

**PLANNED UNIT DEVELOPMENT
& SIDEWALK WAIVER STAFF REPORT****Date: July 15, 2021****DEVELOPMENT NAME**

Ray-Mont Logistics Mobile Incorporated

LOCATION56 Beauregard Street
(Northwest corner of Beauregard Street and Dekle Road).**CITY COUNCIL
DISTRICT**

District 2

PRESENT ZONING

I-2, Heavy Industry District

AREA OF PROPERTY

1 Lot / 38.89± Acres

CONTEMPLATED USE

Planned Unit Development Approval to amend a previously approved Planned Unit Development to allow multiple buildings on a single building site, and Sidewalk Waiver Approval of a request to waive construction of a sidewalk along Beauregard Street.

**TIME SCHEDULE
FOR DEVELOPMENT**

One (1) year

**ENGINEERING
COMMENTS****Planned Unit Development:** ADD THE FOLLOWING NOTES TO THE PUD SITE PLAN:

1. Any work performed in the existing ROW (right-of-way) such as driveways, sidewalks, utility connections, grading, drainage, irrigation, or landscaping will require a ROW permit from the City of Mobile Engineering Permitting Department (208-6070) and must comply with the City of Mobile Right-of-Way Construction and Administration Ordinance (Mobile City Code, Chapter 57, Article VIII).
2. A Land Disturbance Permit application shall be submitted for any proposed land disturbing activity with the property. A complete set of construction plans including, but not limited to, drainage, utilities, grading, storm water detention systems and paving will need to be included with the Land Disturbance permit. This Permit must be submitted, approved, and issued prior to beginning any of the construction work.
3. Any and all proposed land disturbing activity within the property will need to be submitted for review and be in conformance with Mobile City Code, Chapter 17, Storm Water Management and Flood Control; the City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control.

4. Any existing or proposed detention facility shall be maintained as it was constructed and approved. The Land Disturbance Permit application for any proposed construction includes a requirement of a Maintenance and Inspection Plan (signed and notarized by the Owner) for the detention facility. This Plan shall run with the land and be recorded in the County Probate Office prior to the Engineering Department issuing their approval for a Final Certificate of Occupancy.
5. The approval of all applicable federal, state, and local agencies (including all storm water runoff, wetland and floodplain requirements) will be required prior to the issuance of a Land Disturbance permit. The Owner/Developer is responsible for acquiring all of the necessary permits and approvals.
6. The proposed development must comply with all Engineering Department design requirements and Policy Letters.

Sidewalk Waiver: It appears that there is sufficient room within the ROW, or within the property, for the construction of a sidewalk that could be approved through the Land Disturbance Permit process.

TRAFFIC ENGINEERING

COMMENTS

Driveway number, size, location, and design are subject to the approved PUD for this site, shall be approved by Traffic Engineering and conform to AASHTO standards. Required on-site parking, including ADA handicap spaces, shall meet the minimum standards as defined in Section 64-6 of the City's Zoning Ordinance.

URBAN FORESTRY

COMMENTS

Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties [Act 929 of the 1961 Regular Session of the Alabama Legislature (Acts 1961, p. 1487), as amended, and City Code Chapters 57 and 65]. Private removal of trees in the right-of-way will require approval of the Mobile Tree Commission. Removal of heritage trees from undeveloped residential sites, developed residential sites in historic districts, and all commercial sites will require a tree removal permit.

FIRE DEPARTMENT

COMMENTS

All projects within the City Limits of Mobile shall comply with the requirements of the City of Mobile Fire Code Ordinance (2012 International Fire Code). Fire apparatus access is required to be within 150' of all commercial and residential buildings. A fire hydrant is required to be within 400' of non-sprinkled commercial buildings and 600' of sprinkled commercial buildings.

REMARKS

The applicant is requesting Planned Unit Development Approval to amend a previously approved Planned Unit Development to allow multiple buildings on a single buildings site, and Sidewalk Waiver Approval of a request to waive construction of a sidewalk along Beauregard Street.

Planned Unit Development (PUD) review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without

generating excess traffic along minor residential streets in residential districts outside the PUD; and, that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and, to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD.

In pursuit of these purposes, the objectives to be met by a PUD are as follows: a) to encourage innovative and diversified design in building form and site development; b) flexibility, to permit greater flexibility in the location and arrangement of buildings and uses than is generally possible under district regulations; c) to encourage the most efficient and sustainable use of land, especially tracts in the inner part of the city that remain undeveloped or that are appropriate for re-development; d) to preserve and protect as urban amenities the natural features and characteristics of land; e) to encourage the provision of common open space through efficient site design; and, f) to encourage optimum use of available public utilities, streets and community facilities.

PUD approval is site plan-specific, thus any changes to the site plan will require approval by the Planning Commission. Also, PUD approvals expire within one (1) year if no permits for the development are obtained.

The site has been given a Heavy Industry (HI) land use designation per the Future Land Use Plan and Map (FLUM), adopted on May 18, 2017 by the Planning Commission. The Future Land Use Plan and Map complements and provides additional detail to the Development Framework Maps in the Map for Mobile, adopted by the Planning Commission at its November 5, 2015 meeting.

The Heavy Industry designation applies to larger parcels primarily devoted to high-impact industrial activity which is preferably removed from residential and commercial uses. Light industry, industrial business or heavy commercial lands may separate heavy industry from other land uses.

Heavy industrial areas include collection, treatment, and manufacturing processes which use raw materials, are distinguished by the presence of noise, vibration, and/or odors, and benefit from easy access to a multimodal freight transportation network. Certain types of heavy industry are characterized by low building coverage and activities that rely on large areas of outdoor storage of raw material stockpiles and/or waste-product disposal areas, storage tanks, pipelines, and transportation yards to handle the transfer of heavy materials. The outdoor storage areas should be screened as much as possible by the nature of the stored materials.

Land designated as HI may be underdeveloped due to the presence of wetlands on portions of the parcel. In these cases, the wetlands may serve to buffer surrounding uses from the potential impacts of the heavy industrial use. Undeveloped areas of HI parcels that have tree cover may be used as buffering between the heavy industrial use and other uses. Where buffers do not exist naturally, they should be provided as spelled out in the zoning and subdivision regulations. Open areas reserved for dredge disposal are also designated as HI and may contain wetlands.

In Mobile, port terminal facilities, docks, shipyards, drydocks, etc. are mostly owned by the State of Alabama. Although not subject to local zoning, these facilities are shown as heavy industrial uses in the FLUM.

It should be noted that the Future Land Use Plan and Map components of the Map for Mobile Plan are meant to serve as a general guide, not a detailed lot and district plan. In many cases the designation on the new Future Land Use Map may match the existing use of land, but in others the designated land use may differ from what is on the ground today. As such, the Future Land Use Plan and Map allows the Planning Commission and City Council to consider individual cases based on additional information such as the classification request, the surrounding development, the timing of the request, and the appropriateness and compatibility of the proposed use and, where applicable, the zoning classification.

At its October 4, 2012 meeting the Planning Commission approved Subdivision, Planning Approval, Planned Unit Development (PUD), and Sidewalk Waiver requests to, respectively, create one (1) legal lot of record; allow the expansion of an existing rail yard; allow multiple buildings on a single building site; and, to waive construction of a sidewalk along Dekle Road. The Subdivision was finalized, the plat for which is recorded in Mobile County Probate Court; however, the expansion of the rail yard and the development of additional buildings as approved via the PUD never occurred and, as such, the approval has expired. With respect to the Planning Approval, use of the property as a rail yard is allowed by right in an I-2 zoning district, thus the Planning Approval application and the approval of any amendments thereto are unnecessary.

The purpose of this request is to allow construction of a transloading facility and office building on a site already developed with multiple buildings and a rail yard, and to waive construction of a sidewalk along Beauregard Street. The proposed use of the property for the heavy distribution of plastics materials (plastic resin pellets) is allowed by right in an I-2 zoning district.

The applicant provided the following narrative to describe the proposed scope of operations:

“Ray-Mont Logistics facilitates international trade by enhancing the container supply chain in transloading bulk product into container and delivering them just in time to its customers.

Ray-Mont Logistics is set to build a transloading facility in Mobile, Alabama to transload and package plastic resin pellets from railcars into bags, then loaded on pallets up to shipping containers. The pallets will be brought in the outside yard to be placed on trucks to be delivered to customers.

The proposed development is located in an industrial development within I-2 zoning and will consist of the following:

- *Transloading facility;*
- *Plastic resin packaging line system;*
- *Railcar unloading station;*
- *Container maneuvering area;*

- *Small office building.*

The new terminal will be able to process and transload a minimum of 10 railcars per day into 35, 40-ft containers. The new facility will use the existing two tracks and a discharge bin capable to discharge railcars from each track. The new terminal will also have the ability to process the bulk material from rail cars into bags and loaded into shipping containers.”

The site plan illustrates, as mentioned, a transloading facility and office building, which are proposed to be developed south of the existing rail yard in proximity to where its expansion was previously proposed, along Beauregard Street. The site plan does not depict existing improvements, but no changes to the development are anticipated by this request. Nevertheless, revision of the site plan to show the entire PUD site and depict all existing and proposed site improvements may be an appropriate request which, if approved, should be provided to Planning staff prior to the approval of any permits.

Each of the proposed buildings are significantly distant from one another, with the size of the proposed transloading facility depicted as 15,000 square feet. The size of the proposed office building is not provided and should be depicted on the revised site plan, if approved.

Off-street parking is illustrated along Beauregard Street, the calculations for which are not provided. Without knowing the size of the office building or the number of employees anticipated working within the transloading facility, staff cannot determine if the amount of parking is adequate. Furthermore, the dimensions of the parking stalls are not provided, and no van accessible space(s) is/are designated. As such, this information should be provided on the revised site plan, if approved, in compliance with Sections 64-6.A.2. and 64-6.A.6. of the Zoning Ordinance, and in compliance with International Building Code standards.

Parking lot lighting is required for parking lots with 25 or more parking spaces. If applicable, a photometric plan shall be required at the time of permitting, and shall comply with the requirements of Section 64-6.A.8. of the Zoning Ordinance.

The parking lot is proposed to be paved, along with a large portion of the interior of the lot, presumably for maneuvering of cargo and other transloading activities. Aggregate surfacing is proposed around most of the transloading facility and is acceptable in an I-2 zoning district.

Access to the site is along Beauregard Street, through the proposed parking lot, and the driveway meets the width requirement of Section 64-6.A.2. for two-way traffic; however, it is unclear if the parking lot will accommodate two-way traffic circulation. As such, the width of the access aisles within the proposed parking lot should be depicted on the revised site plan, if approved, in compliance with Section 64-6.A.2. of the Zoning Ordinance.

Tree plantings are illustrated on the site plan along Beauregard Street, but only to the limits of the proposed development. Typically, tree plantings are required along all street frontages in an I-2 zoning district, but the previous PUD approval did not address this requirement, with the previous site plan also illustrating tree plantings along Beauregard Street and only to the limit of

the development. As such, it may be appropriate to allow the reduced amount of tree plantings at this time. However, it should be noted that the dimension of the frontage along which the trees are proposed to be planted is not provided, and no table depicting the required number of trees is provided. As such, staff cannot verify if the amount of proposed tree plantings is accurate. If approved, this information should be illustrated on a revised site plan and provided to staff at the time of permitting; however, the applicant should be advised that tree plantings may be required along all frontages and illustrated on a revised site plan, in compliance with Section 64-4.E.3.a.(2). of the Zoning Ordinance.

Landscape area is illustrated on the site plan, but no calculations are provided for staff to verify it will meet the landscaping percentage requirements of Section 64-4.E.3.a.(1). of the Zoning Ordinance. If approved, this information should be depicted on a revised site plan and provided to staff at the time of permitting.

Without any calculations for tree plantings or landscape area for staff to verify compliance, the PUD site plan should at least be revised with a note stating that the site shall comply with the tree planting and landscape area requirements of Section 64-4.E.3. of the Zoning Ordinance.

A dumpster pad is illustrated next to the proposed transloading facility, and a note on the site plan states it is to be enclosed and connected to the sanitary sewer in compliance with Section 64-4.D.9. of the Zoning Ordinance. This information should be retained on the revised site plan, if approved. The applicant should be advised that a separate permit is required for the construction of the dumpster pad and its enclosure.

The site plan delineates the limits of the proposed development without including, as mentioned, the existing development. It should be noted that, if the area is leased separately from the remainder of the property as a separate parcel, then approval of a subdivision shall be required per Section 11-52-1 of the Code of Alabama 1975, as amended. Revision of the PUD to reflect the subdivision would also be required at that time.

Regarding the Sidewalk Waiver request, the applicant provided the following narrative to justify the request:

“Request of approval to waive construction of sidewalks along Beauregard Street for the address located at 56 Beauregard Street. The propose development is an industrial development within I-2 zoning. Sidewalk along the northern Beauregard frontage is presently a dead end at 76 Beauregard St. with no pedestrian connections at Dekle, which was previously exempted from sidewalk requirements in 2012.”

As mentioned, a waiver was granted to exempt the construction of a sidewalk along Dekle Road in 2012. The site plan for the proposed PUD at that time illustrated a sidewalk along Beauregard Street.

The Mobile City Council adopted a “complete streets” policy on May 31, 2011. The purpose of the complete streets policy is to support the design and construction of streets to enable safe access to all users, including pedestrians, bicyclists, transit riders, motorists, commercial and

emergency vehicles, and for people of all ages and abilities. The fact that few or no sidewalks exist within the vicinity of the subject site does not, by virtue of their absence, justify exemption from the complete streets policy considering its intent is to encourage diverse mobility throughout the city. A waiver of the sidewalk, as requested, will limit accessibility for pedestrians.

While the subject site may be within a predominantly industrially developed area, which may suggest that pedestrian traffic in the vicinity is minimal and would not likely increase due to the inherent conditions of such development, there are nevertheless no observable conditions, topographical or otherwise, that justify exemption of a sidewalk at this location. This is perhaps substantiated by Engineering comments, which confirm that a sidewalk can be constructed along Beauregard Street.

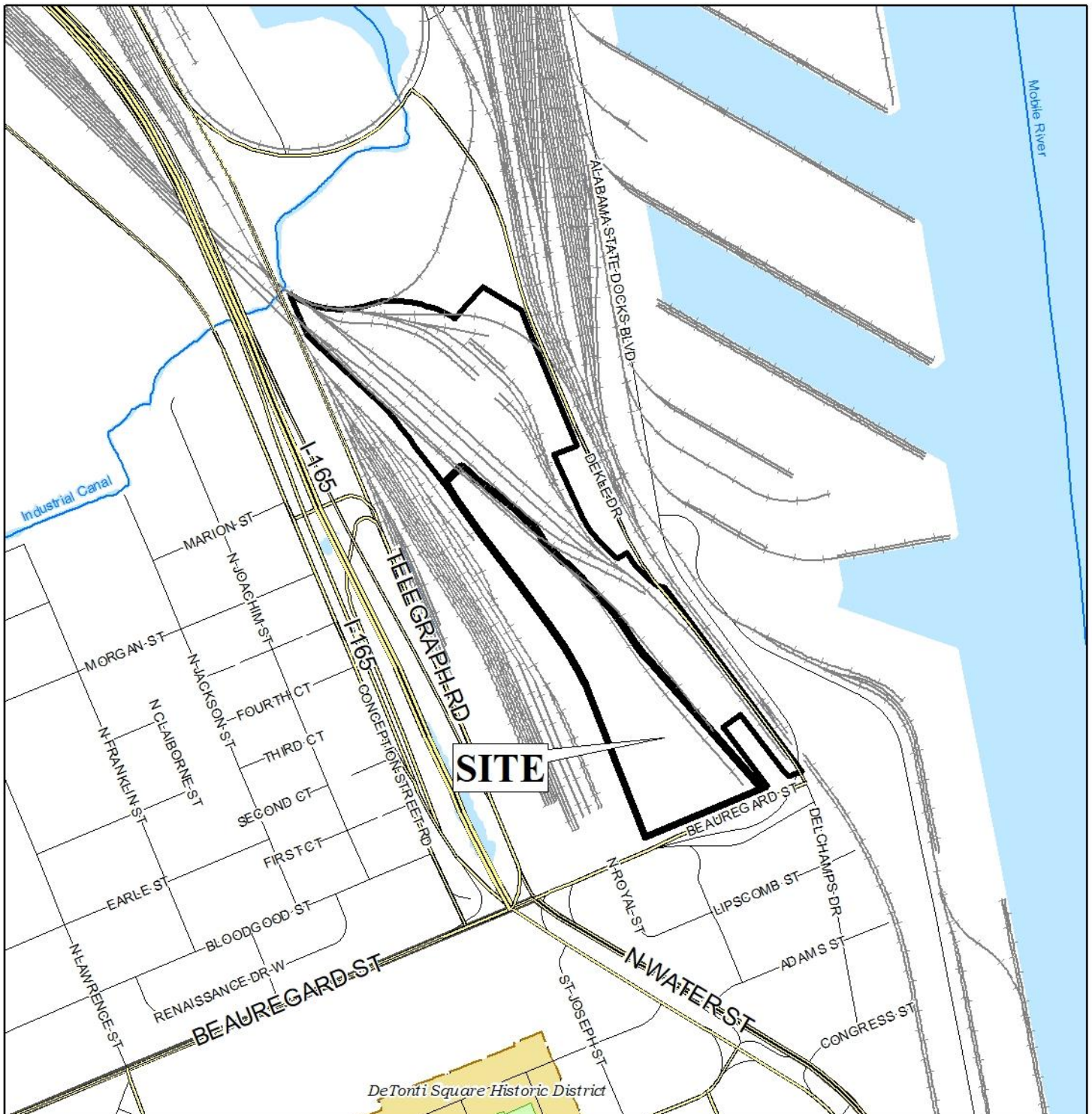
RECOMMENDATION

Planned Unit Development: Staff recommends this application be heldover to the August 5th meeting, with revisions and any additional information submitted by July 23rd, to allow the applicant time to address the following:

- 1) Submission of a complete Subdivision application with all fees to reflect the proposed subdivision of the lease parcel, to be reviewed concurrently with the PUD request, or submission of nonconforming documentation to establish the site as a legal lot of record prior to 1952;
- 2) Revision of the site plan to show the entire PUD site and depict all existing and proposed site improvements, including the size of the proposed office building;
- 3) Provision of a table on the revised site plan depicting off-street parking calculations, in compliance with Section 64-6.A.6. of the Zoning Ordinance;
- 4) Revision of the site plan to designate van-accessible/handicap parking spaces in compliance with International Building Code standards;
- 5) Revision of the site plan to depict compliance with the minimum parking stall and access aisle dimensions of Section 64-6.A.2. of the Zoning Ordinance;
- 6) Placement of a note on the revised site plan stating the parking lot lighting shall comply with Section 64-6.A.8. of the Zoning Ordinance, and a photometric plan shall be submitted at the time of permitting;
- 7) Revision of the site plan to illustrate compliance with the tree planting and landscape area requirements of Section 64-4.E.3. of the Zoning Ordinance.

Sidewalk Waiver: Staff recommends this application be heldover until the August 19th meeting to allow the applicant time to provide the additional information requested by staff for the PUD request.

LOCATOR MAP



APPLICATION NUMBER 10 DATE July 15, 2021

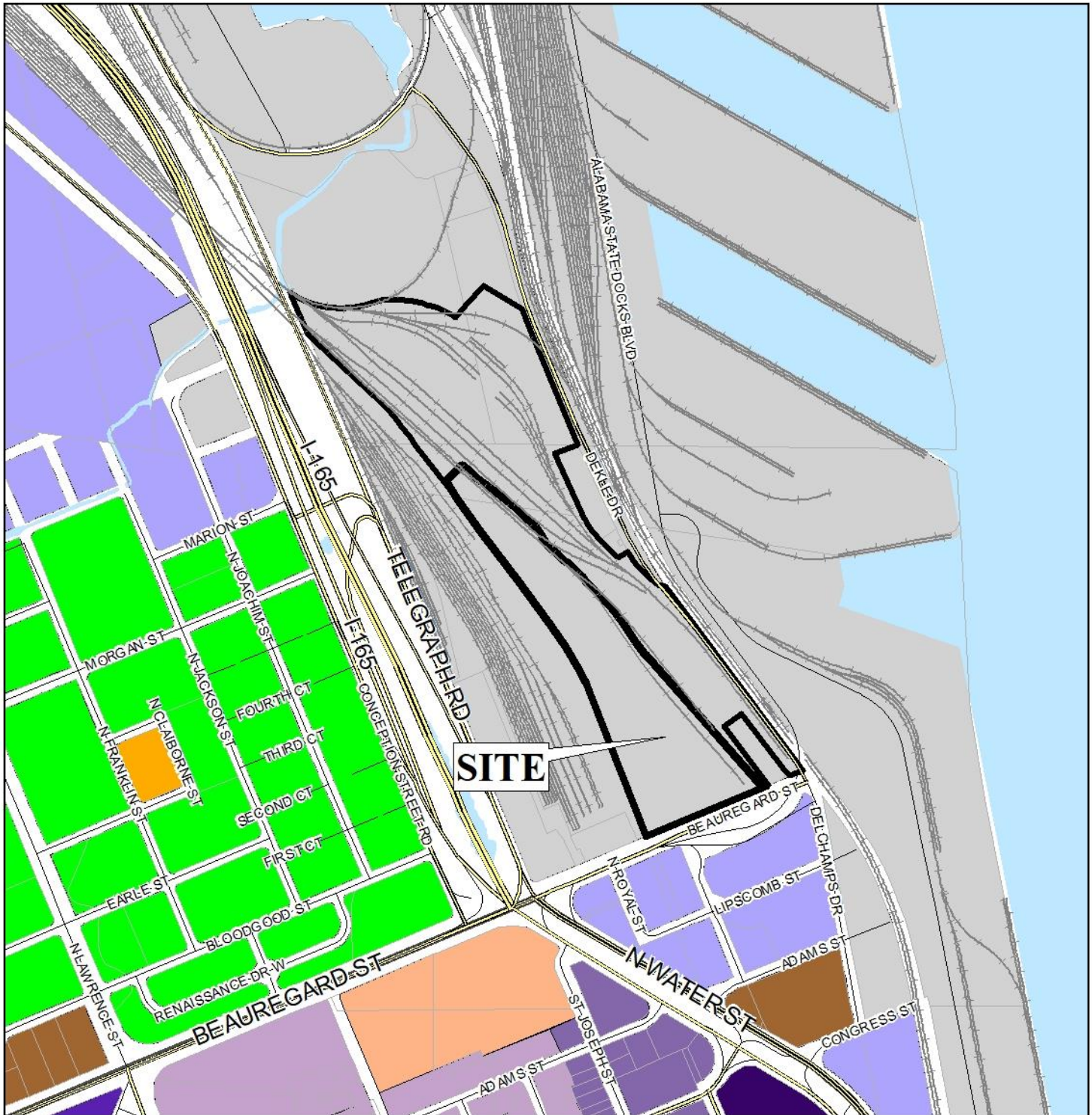
APPLICANT Ray-Mont Logistics Mobile Incorporated

REQUEST Planned Unit Development, Sidewalk Waiver



NTS

LOCATOR ZONING MAP



APPLICATION NUMBER 10 DATE July 15, 2021

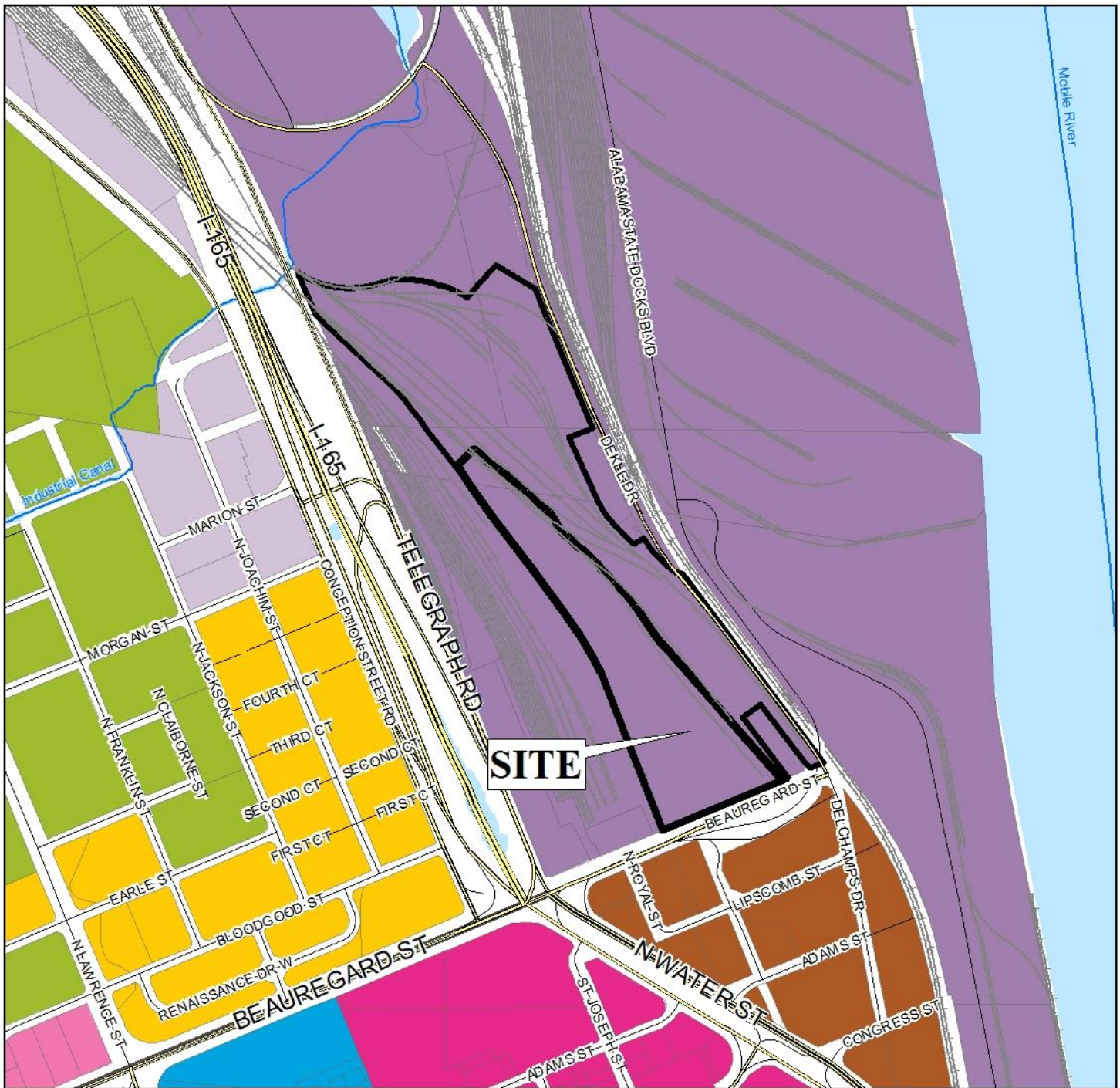
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NTS

FLUM LOCATOR MAP



APPLICATION NUMBER 10 DATE July 15, 2021

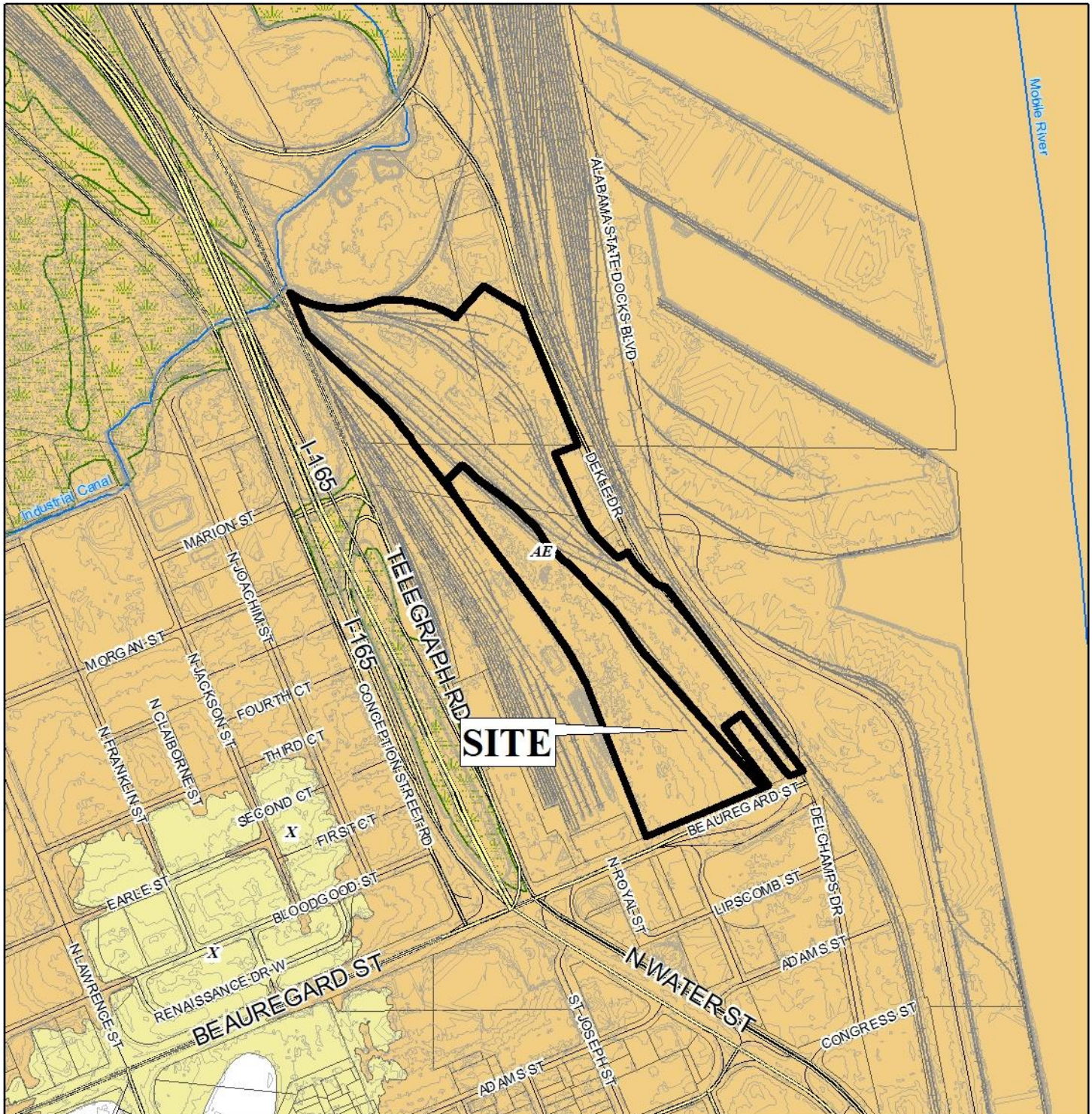
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REQUEST Planned Unit Development, Sidewalk Waiver

Low Density Residential	Neighborhood Center - Traditional	Downtown Waterfront	Parks & Open Space
Mixed Density Residential	Neighborhood Center - Suburban	Light Industry	Water Dependent
Downtown	Traditional Corridor	Heavy Industry	
District Center	Mixed Commercial Corridor	Institutional	



ENVIRONMENTAL LOCATOR MAP



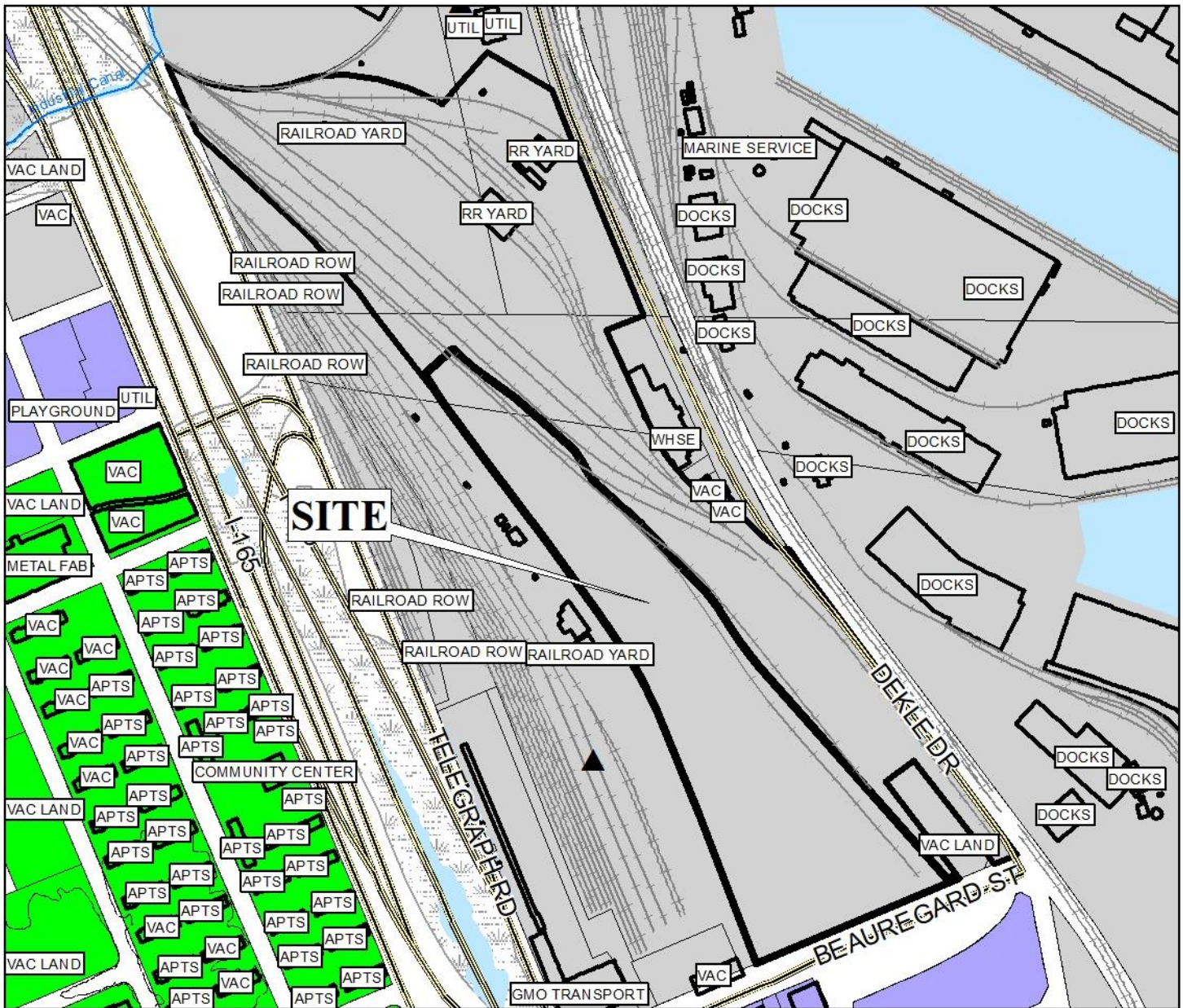
APPLICATION NUMBER 10 DATE July 15, 2021

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REQUEST Planned Unit Development, Sidewalk Waiver



PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The site is surrounded by industrial units. Residential units lie west of the site.

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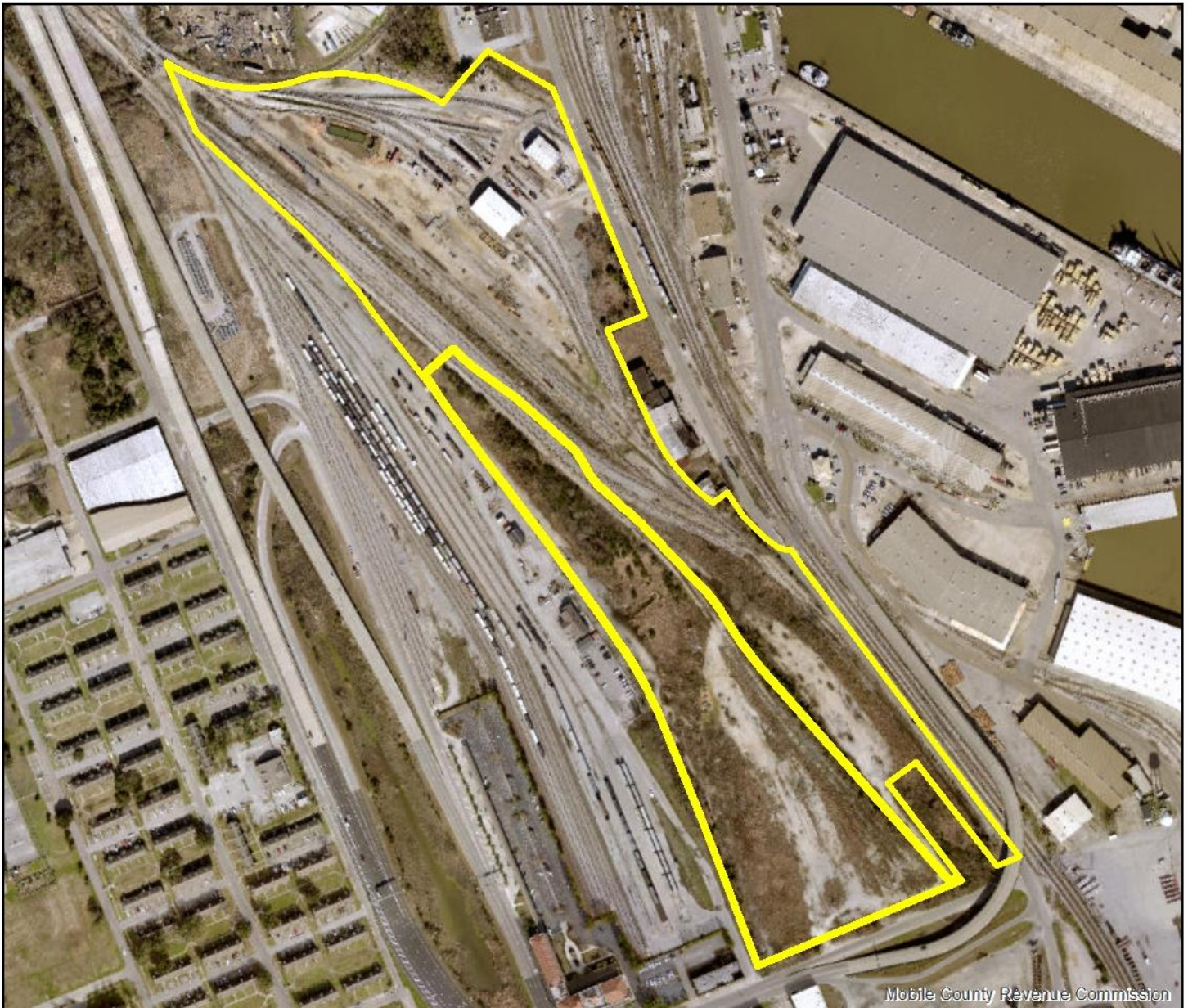
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REQUEST Planned Unit Development, Sidewalk Waiver

R-A	R-3	T-B	B-2	B-5	MUN	SD-WH	T5.1
R-1	R-B	B-1	B-3	I-1	OPEN	T3	T5.2
R-2	H-B	LB-2	B-4	I-2	SD	T4	T6



PLANNING COMMISSION VICINITY MAP - EXISTING AERIAL



The site is surrounded by industrial units. Residential units lie west of the site.

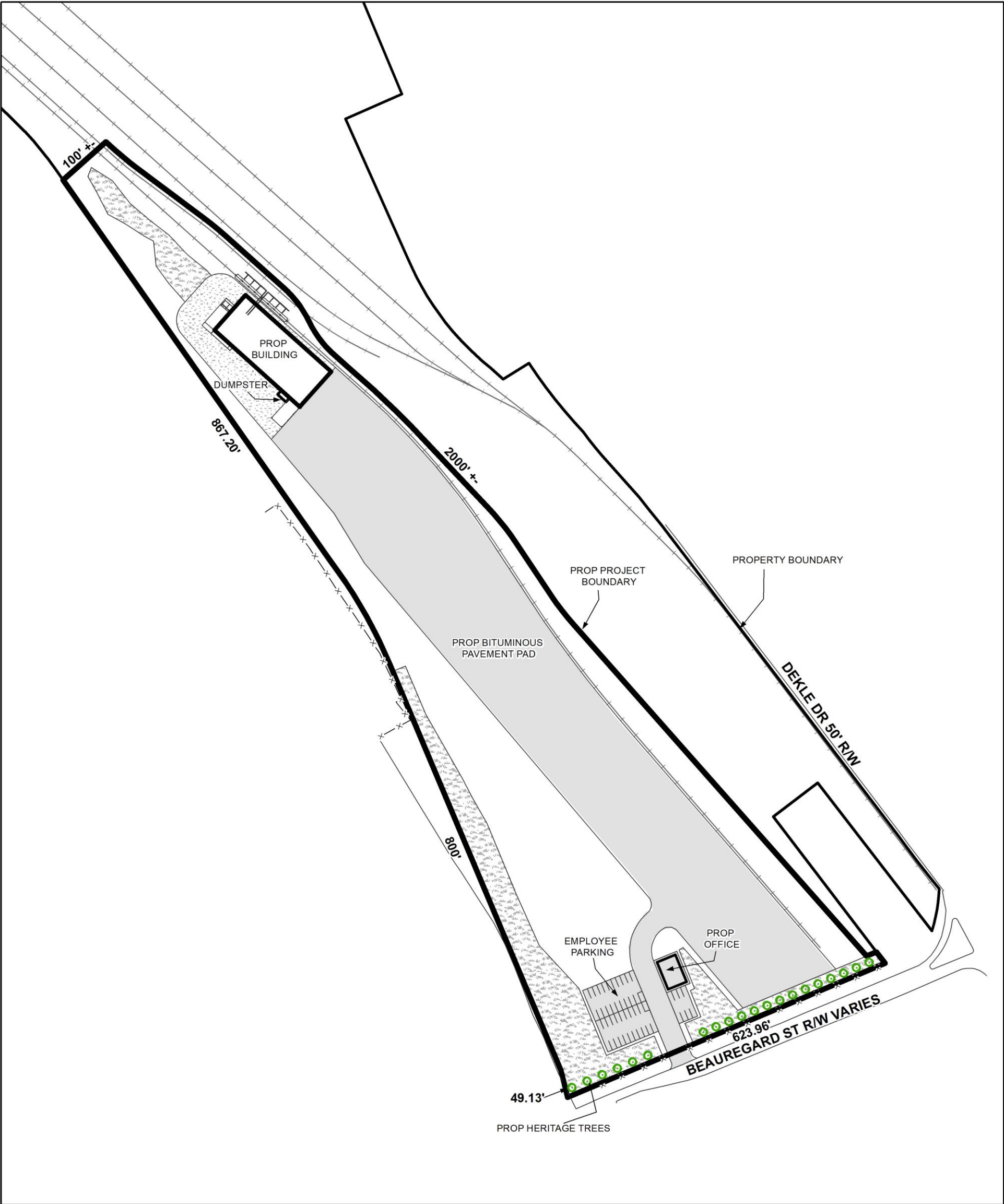
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SITE PLAN

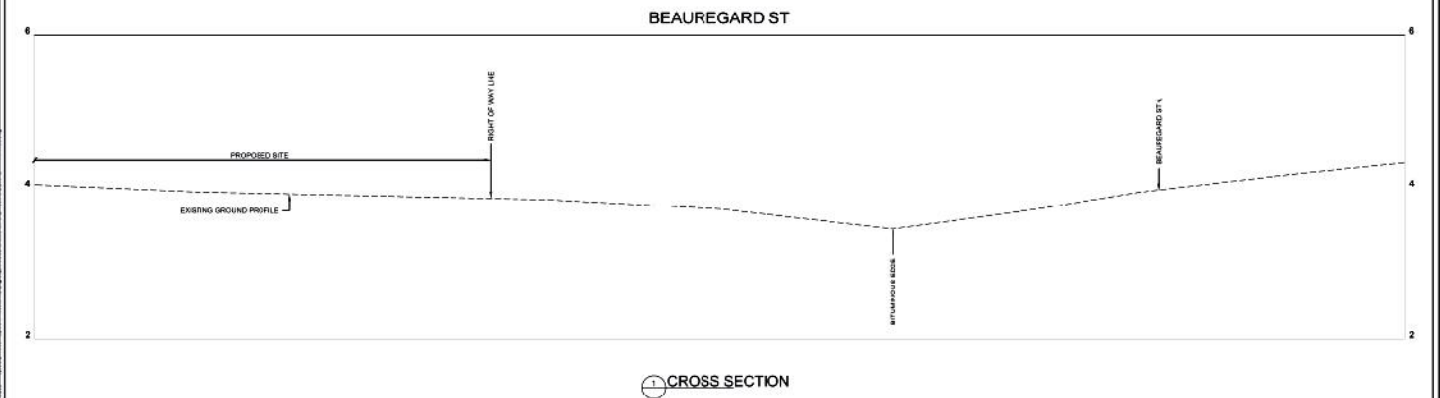
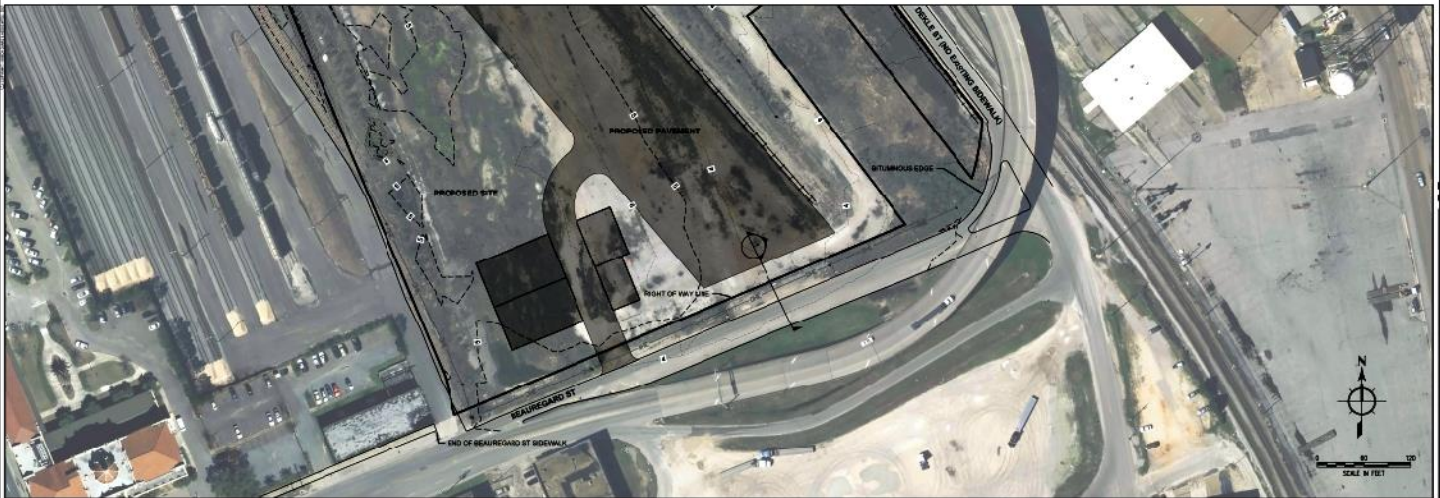


The site plan illustrates the proposed building, pavement, parking, and landscaping.

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APPLICANT Ray-Mont Logistics Mobile Incorporated
REQUEST Planned Unit Development, Sidewalk Waiver



DETAIL SITE PLAN



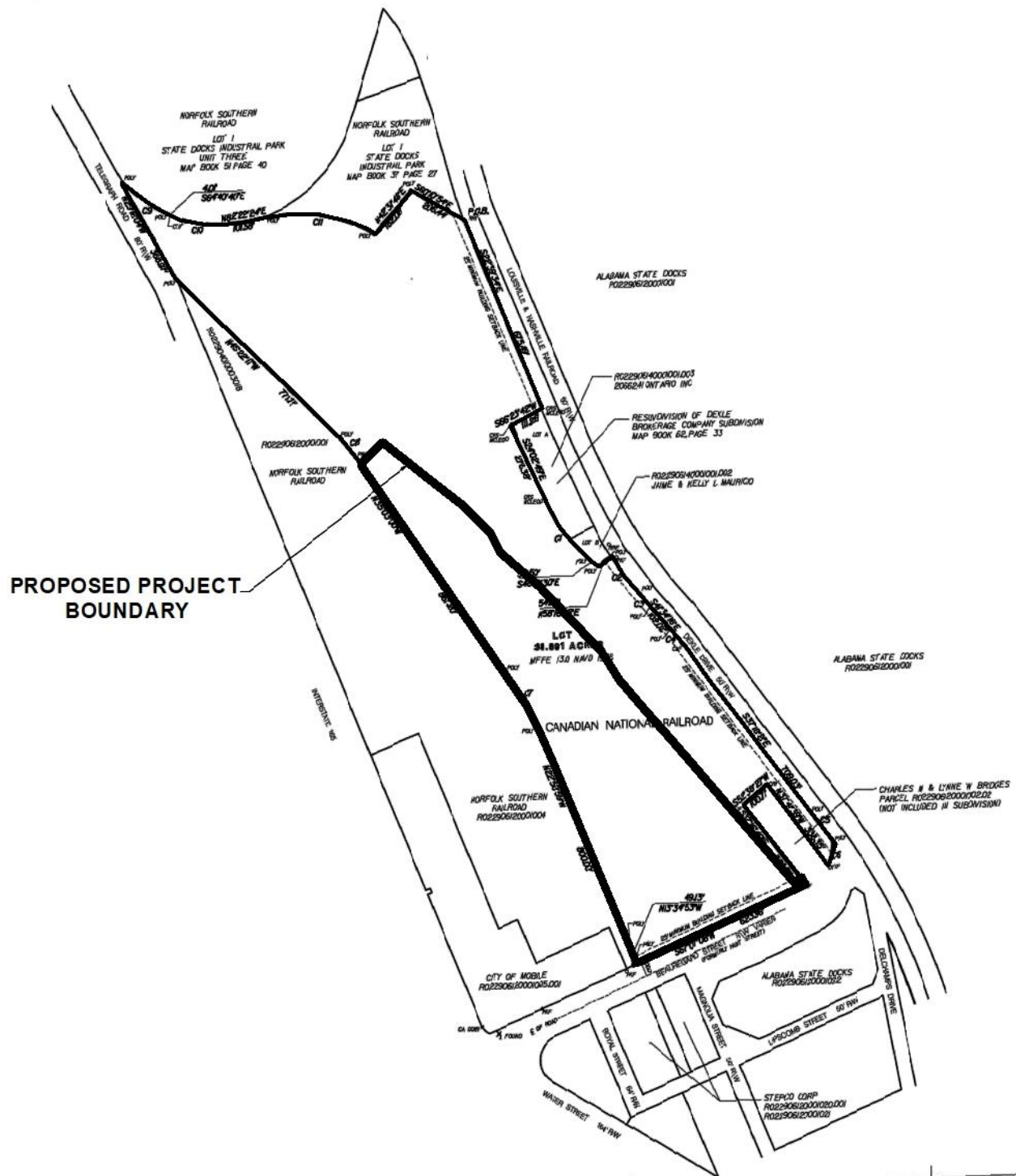
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DETAIL SITE PLAN



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