

SIERRA ESTATES SUBDIVISION, PHASE I

Engineering Comments: Must comply with the Mobile County Flood Damage Prevention Ordinance. Development shall be designed to comply with the storm water detention and drainage facility requirements of the City of Mobile storm water and flood control ordinances, and requiring submission of certification from a licensed engineer certifying that the design complies with the storm water detention and drainage facility requirements of the City of Mobile storm water and flood control ordinances prior to the issuance of any permits. New public roads shall be constructed and paved to standards for County Maintenance, and accepted by Mobile County, while new private roads shall be constructed and paved to minimum County or Subdivision Regulation standards, whichever are greater.

Fire-Rescue Department Comments: Projects outside the City Limits of Mobile, yet within the Planning Commission Jurisdiction fall under the State or County Fire Code. (2012 IFC).

MAWSS Comments: MAWSS has No water or sewer services available. Both water and sewer could be made available by extending both mains approximately 3350 feet. A Capacity application would have to be certified by Volkert Engineering, Inc.

The plat illustrates the proposed 36-lot, 15.8 ± acre subdivision which is located at the Northeast corner of Airport Boulevard and Thornburg Drive (private road), in the Planning Jurisdiction. The applicant states that the subdivision is served by public water and sanitary sewer systems.

The purpose of this application is to create 36 legal lots of record from portions of two existing metes-and-bounds parcels which will include the construction of new streets. It should be noted that the site is included in the Burlington & Sierra Master Plan Subdivision and Eliza Jordan Corner Subdivision, both of which were approved by the Commission at its November 19, 2015 meeting. The area of the Burlington & Sierra Master Plan Subdivision encompasses proposed Lot 2 of Eliza Jordan Corner Subdivision. Therefore, if approved, the Final Plat for this current Subdivision cannot be signed until the Final Plat for Eliza Jordan Corner Subdivision is recorded in Probate Court; otherwise, the area of proposed Lot 1 of Eliza Jordan Corner Subdivision would become a further metes-and-bounds parcel.

The site fronts Airport Boulevard which is a component of the Major Street Plan with a planned 100' right-of-way. As the current right-of-way width along Airport Boulevard is 80', dedication sufficient to provide 50' from the centerline of Airport Boulevard would be required. The 25' minimum building setback line is not indicated along the existing frontage of Airport Boulevard or any of the proposed lots. Therefore, the plat should be revised to illustrate the 25' minimum building setback line along all street frontages and measured from any required dedication along Airport Boulevard.

Access management is a concern along Airport Boulevard; therefore, a note should be required on the Final Plat, if approved, stating that Lots 1 and 36 are denied access to Airport Boulevard. Access should be denied to Thornburg Drive (private road), Calvert Road North and the 20' easement for ingress and egress, all along the West side of the site. All lots should be limited to one curb cut each, with the size, location and design to be approved by Mobile County Engineering.

No lot areas are indicated on the preliminary plat. The plat should be revised to label each lot with its size in both square feet and acres, or a table should be furnished on the Final Plat providing the same information. The size of Lots 1 and 36 should be indicated after any required frontage dedication along Airport Boulevard.

Section V.D.3. of the Subdivision Regulations states that the depth of lots shall not be more than 3.5 times the width of the lot at the building setback line. Lots 15, 16 and 22 exceed the allowable ratio, but are the same as presented with the previously approved Master Plan. Therefore, a waiver of this section would be in order.

No easements are indicated on the plat other than the 20' easement for ingress and egress along the West border. If any other easements are proposed (such as drainage or utility easements), they should be indicated on Final Plat. A note should be required on the Final Plat stating that no structures may be constructed or placed within the ingress and egress easement or any other easements.

Development of the site will require the construction of new streets. Therefore, the Final Plat should not be signed until a County Letter of Acceptance for the constructed streets is provided.

The site has closed-end streets that exceed 600 feet. Thus, a waiver of Section V.B.6 of the Subdivision Regulations would be required. The plat indicates a compliant turnaround right-of-way diameter of 120 feet for the North-South street, but no turnaround is indicated for the street stub to the East. Therefore, the plat should be revised to provide a temporary turnaround on the street stub to the Future Development area. There is only one access point for all lots. Appendix D of the IFC, requires two points of access for single-family subdivisions with 30 or more lots. Therefore, the applicant should obtain written approval from the State Fire Marshall to allow one point of access for the Subdivision prior to signing the Final Plat.

The site has property lines at an intersection corner. Section V.D.6. of the Subdivision Regulations requires that *"property lines at street intersection corners shall be arcs having radii of at least 10 feet or shall be chords of such arcs."* The plat should be revised to label the curb radii onto Airport Boulevard meeting this requirement.

This site is located in Mobile County, thus a note should be placed on the Final Plat stating that any lots which are developed commercially and adjoin residentially developed property must provide a buffer, in compliance with Section V.A.8. of the Subdivision Regulations.

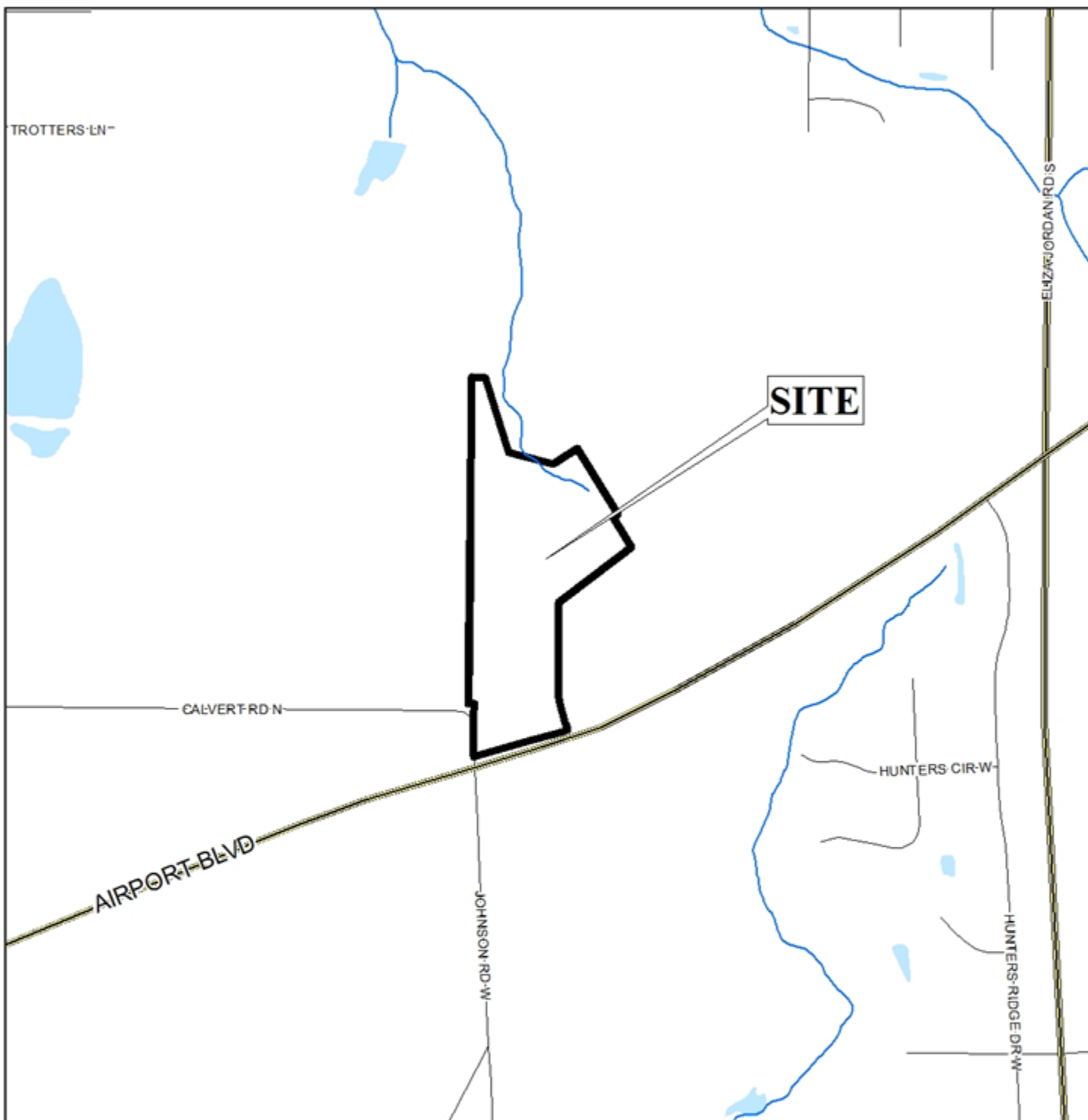
The site will have to comply with the City of Mobile storm water and flood control ordinances. A note regarding this requirement should appear on the Final Plat.

RECOMMENDATION With waivers of Sections V.B.6. and V.D.3. of the Subdivision Regulations, the plat meets the minimum requirements and is recommended for Tentative Approval, subject to the following conditions:

- 1) recording of the Eliza Jordan Corner Subdivision prior to the signing of the Final Plat;

- 2) dedication sufficient to provide 50' from the centerline of Airport Boulevard;
- 3) revision of the plat to indicate the 25' minimum building setback line along all street frontages and measured from any required dedication along Airport boulevard;
- 4) placement of a note on the Final Plat stating that Lots 1 and 36 are denied access to Airport Boulevard;
- 5) placement of a note on the Final Plat stating that Lots 1-16 are denied access to Thornburg Drive (private road), Calvert road North and the 20' easement for ingress and egress, all along the West side of the site;
- 6) placement of a note on the Final Plat stating that all lots are limited to one curb cut each, with the size, location and design to be approved by Mobile County Engineering;
- 7) revision of the plat to label each lot with its size in both square feet and acres, or the furnishing of a table on the Final Plat providing the same information;
- 8) the indication of the sizes for Lots 1 and 36 after any required frontage dedication along Airport Boulevard;
- 9) retention of the 20' ingress and egress easement along the West side and the illustration of any other easements proposed;
- 10) placement of a note on the Final Plat stating that no structures may be constructed or placed within the ingress and egress easement or any other easements;
- 11) the provision of a County Letter of Acceptance for new street construction within the Subdivision prior to signing the Final Plat;
- 12) revision of the plat to provide a temporary turnaround on the street stub to the Future Development area;
- 13) the submittal of written approval from the State Fire Marshall to allow only one point of access for the Subdivision prior to signing the Final Plat;
- 14) revision of the plat to indicate street intersection curb radii in compliance with Section V.D.6. of the Subdivision Regulations along Airport Boulevard;
- 15) placement of a note on the Final Plat stating that any lots which are developed commercially and adjoin residentially developed property must provide a buffer, in compliance with Section V.A.8. of the Subdivision Regulations;
- 16) compliance with the Engineering comments, and placement as a note on the Final Plat: *(Must comply with the Mobile County Flood Damage Prevention Ordinance. Development shall be designed to comply with the storm water detention and drainage facility requirements of the City of Mobile storm water and flood control ordinances, and requiring submission of certification from a licensed engineer certifying that the design complies with the storm water detention and drainage facility requirements of the City of Mobile storm water and flood control ordinances prior to the issuance of any permits. New public roads shall be constructed and paved to standards for County Maintenance, and accepted by Mobile County, while new private roads shall be constructed and paved to minimum County or Subdivision Regulation standards, whichever are greater); and*
- 17) compliance with the Fire-Rescue Department comments: *[Projects outside the City Limits of Mobile, yet within the Planning Commission Jurisdiction fall under the State or County Fire Code. (2012 IFC)].*

LOCATOR MAP



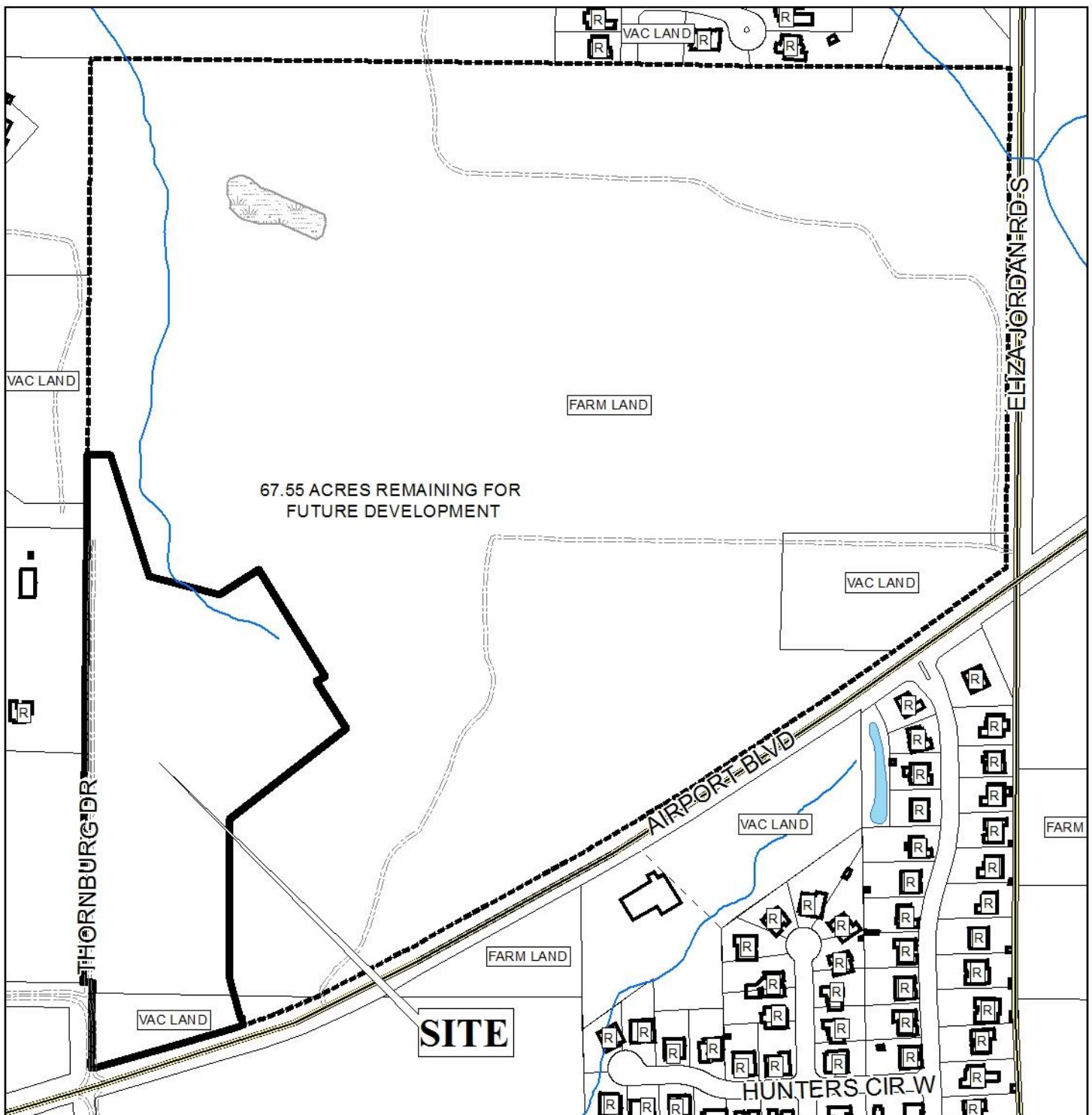
APPLICATION NUMBER 10 DATE January 7, 2016

APPLICANT Sierra Estates Subdivision, Phase I

REQUEST Subdivision



SIERRA ESTATES SUBDIVISION, PHASE I

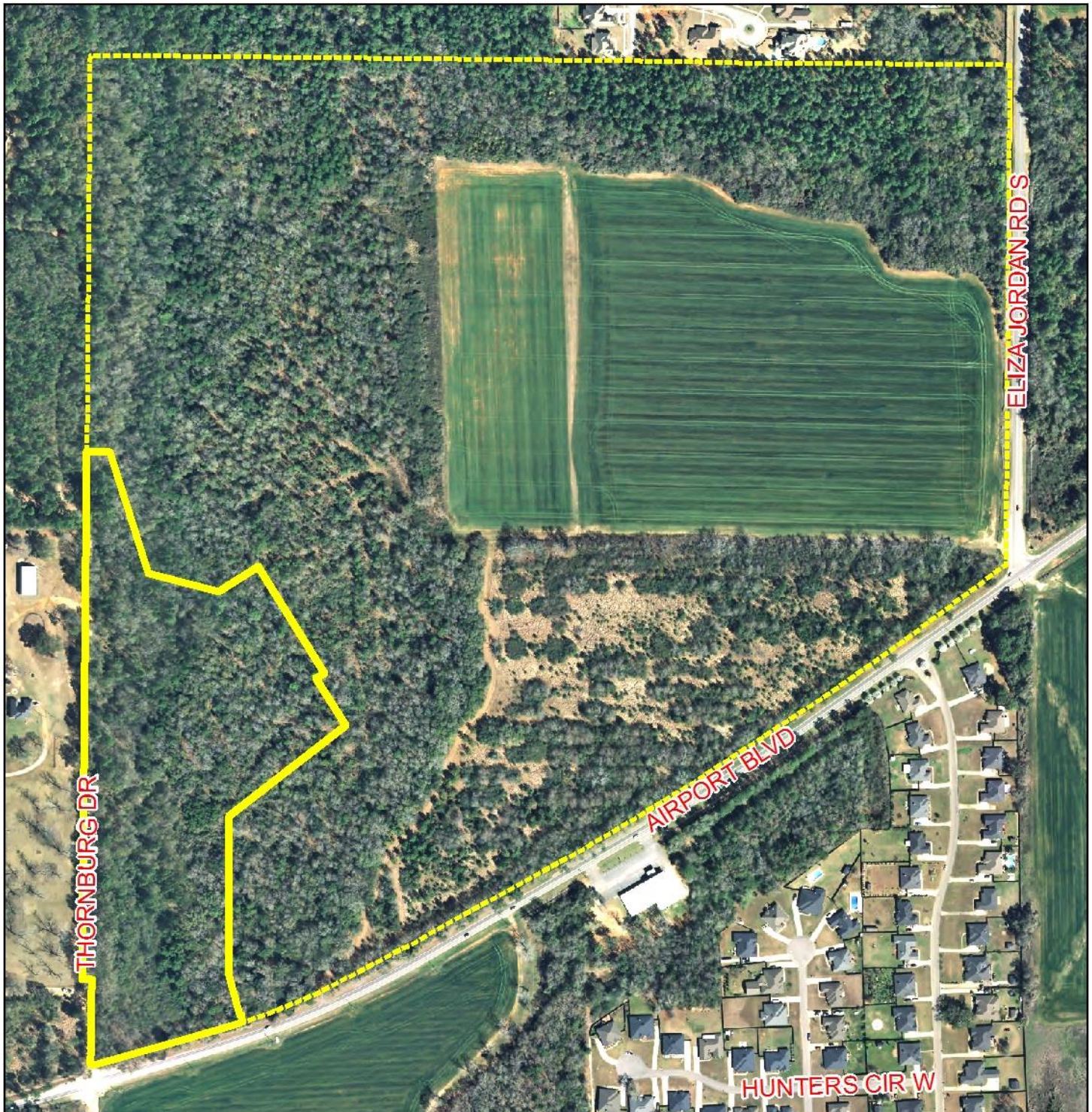


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R-A	R-3	T-B	B-2	B-5	MUN	SD-WH	T5.1
R-1	R-B	B-1	B-3	I-1	OPEN	T3	T5.2
R-2	H-B	LB-2	B-4	I-2	SD	T4	T6



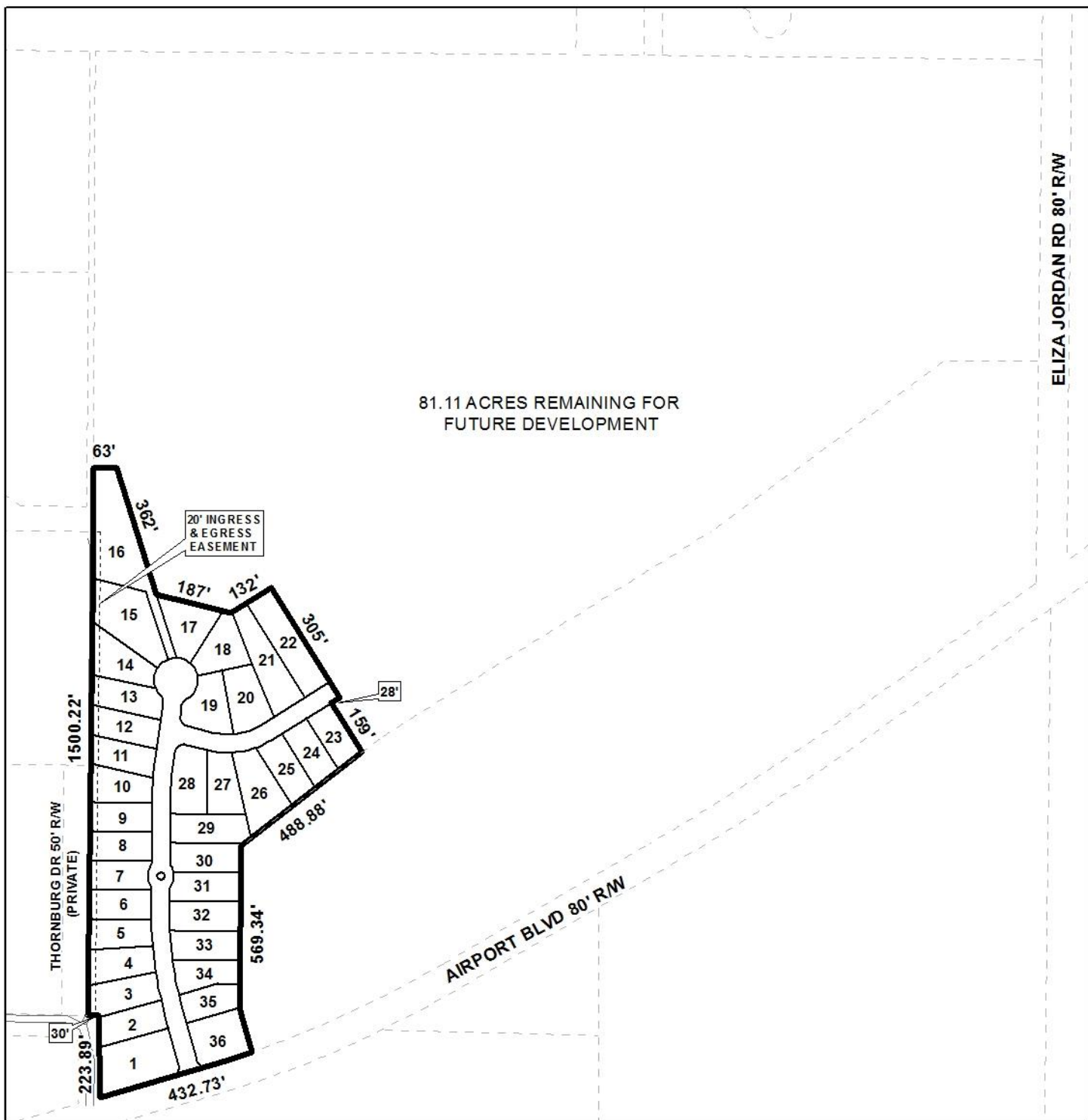
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DETAIL SITE PLAN



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REQUEST _____ Subdivision _____



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