

THE LOFTS AT ST. EMANUEL SUBDIVISION

Engineering Comments: FINAL PLAT COMMENTS (should be addressed prior to submitting the FINAL PLAT for review and/or signature by the City Engineer):

- A. Provide all of the required information on the SUBDIVISION PLAT (i.e. signature blocks, signatures, certification statements, written legal description, required notes, legend, scale, bearings and distances) that is required by the current Alabama State Board of Licensure for Professional Engineers and Land Surveyors.
- B. Revise the written legal description to include the proposed subdivision boundary.
- C. Show and label all flood zones.
- D. Show and label the MFFE (Minimum Finished Floor Elevation) on each lot that contains an AE, V, or X (shaded) flood zone designation.
- E. Provide and label the monument set or found at each subdivision corner.
- F. Provide the Surveyor's, Owner's (notarized), Planning Commission, and Traffic Engineering signatures.
- G. Add a note to the SUBDIVISION PLAT stating that a Land Disturbance permit will be required for any land disturbing activity in accordance with Mobile City Code, Chapter 17, Storm Water Management and Flood Control; the City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control.
- H. Add a note to the Plat stating that the approval of all applicable federal, state, and local agencies (including all storm water runoff, wetland and floodplain requirements) will be required prior to the issuance of a Land Disturbance permit.
- I. Add a note that sidewalk is required to be constructed, and/or repaired, along the frontage of each lot, or parcel, at time of new development or construction, unless a sidewalk waiver is approved.
- J. Provide a copy of the FINAL SUBDIVISION PLAT to the Engineering Dept. for review. No signatures are required on this drawing.
- K. After addressing all of the FINAL SUBDIVISION PLAT review comments by the Engineering Dept. provide the red-line markup, the ORIGINAL (with all other signatures) and one (1) copy of the revised original (signatures not required) of the revised Final Plat to the Engineering Department.

Traffic Engineering Comments: Lot is limited to one curb cut with size, location and design to be approved by Traffic Engineering and conform to AASHTO standards.

Urban Forestry Comments: Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 2015-116 and City Code Chapters 57 and 64).

Fire-Rescue Department Comments: All projects within the City Limits of Mobile shall comply with the requirements of the City of Mobile Fire Code Ordinance (2012 International Fire Code).

MAWSS Comments: No comments.

The plat illustrates the proposed 1-lot, 0.1± acre subdivision which is located on the West side of St. Emanuel Street, 65'± South of Dauphin Street within Council District 2. The applicant states

that the subdivision is served by city water and sanitary sewer systems. The purpose of this application is to create one (1) legal lot of record from two (2) metes-and-bounds parcels.

The site has been given a Downtown land use designation, per the recently adopted Future Land Use Plan and Map. The Future Land Use Plan and Map complements and provides additional detail to the Development Framework Maps in the Map for Mobile, adopted by the Planning Commission at its November 5, 2015 meeting.

Downtown is called out as a separate land use designation due to its distinct role, layout and fabric.

As a land use district, Downtown is the ultimate mixed-use environment. Land development and redevelopment will emphasize variety, mixed uses, and unity of form within buildings or complexes.

As the city's and region's center for commercial and service employment, Downtown supports intense development and a dynamic combination of uses: specialty and regional retail shopping and offices; business support services; urban housing at higher densities (starting at 10 du/ac); civic, educational and cultural destinations; entertainment options; and other public amenities including active and passive park space. The successful integration of a mix of housing types and densities will be critical to achieve a vibrant, 24/7-active Downtown Mobile.

Development in the Downtown district will focus on new, redeveloped and adaptively reused buildings that frame attractive, human-scaled streetscapes, memorable public spaces, bicycle and pedestrian-friendly streets and convenient transit access to jobs, housing and entertainment. Accordingly, certain areas of Downtown will be more intensively developed to facilitate that pedestrian orientation.

It should be noted that the Future Land Use Plan and Map components of the Map for Mobile Plan are meant to serve as a general guide, not a detailed lot and district plan. In many cases the designation on the new Future Land Use Map may match the existing use of land, but in others the designated land use may differ from what is on the ground today. As such, the Future Land Use Plan and Map allows the Planning Commission and City Council to consider individual cases based on additional information such as the classification request, the surrounding development, the timing of the request, and the appropriateness and compatibility of the proposed use and, where applicable, the zoning classification.

The site fronts St. Emanuel Street, a minor street with curb and gutter and an existing 36' right-of-way. Currently, this site and all of the property in the vicinity fronting St. Emanuel Street is built-out and, as such, dedication would be impractical and therefore, if approved, Section V.B.14. of the Subdivision Regulations should also be waived.

Section 64.3.I.8.(b).3 of the DDD ordinance require a minimum property width of 18' and a maximum property width of 120' along the primary frontage in a T-5.2, Mixed-Use, Medium-Intensity Sub-District. The proposed Lot 1 meets the property width requirements.

There are no building setback lines illustrated on the preliminary plat. The DDD, per Section 64-3.I.8.(d), allows from 0' minimum to 12' maximum setback along all street frontages. If approved, the Final Plat should be revised to illustrate a 12' maximum building setback line along all street frontages, with a note stating a 0' minimum is allowed.

The DDD requires a parking setback of at least the building setback for T-5.2 Sub-districts per Section 64-3.I.8.(d) of the DDD code. If approved, a note should be placed on the Final Plat indicating that if parking is provided in the future, a parking setback of at least the building setback will be required.

The proposed lot does not meet the 7,200 square foot minimum lot size requirements of Section V.D.2. of the Subdivision Regulations for lots served by public water and sanitary sewer systems. However, it should be noted that there is no minimum lot area requirement for lots within the DDD. The preliminary plat provides the lot sizes in square feet and in acres. If approved, the lot size information should be retained in both square feet and acres on the Final Plat.

As a means of access management, a note should be placed on the Final Plat, if approved, stating that Lot 1 is limited to one curb cut, with size, location and design to be approved by Traffic Engineering and conform to AASHTO standards.

It should be noted than any new building construction must comply with the frontage requirements of Section 64-3.I.15 of the Zoning Ordinance.

It should also be noted that because this property is located in the Downtown Development District and the Lower Dauphin Street Historic District, therefore any future development to this site will require Consolidated Review Committee (CRC) and Architectural Review Board (ARB) approval.

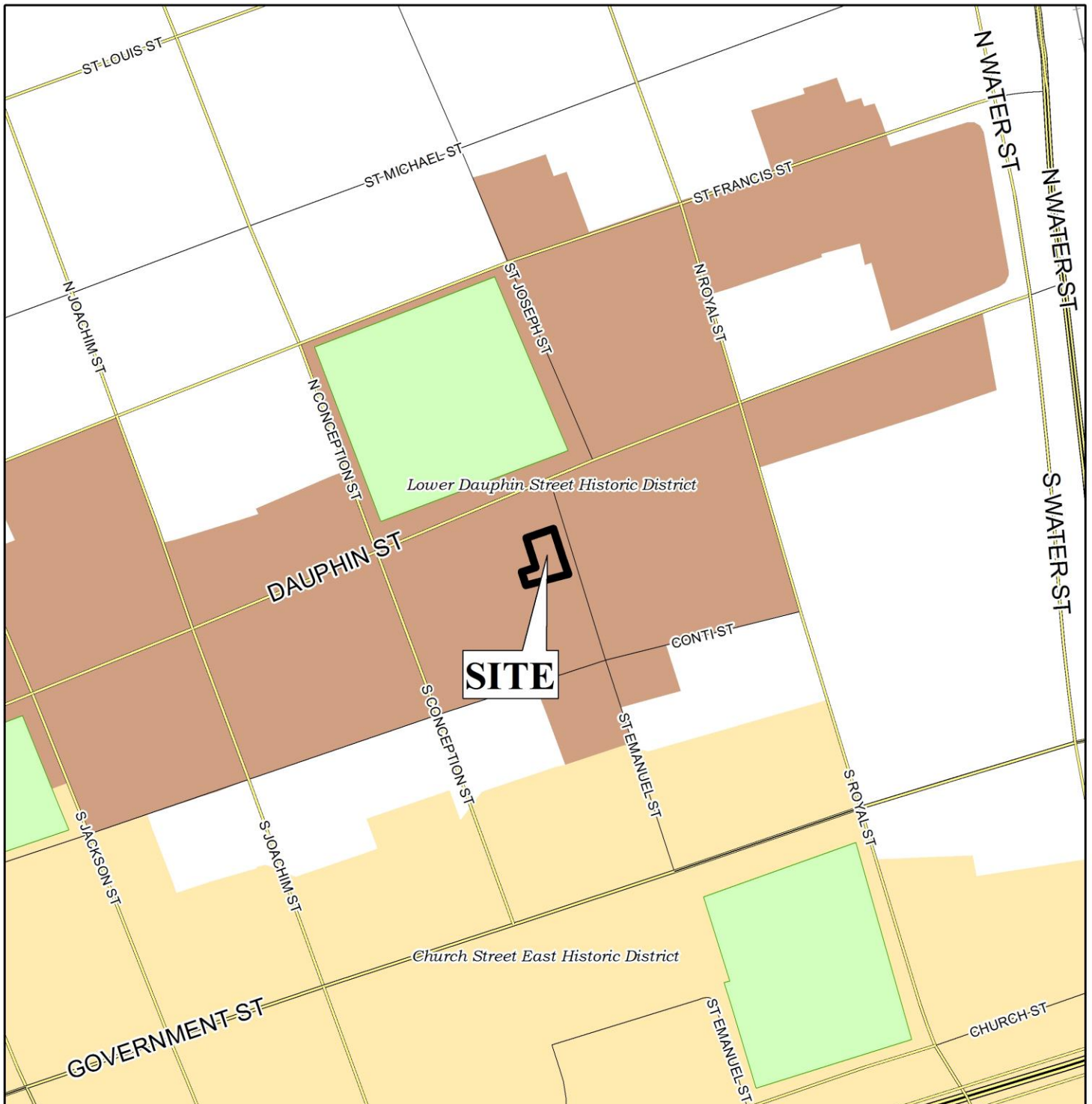
Based upon the preceding, this application is recommended for Tentative Approval, subject to the following conditions:

- 1) Retention of the 36' right-of-way width to St. Emanuel Street;
- 2) Illustration of a 12' maximum building setback line along all street frontages, with a note stating that 0' minimum building setback is allowed;
- 3) Retention of lot size information in both square feet and acres;
- 4) Placement of a note on the Final Plat stating that Lot 1 is limited to one curb cut per street frontage, with size, location and design to be approved by Traffic Engineering and conform to AASHTO standards.
- 5) Placement of a note stating that if parking is provided in the future, a parking setback of at least the building setback will be required.
- 6) Compliance with Engineering comments: (*FINAL PLAT COMMENTS (should be addressed prior to submitting the FINAL PLAT for review and/or signature by the City Engineer): A. Provide all of the required information on the SUBDIVISION PLAT (i.e. signature blocks, signatures, certification statements, written legal description, required notes, legend, scale, bearings and distances) that is required by the current Alabama State Board of Licensure for Professional Engineers and Land Surveyors. B. Revise the written legal description to include the proposed subdivision boundary. C. Show and*

label all flood zones. D. Show and label the MFFE (Minimum Finished Floor Elevation) on each lot that contains an AE, V, or X (shaded) flood zone designation. E. Provide and label the monument set or found at each subdivision corner. F. Provide the Surveyor's, Owner's (notarized), Planning Commission, and Traffic Engineering signatures. G. Add a note to the SUBDIVISION PLAT stating that a Land Disturbance permit will be required for any land disturbing activity in accordance with Mobile City Code, Chapter 17, Storm Water Management and Flood Control; the City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control. H. Add a note to the Plat stating that the approval of all applicable federal, state, and local agencies (including all storm water runoff, wetland and floodplain requirements) will be required prior to the issuance of a Land Disturbance permit. I. Add a note that sidewalk is required to be constructed, and/or repaired, along the frontage of each lot, or parcel, at time of new development or construction, unless a sidewalk waiver is approved. J. Provide a copy of the FINAL SUBDIVISION PLAT to the Engineering Dept. for review. No signatures are required on this drawing. K. After addressing all of the FINAL SUBDIVISION PLAT review comments by the Engineering Dept. provide the red-line markup, the ORIGINAL (with all other signatures) and one (1) copy of the revised original (signatures not required) of the revised Final Plat to the Engineering Department.);

- 7) Compliance with Traffic Engineering comments: (Lot is limited to one curb cut with size, location and design to be approved by Traffic Engineering and conform to AASHTO standards.);
- 8) Compliance with Urban Forestry comments (Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 2015-116 and City Code Chapters 57 and 64).); and Compliance with Fire Department comments and placement of a note (All projects within the City Limits of Mobile shall comply with the requirements of the City of Mobile Fire Code Ordinance (2012 International Fire Code).

LOCATOR MAP



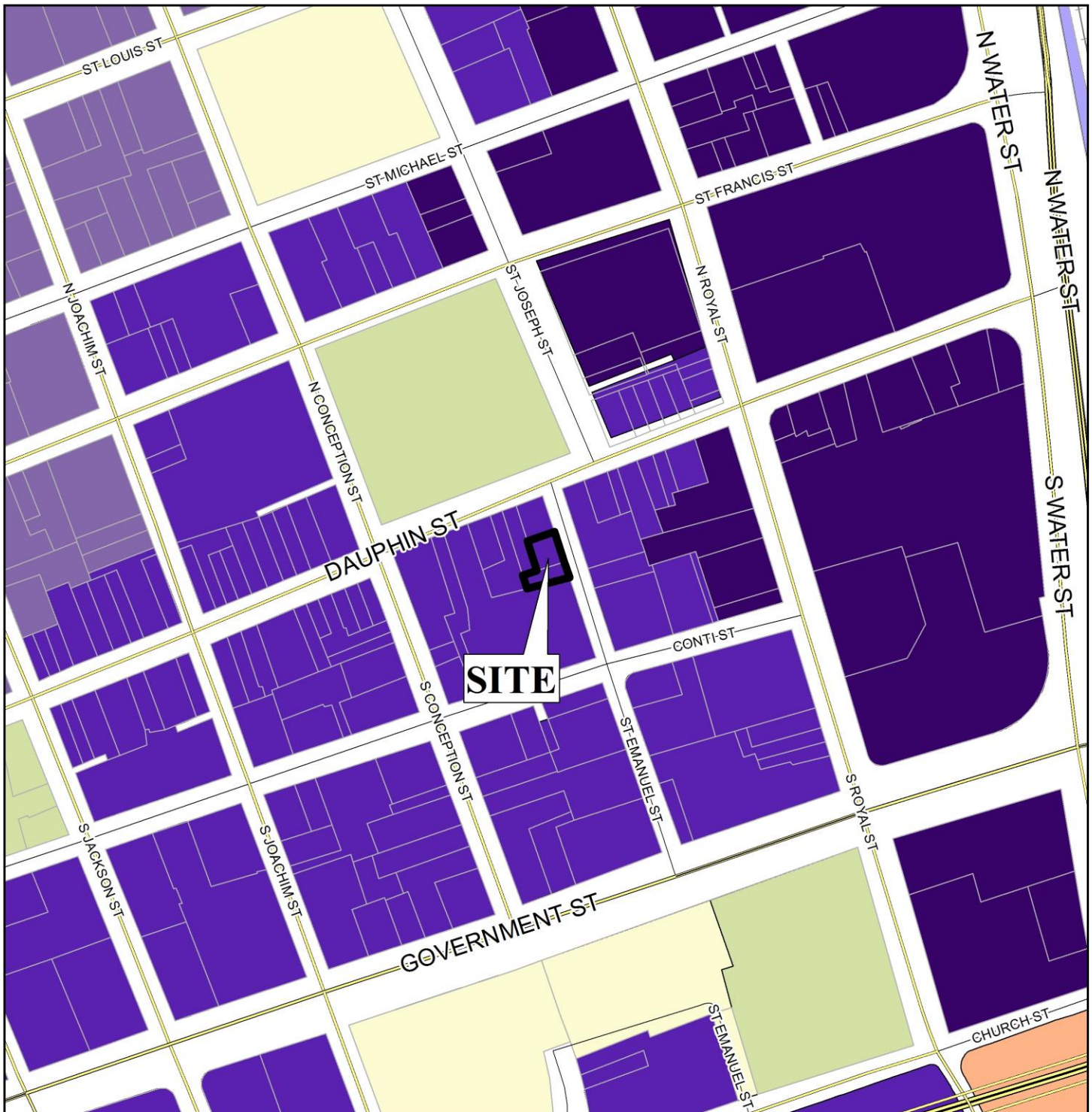
APPLICATION NUMBER 8 DATE April 19, 2018

APPLICANT The Lofts at St. Emanuel Subdivision

REQUEST Subdivision



LOCATOR ZONING MAP



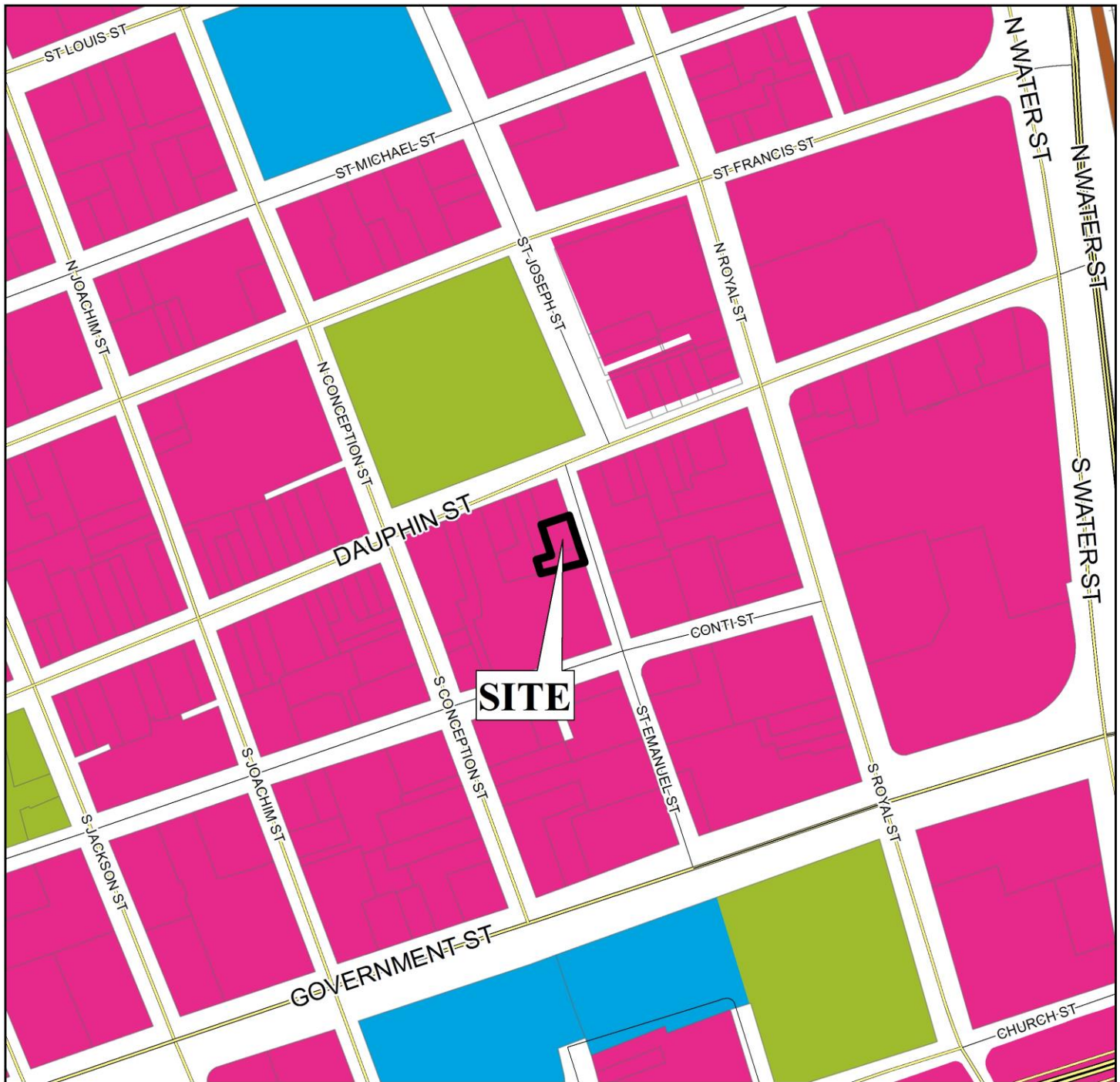
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REQUEST Subdivision



FLUM LOCATOR MAP



APPLICATION NUMBER 8 DATE April 19, 2018

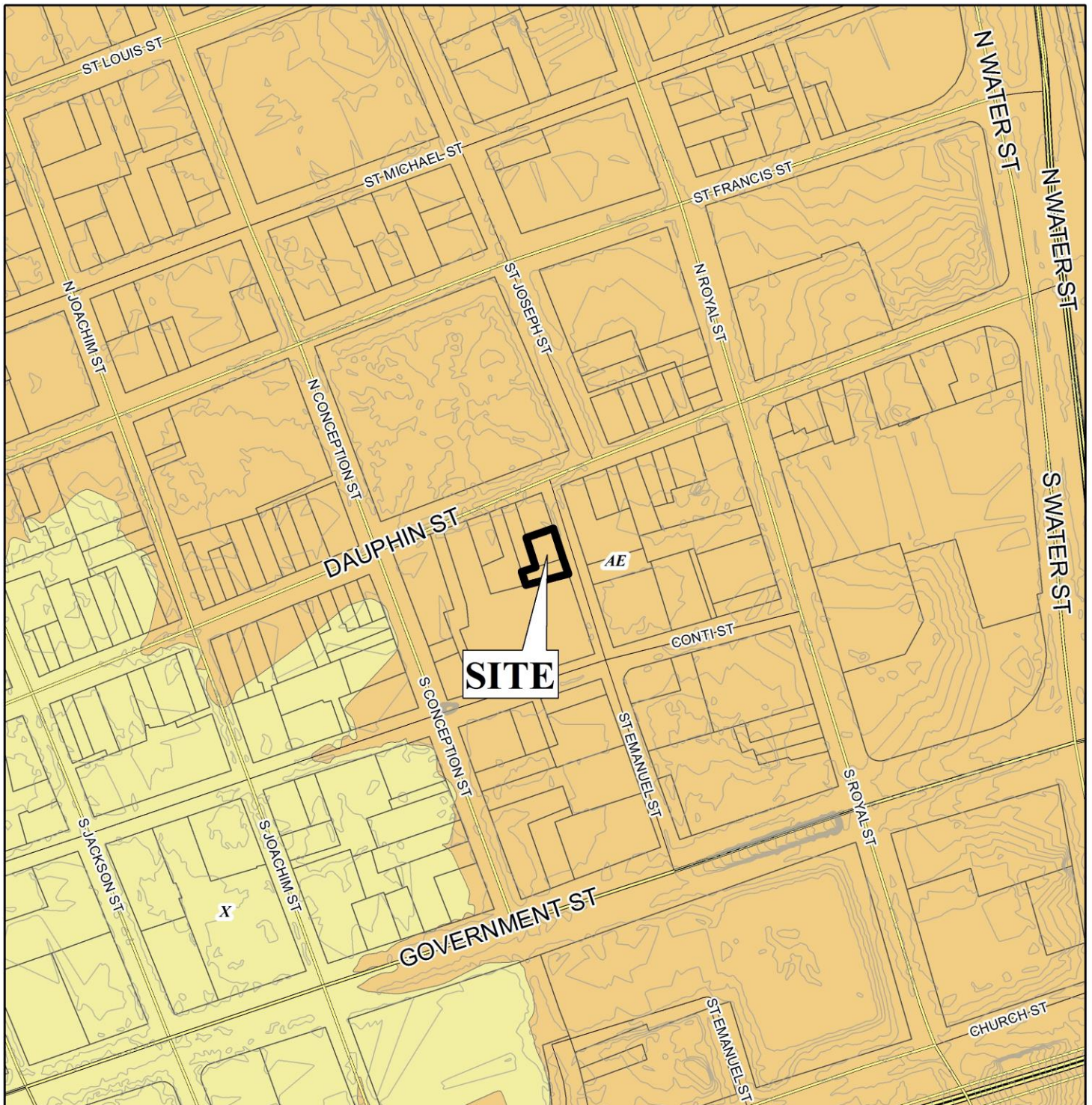
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Low Density Residential	Neighborhood Center - Traditional	Downtown Waterfront	Parks & Open Space
Mixed Density Residential	Neighborhood Center - Suburban	Light Industry	Water Dependent
Downtown	Traditional Corridor	Heavy Industry	
District Center	Mixed Commercial Corridor	Institutional	



ENVIRONMENTAL LOCATOR MAP



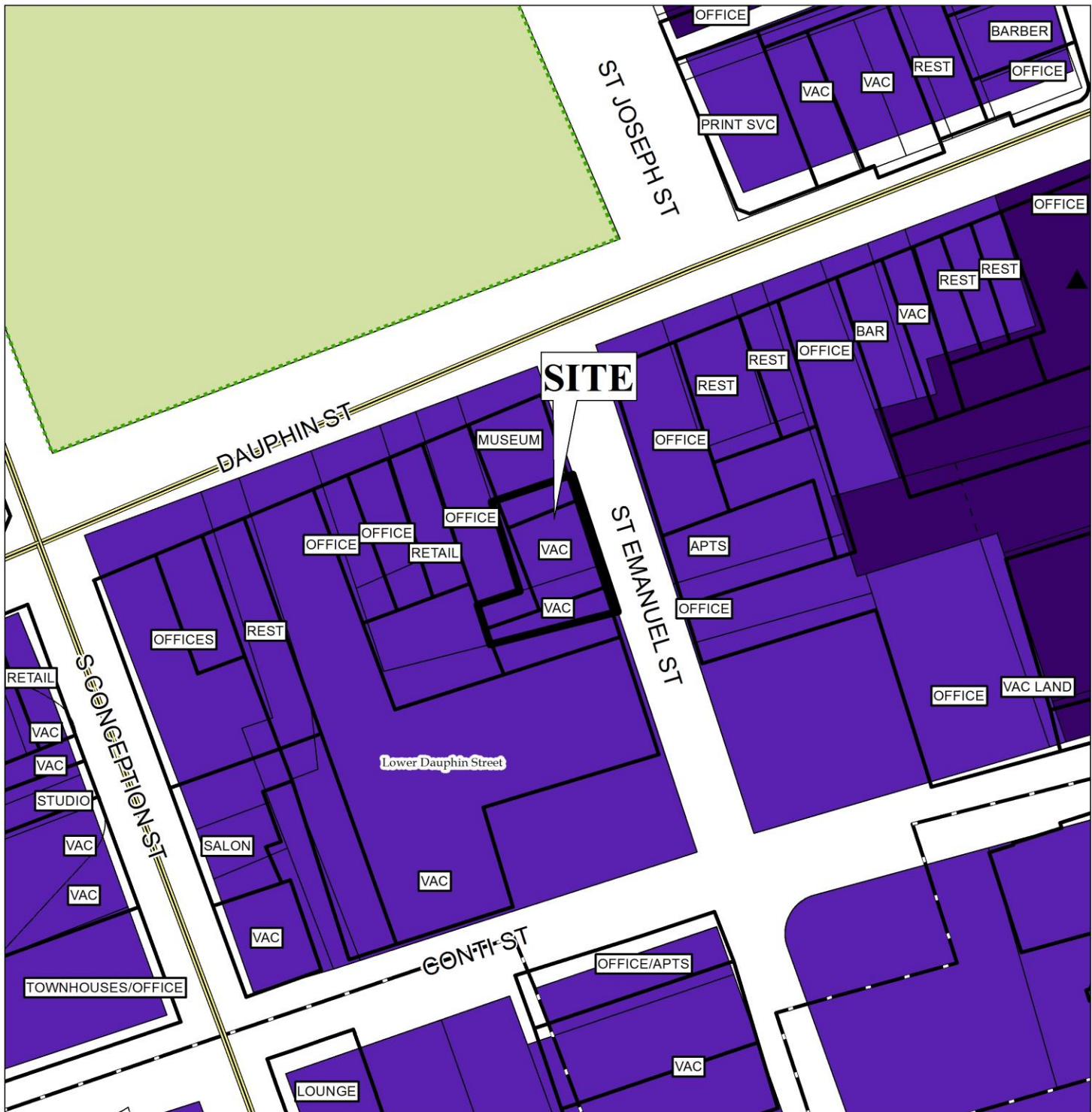
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REQUEST Subdivision



THE LOFTS AT ST. EMANUEL SUBDIVISION



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R-A	R-3	T-B	B-2	B-5	MUN	SD-WH	T5.1
R-1	R-B	B-1	B-3	I-1	OPEN	T3	T5.2
R-2	H-B	LB-2	B-4	I-2	SD	T4	T6



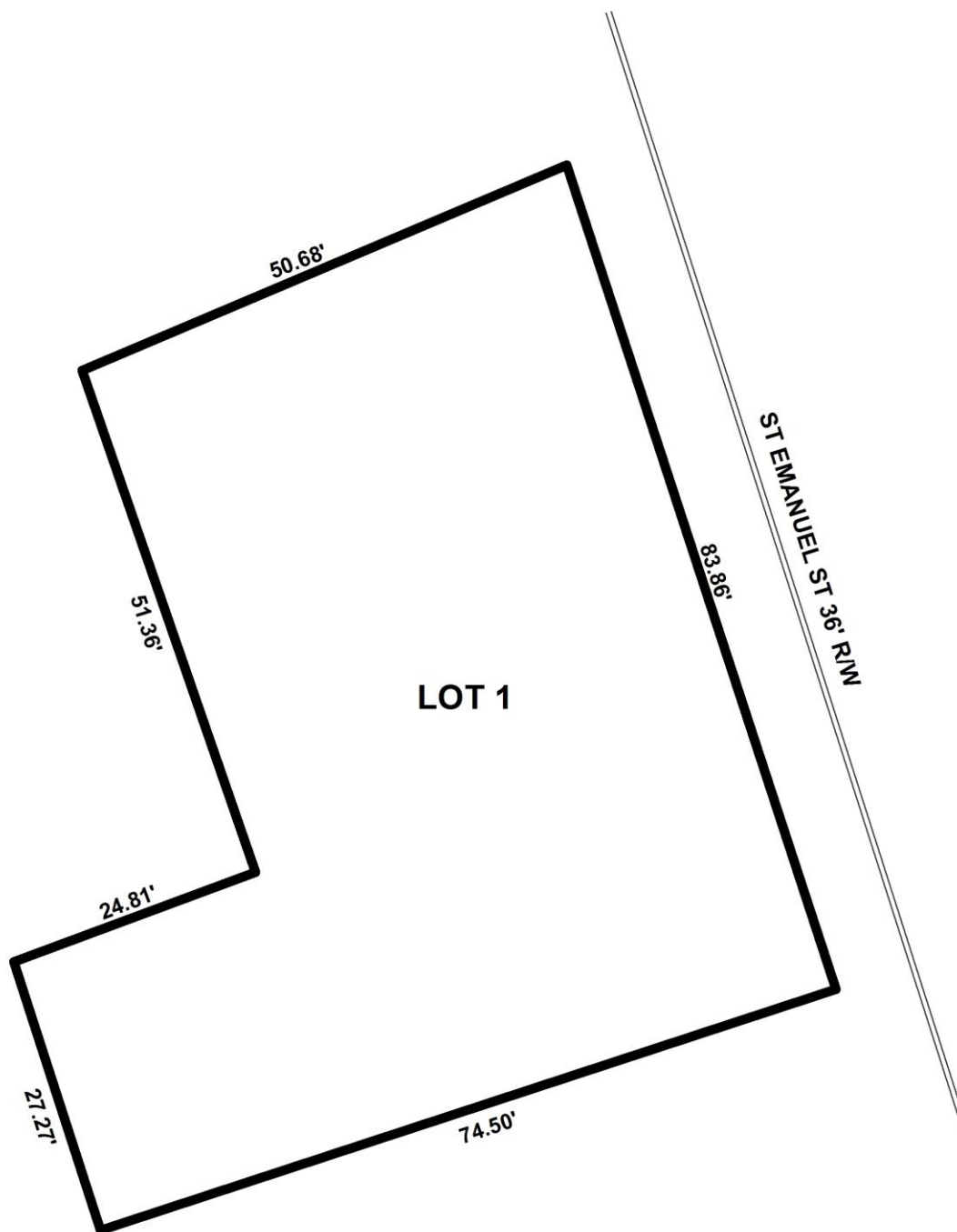
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APPLICATION NUMBER 8 DATE April 19, 2018



DETAIL SITE PLAN



APPLICATION NUMBER 8 DATE April 19, 2018

APPLICANT The Lofts at St. Emanuel Subdivision

REQUEST Subdivision



