

**PLANNED UNIT DEVELOPMENT
STAFF REPORT****Date: July 15, 2004**

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| <u>DEVELOPMENT NAME</u> | D'Iberville Town Homes |
| <u>LOCATION</u> | South side of Southland Drive, 800'± West of Knollwood Drive, extending to the West terminus of Southland Drive |
| <u>CITY COUNCIL DISTRICT</u> | District 6 |
| <u>PRESENT ZONING</u> | R-3, Multi-Family Residential |
| <u>AREA OF PROPERTY</u> | 12 ½ Acres |
| <u>CONTEMPLATED USE</u> | Multiple buildings on a single building site for a residential, condominium, town-home complex with private streets and shared parking. |
| <u>TIME SCHEDULE FOR DEVELOPMENT</u> | Phase One—August 2004 |
| <u>ENGINEERING COMMENTS</u> | Must comply with all stormwater and flood control ordinances. Any work performed in the right of way will require a right of way permit. |
| <u>TRAFFIC ENGINEERING COMMENTS</u> | Driveway number, size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards. All one-way traffic flows should be signed and marked according to MUTCD standards. |
| <u>URBAN FORESTRY COMMENTS</u> | Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64). |
| <u>REMARKS</u> | The applicant proposes to develop the lot in phases; phase one will consist of 33 units and phase 2 will have 31 units. Whereas multiple buildings on one lot are proposed, Planned Unit Development (PUD) approval is required. |

Planned Unit Development review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating excess traffic along minor residential streets in residential districts

outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD.

The site received Tentative Approval as a Lot B, The Preserve Subdivision. The staff has met with the agent; however, some of the terms used in the application are inconsistent and with Mobile's ordinances and regulations and at this point the specific concerns should be addressed.

First, there is a note on the site plan stating that the individual units are to be sold and described as a single lot. In development meetings with the agent, the individual townhomes will be sold as a "unit" of Lot B, The Preserve Subdivision. This is essentially a condominium form of ownership that would be allowed with PUD approval and would not require subdivision approval—no new lots are being created, simply "units" of Lot B, The Preserve Subdivision are being sold. However, Lot B, The Preserve Subdivision, must be recorded prior to the issuance of any building permits for the site.

The second point of concern relates to traffic circulation. As illustrated on the site plan, the individual units will be served by either a marginal access road, which has a right-of-way of 40-feet; or alleys having 20-feet of right-of-way. Rights-of-ways would indicate that either the streets are to be constructed to city standards and accepted by the City for maintenance, or that private streets are proposed; either situation would require subdivision approval. This was explained to the agent; however, no subdivision application was submitted, and based upon extensive discussion with the agent, these "roads" and "alleys" are simply internal circulation drives similar to those serving other types of multi-family residential developments. Nevertheless, it should be noted that the minimum width needed for two-way traffic is 24-feet and additional width may be needed to comply with Traffic Engineering requirements.

In regard to parking, each unit will have a two-car garage, and there are multiple parking areas scattered throughout both sites; the number of parking spaces exceeds the minimum requirements of the Zoning Ordinance.

The individual buildings will have between two and five units per building and in most instances there will be a common or service area between the buildings. The PUD Section of the Zoning Ordinance requires a minimum separation of 8 feet between two-story buildings and windows would not be allowed on the exterior walls flanking the common or service areas.

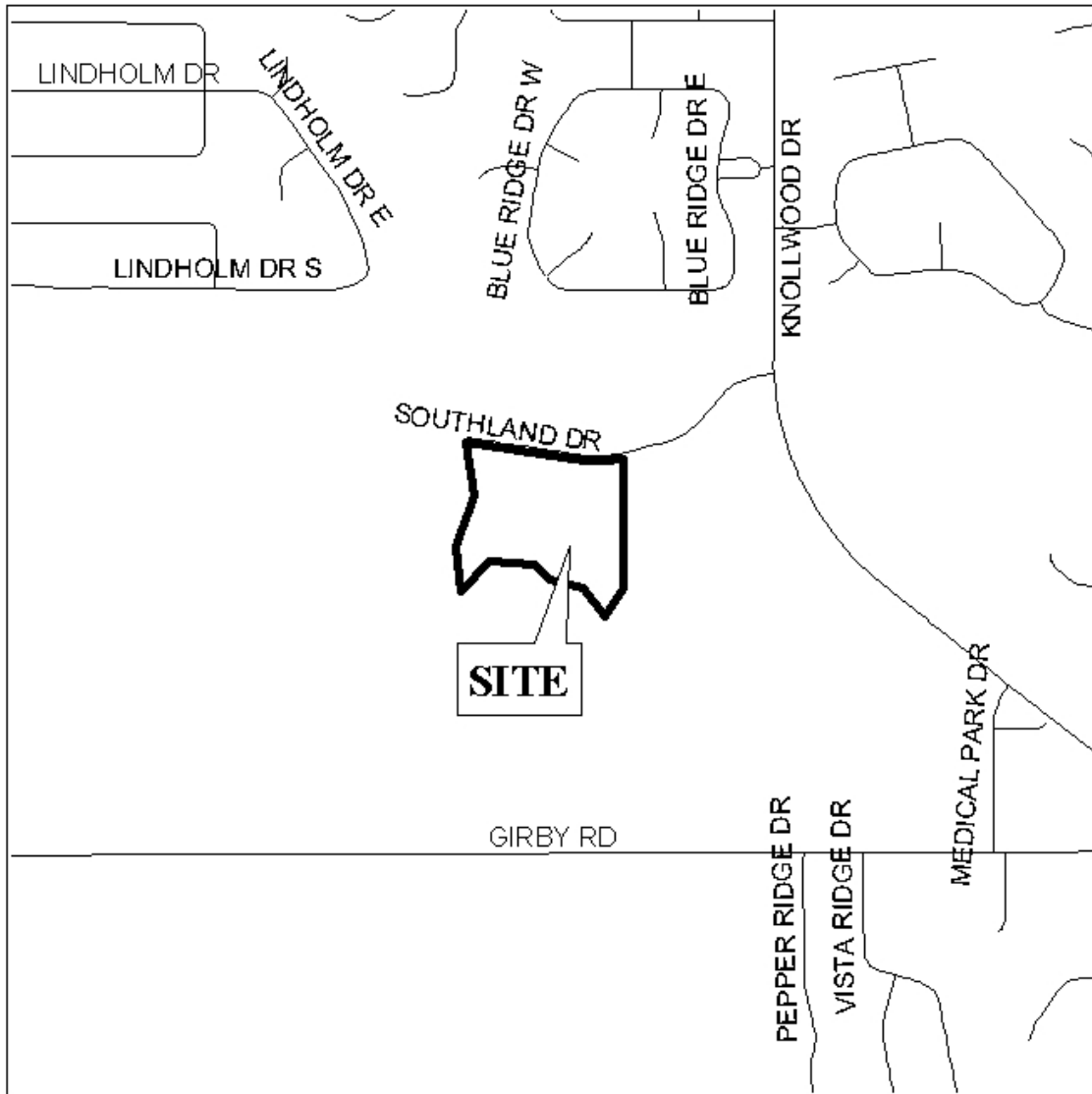
The applicant proposes a 20-foot setback along Southland Drive. The standard setback is 25-feet; however, the streetscape will contain a sidewalk, street plantings, lighting and benches, and the slightly reduced setback is more characteristic of the urban community setting the developer is proposing.

As with any new construction, full compliance with the landscaping and tree planting requirements of the Ordinance will be required. Additionally, the site contains wetlands and adjoins a creek, thus the area could be considered environmentally sensitive; therefore, the approval of all applicable federal, state and local agencies would be required. Moreover, as the site adjoins R-1, Single-Family Residential property to the South, the provision of a buffer in compliance with Section IV.D.1. should be provided.

RECOMMENDATION

Based upon the preceding, this application is recommended for approval subject to the following conditions: 1) that Lot B, The Preserve Subdivision be recorded prior to the issuance of any building permits for the site; 2) that the marginal access roads and alleys simply be developed as internal circulation drives and comply with the Zoning Ordinance (minimum width of 24-feet for two-way traffic) and Traffic Engineering requirements; 3) that a minimum separation of eight-feet be provided between two-story buildings and no windows allowed on the flanking walls; 4) full compliance with the landscaping and tree planting requirements of the Zoning Ordinance; 5) the approval of all applicable federal, state and local agencies; 6) the provision of a sidewalk, plantings, benches and lighting along Southland Drive as stating in the application; 7) the provision of a buffer, in compliance with Section IV.D.1. where the site adjoins R-1, Single-Family Residential zoning; and 8) full compliance with all municipal codes and ordinances.

LOCATOR MAP



APPLICATION NUMBER 8 & 9 DATE July 1, 2004

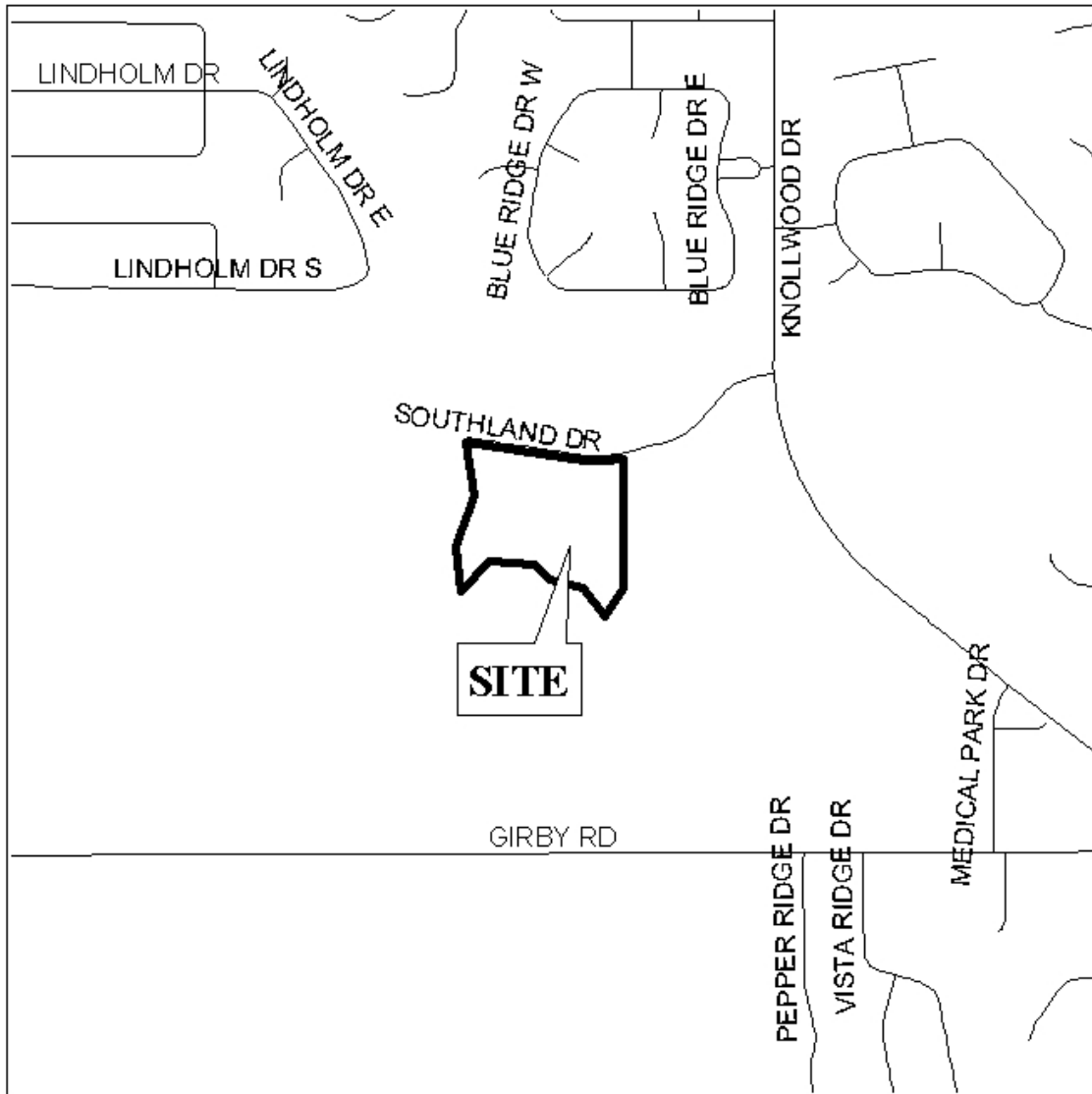
APPLICANT D'Tberville Town Homes

REQUEST Planned Unit Development



NTS

LOCATOR MAP



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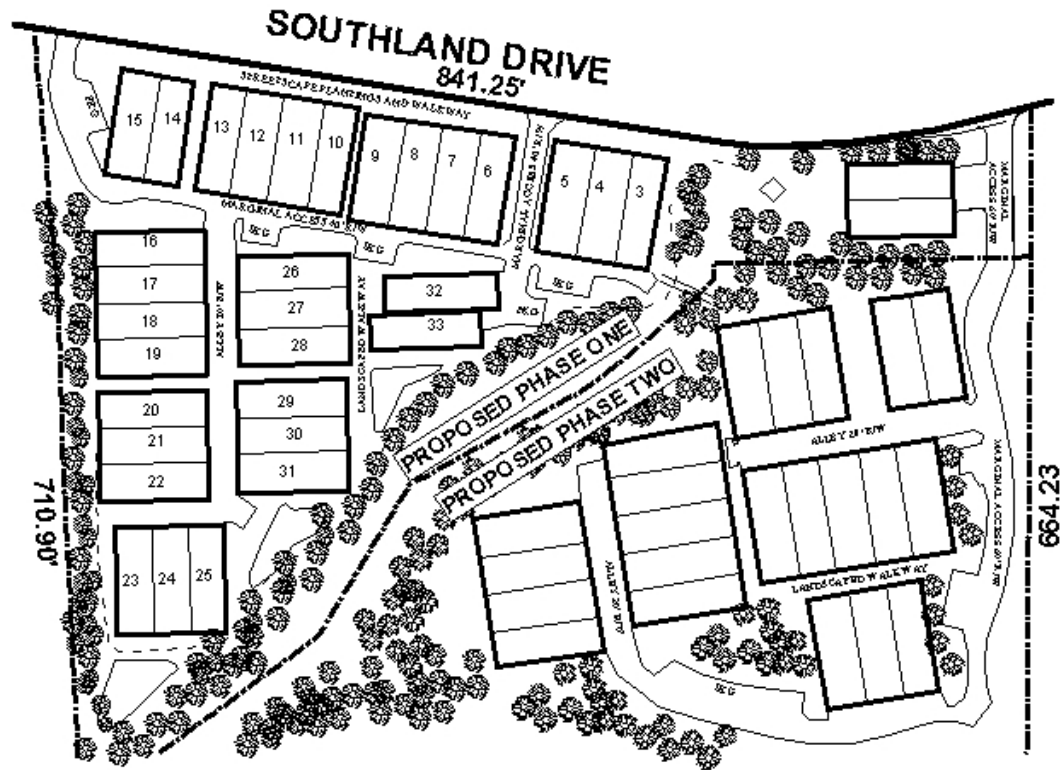
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SITE PLAN



The site is located on the South side of Southland Drive, 800' West of Knollwood Drive, extending to the West terminus of Southland Drive. The plan illustrates the proposed structures, drives, and landscaping.

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