required.

The proposed new street is labeled a "35' non-exclusive ingress/egress easement for private road". However, an easement is not acceptable; there must be private street right-of-way. Additionally, Section VIII.E.2(c) of the Subdivision Regulations requires private streets with two lanes to have a minimum right-of-way of 50 feet and a minimum pavement width of 18 feet with four feet of paved wings. Section VIII of the Subdivision Regulations permits modifications to standard regulatory circumstances, where the normal application of the Regulations would cause undue hardship; 2) Innovative design, which can include cluster or Traditional Neighborhood Development subdivisions, as well as townhomes, terrace houses, multi-family projects, and commercial development; or 3) Family subdivisions, which allow a private street to served up to

five lots. The applicant has not indicated if there are unusual difficulties that would result in a hardship, and has not presented a subdivision plat that reflects features associated with innovative design, e.g., a gated subdivision; documentation of these circumstances would be required to justify consideration for a private street subdivision.

The required 25' building setback lines are shown along the existing streets and for most lots along the proposed street; however, proposed Lot 6 is shown with a 15' setback line, and with an existing building that extends to the front property line.

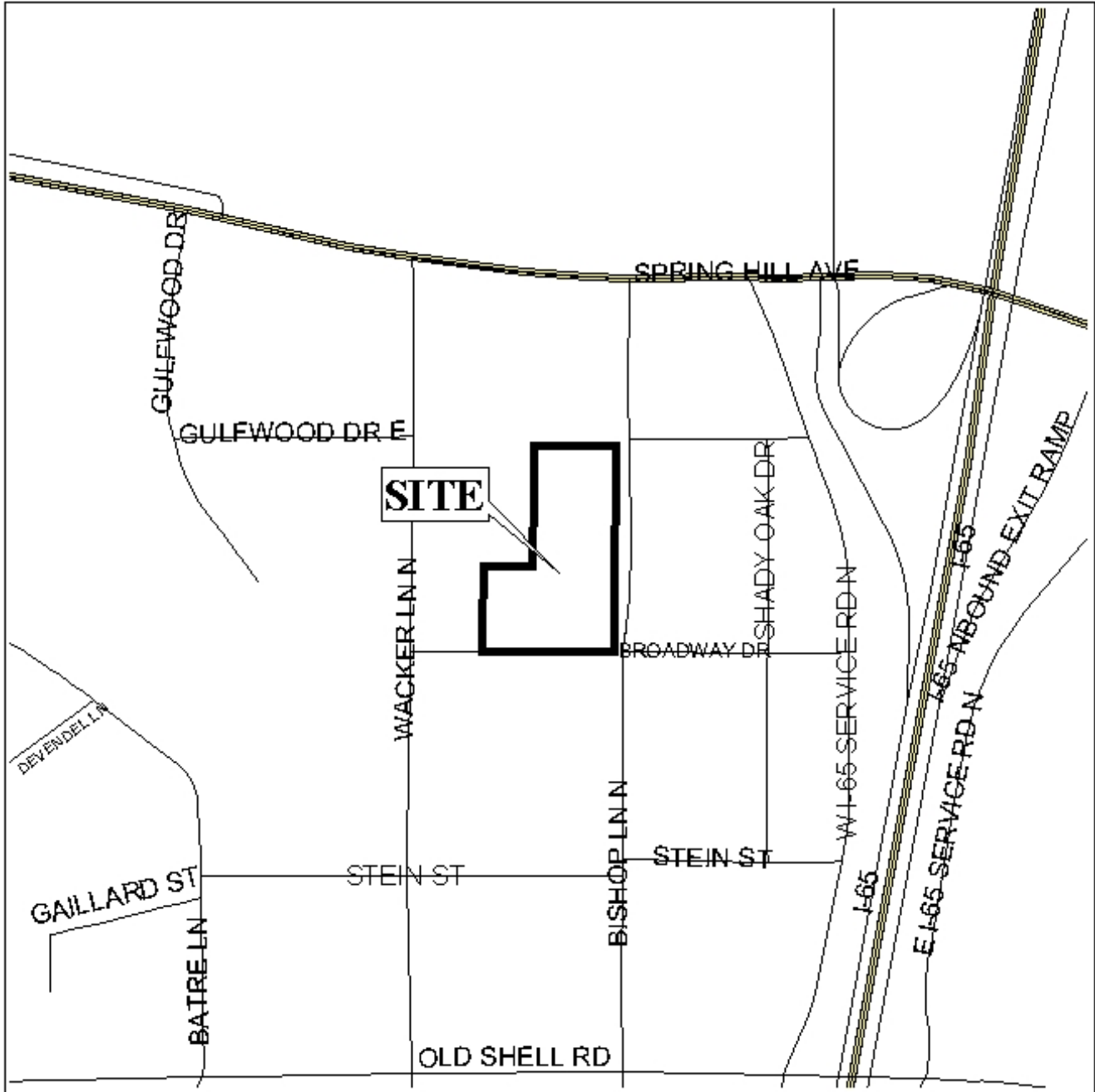
It should be noted that, with the prescribed street width of 50 feet, the existing building would be within the right-of-way; even with the substandard 35' proposed width, the building would encroach on the required building setback of 25 feet, or even on the proposed 15' setback. Thus, to accomplish the subdivision with the minimum standards of the Subdivision Regulations, the existing structure would have to be removed or relocated.

The plat also shows a substandard turnaround with a flat side abutting the adjacent property. There is no indication whether this is a temporary turnaround intended to extend into the western parcel not included in this subdivision. If continuation of the street into the adjacent parcel is not planned, the turnaround should be reconfigured as a permanent turnaround of standard dimensions.

An existing semicircular asphalt drive with two curb cuts onto Bishop Lane North is shown for proposed Lot 5 and a portion of proposed Lot 7. As a means of access management, Lots 1, 5, and 7 should be denied access to Bishop Lane North, and the removal of the asphalt drive should be required; also, Lots 1 through 3 should be denied access to Broadway Drive.

Based on the preceding, a holdover is recommended to allow the applicant to submit a full Planned Unit Development application; documentation of unusual difficulty or circumstances, or of innovative design; and a revised plat illustrating compliance with private street standards, turnaround standards, and setbacks. These documents should be submitted by June 13 for the July 7 meeting.

LOCATOR MAP

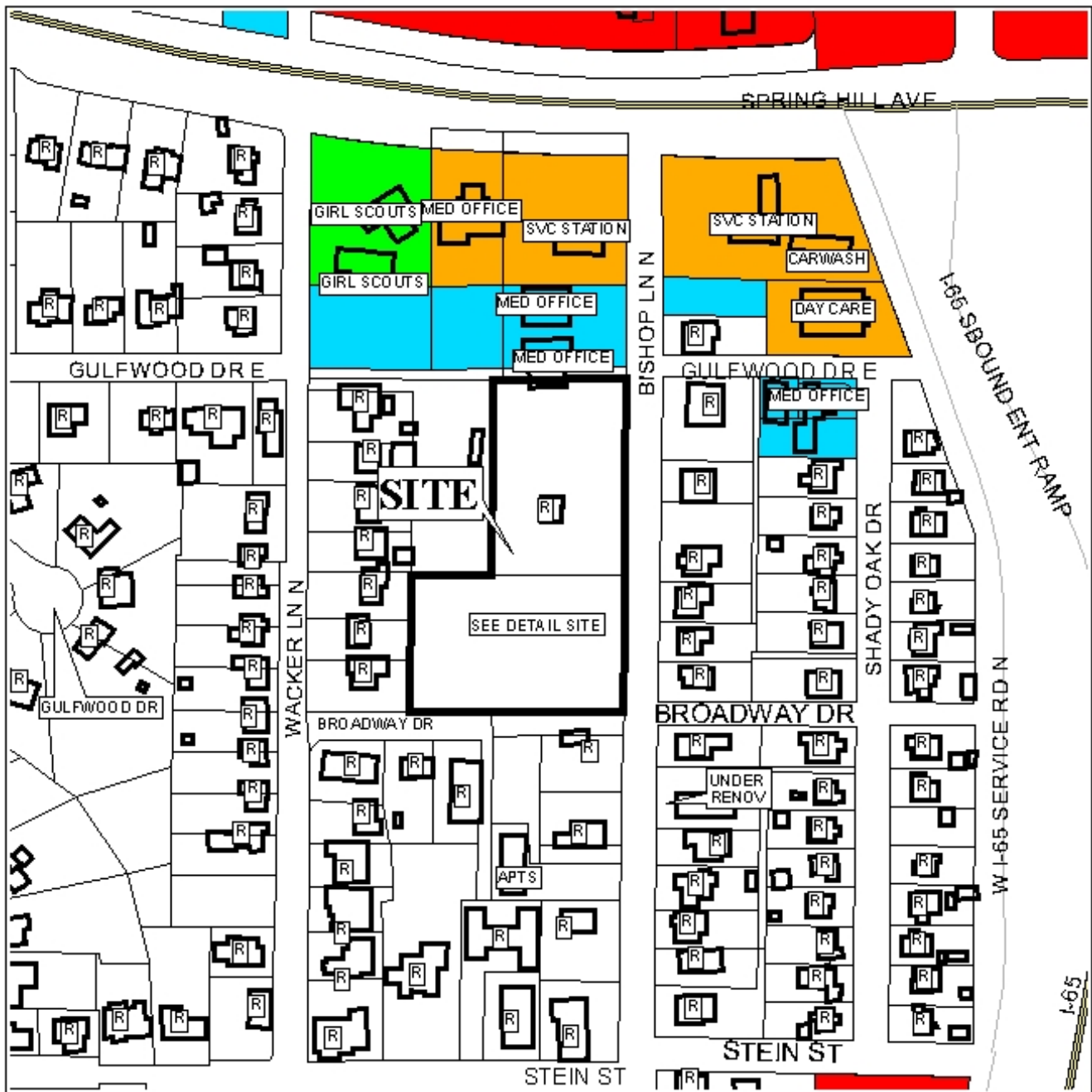


APPLICATION NUMBER 10 DATE June 2, 2005
APPLICANT Springhill Place Subdivision
REQUEST Subdivision



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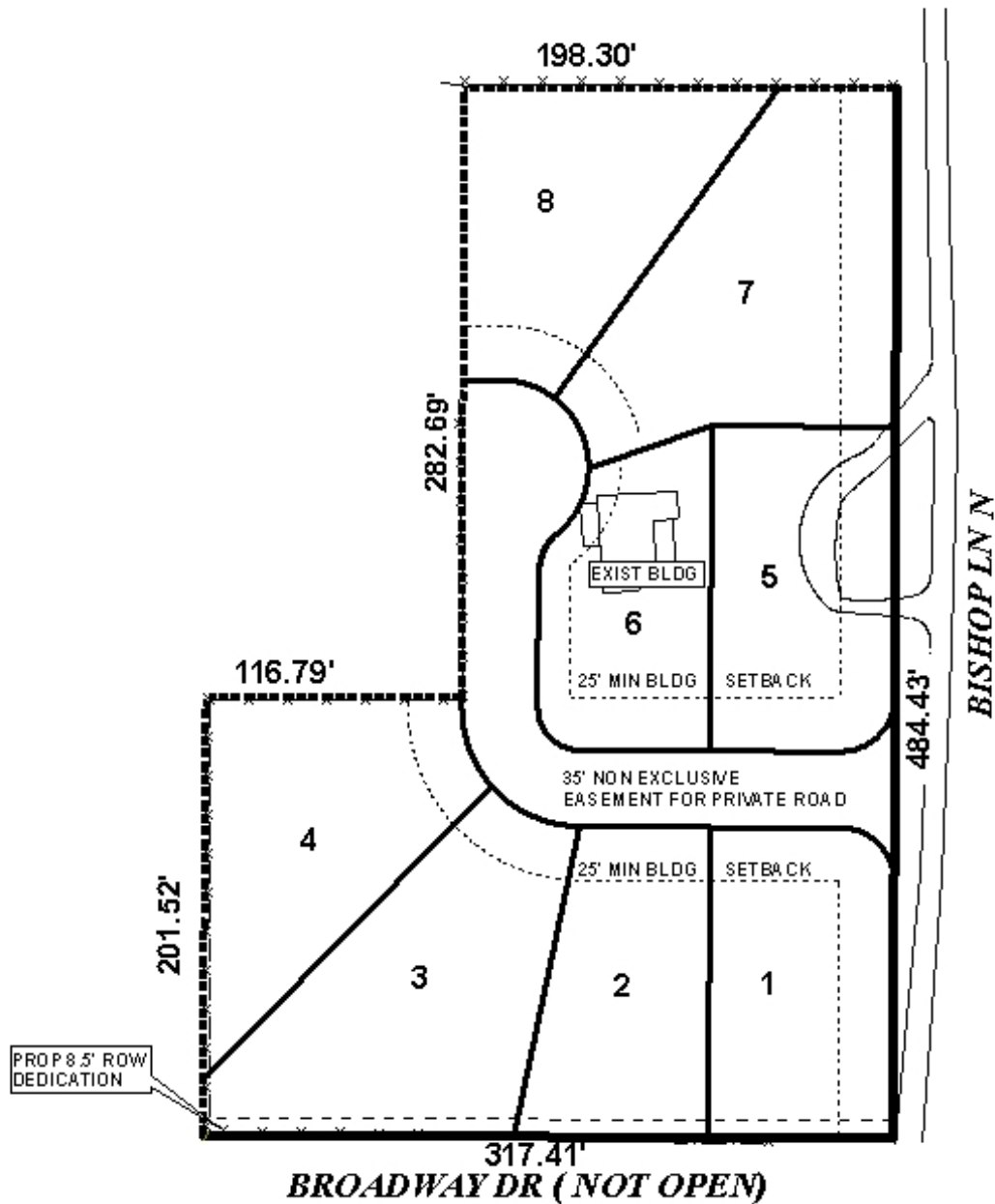
LEGEND

R-1	R-2	R-3	R-A	R-B	H-B	B-1	LB-2	B-2	B-3	B-4	B-5	I-1	I-2
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DETAIL SITE PLAN



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