

## **WESLEY STATION SUBDIVISION**

Engineering Comments: Realignment of drainage system subject to review of City Engineer. The final approved location of the drainage easement, if approved to be altered from existing, may differ from that shown on the preliminary plat submitted for Planning Commission. Must comply with all storm water and flood control ordinances. Any increase in impervious area in excess of 4,000 square feet will require detention. Any work performed in the right of way will require a right of way permit, in addition to any required land disturbance permits. Drainage from any new dumpster pads cannot discharge to storm sewer; must have connection to sanitary sewer.

Traffic Engineering Comments: Driveway number, size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards.

Urban Forestry Comments: Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

Fire Department Comments: All projects must comply with the requirements of the 2003 International Fire Code, including Appendices B through D, as adopted by the City of Mobile, and the 2003 International Existing Building Code, as appropriate.

The plat illustrates the proposed 4.3 acre  $\pm$ , 3 lot subdivision which is located at the Southeast corner of Airport Boulevard and Wesley Avenue, and is in Council District 6. The applicant states that the subdivision is served by both public water and sanitary sewer.

The purpose of this application is to resubdivide two existing lots into three lots, and to relocate a drainage and utility easement to a proposed lot line. The two lot configuration was approved by the Planning Commission at its December 7, 2006 meeting, and the approval included curb-cut limitations and specific buffering requirements as follows:

- 1) the provision of an eight-foot wooden privacy fence and vegetative buffer where the site adjoins residential zoning;
- 2) placement of a note on the final plat stating that no structures, parking or driveway(s) may be constructed within the southern 50' (between Wesley Avenue and Henckley Avenue [to be vacated]);
- 3) compliance with Engineering comments (*Land disturbance application required for required relocation of drainage structures, in approved applicable easements. After relocation of drainage structures is complete, the vacation of rights-of-way will be applied for. Upon vacation of the right-of way, another land disturbance application will be made for the construction of a City Engineering approved cul-de-sac. In future development, detention will be required on-site. Must comply with all storm water and flood control ordinances. Any work performed in the right of way will require a right of way permit.*);
- 4) completion of the vacation process prior to the recording of the final plat;
- 5) placement of a note on the final plat stating that lot 1 is limited to one curb-cut onto Airport Boulevard and one curb-cut onto Wesley Avenue, that lot 2 is limited to one

- curb-cut onto Airport Boulevard and denied access to Henckley Avenue, and that the size, design and location of all curb-cuts are to be approved by the Planning Section of Urban Development and Traffic Engineering, and comply with AASHTO standards;
- 6) the provision of a modified turnaround at the northern terminus of Henckley Avenue;
  - 7) placement of a note on the final plat stating that access to Henckley Avenue is denied; and
  - 8) full compliance with all other municipal codes and ordinances.

It should also be pointed out that the adopted zoning amendment for the site from 2004 includes additional conditions above and beyond those required by the subdivision approval.

The site fronts Airport Boulevard, Wesley Avenue, Henckley Avenue, and an unopened right-of-way for Government Street. The rights-of-way for Airport Boulevard and Wesley Avenue meet minimum requirements. The right-of-way width for the Henckley Avenue street stub is only 40-feet, however, direct access to Henckley is not allowed via the zoning amendment restrictions or the previously approved subdivision conditions. Airport Boulevard is a proposed major street.

As Airport Boulevard is a major street, Lot 1 should be limited to one curb-cut onto Airport Boulevard (*as previously allowed*), Lot 2 should be limited to one curb-cut onto Airport Boulevard (*as previously allowed*), and Lots 1 and 3 should be limited to one shared curb-cut onto Wesley Avenue (*previously the entire frontage along Wesley Avenue was part of Lot 1, and was allowed one curb-cut*). The size, design and location the curb-cuts are to be approved by Traffic Engineering, and are to be designed to conform with AASHTO standards. Access to Henckley Avenue and the unopened right-of-way for Government Street should be denied.

As previously mentioned, the resubdivision will also allow for the relocation of a previously recorded drainage and utility easement. The applicant has initiated the vacation process for the existing easement, and this vacation process should be completed prior to the signing of the final plat, if approved.

The previously required 50-foot no-build buffer, the 35-foot vegetative buffer, the 10-foot vegetative buffer, and the 25-foot building setback line along public streets are shown on the preliminary plat, and should also be shown on the final plat, if approved. A note should also be placed on the final plat stating that no structures, parking or driveway(s) may be constructed within the southern 50 feet of Lots 2 and 3 between Wesley Avenue and Henckley Avenue.

The previously required 8-foot high wood privacy fence is not depicted or included as a note on the plat, thus the plat should be revised to also depict and note that an eight-foot high wooden privacy fence and vegetative buffer should be provided where the site adjoins residential zoning.

Finally, the geographic area defined by the city of Mobile and its planning jurisdiction, including this site, may contain Federally-listed threatened or endangered species as well as protected non-game species. Development of the site must be undertaken in compliance with all local, state and Federal regulations regarding endangered, threatened or otherwise protected species.

Based upon the preceding, the application is recommended for Tentative Approval, subject to the following conditions:

- 1) the provision of an eight-foot wooden privacy fence and vegetative buffer where the site adjoins residential zoning;
- 2) placement of a note on the final plat stating that no structures, parking or driveway(s) may be constructed within the southern 50 feet (between Wesley Avenue and the West right-of-way line of Henckley Avenue);
- 3) depiction and labeling of the 35-foot vegetative buffer along the southern boundary of the site, from the West right-of-way line of Henckley Avenue, as shown on the preliminary plat;
- 4) depiction and labeling of the 10-foot vegetative buffer along the eastern boundary of the site, as shown on the preliminary plat;
- 5) compliance with Engineering comments (*Realignment of drainage system subject to review of City Engineer. The final approved location of the drainage easement, if approved to be altered from existing, may differ from that shown on the preliminary plat submitted for Planning Commission. Must comply with all storm water and flood control ordinances. Any increase in impervious area in excess of 4,000 square feet will require detention. Any work performed in the right of way will require a right of way permit, in addition to any required land disturbance permits. Drainage from any new dumpster pads cannot discharge to storm sewer; must have connection to sanitary sewer*);
- 6) completion of the utility and drainage easement vacation process prior to the recording of the final plat;
- 7) placement of a note on the final plat stating that Lot 1 should be limited to one curb-cut onto Airport Boulevard, Lot 2 should be limited to one curb-cut onto Airport Boulevard, and Lots 1 and 3 should be limited to one shared curb-cut onto Wesley Avenue, with the size, design and location of all curb-cuts are to be approved by the Planning Section of Urban Development and Traffic Engineering, and comply with AASHTO standards;
- 8) placement of a note on the final plat stating that access to Henckley Avenue and the unopened right-of-way of Government Street is denied;
- 9) labeling of each lot with its size in square feet, or placement of a table on the plat with the same information; and
- 10) placement of a note on the final plat stating that approval of all applicable Federal, state and local agencies for endangered, threatened or otherwise protected species is required prior to the issuance of any permits or land disturbance activities.

# LOCATOR



APPLICATION NUMBER 9 DATE July 22, 2010

APPLICANT Wesley Station Subdivision

REQUEST Subdivision



# WESLEY STATION SUBDIVISION



APPLICATION NUMBER 9 DATE July 22, 2010

LEGEND

R-1	R-2	R-3	R-A	R-B	H-B	T-B	B-1	LB-2	B-2	B-3	B-4	B-5	I-1	I-2
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# WESLEY STATION SUBDIVISION

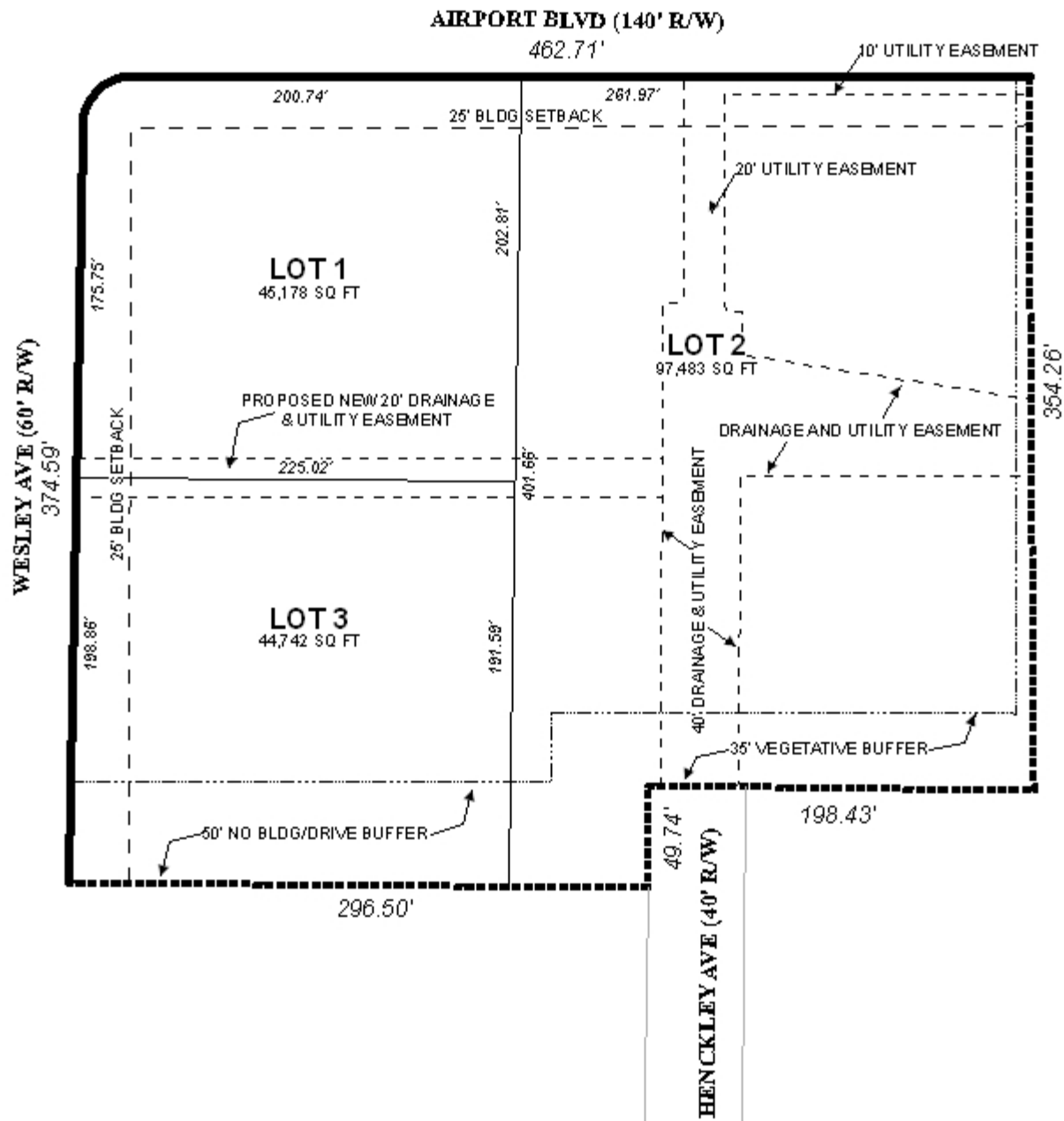


APPLICATION NUMBER 9 DATE July 22, 2010



NTS

# DETAIL SITE PLAN



APPLICATION NUMBER 9 DATE July 22, 2010  
 APPLICANT Wesley Station Subdivision  
 REQUEST Subdivision

