

**ZONING AMENDMENT,
PLANNED UNIT DEVELOPMENT &
SUBDIVISION STAFF REPORT****Date: November 15, 2018**

<u>NAME</u>	Abhishek Banerjee
<u>SUBDIVISION NAME</u>	Villas at Spring Hill Subdivision
<u>LOCATION</u>	162 and 186 East Drive (West side of East Drive, 130'± North of Sussex Drive, extending to the East side of Center Street).
<u>CITY COUNCIL DISTRICT</u>	District 6
<u>PRESENT ZONING</u>	R-1, Single-Family Residential District
<u>PROPOSED ZONING</u>	R-2, Two-Family Residential District
<u>REASON FOR REZONING</u>	An increased need for multi-family housing.
<u>AREA OF PROPERTY</u>	24 Lots / 4.0± Acres
<u>CONTEMPLATED USE</u>	<p>Subdivision approval to create 24 legal lots of record from two legal lots of record, Planned Unit Development approval to allow reduced front and side yard setbacks for a proposed subdivision, and Rezoning from R-1, Single-Family Residential to R-2, Two-Family Residential.</p> <p>It should be noted, however, that any use permitted in the proposed district would be allowed at this location if the zoning is changed. Furthermore, the Planning Commission may consider zoning classifications other than that sought by the applicant for this property.</p>
<u>TIME SCHEDULE</u>	None given.
<u>ENGINEERING COMMENTS</u>	<p>Subdivision: FINAL PLAT COMMENTS (should be addressed prior to submitting the FINAL PLAT for review and/or signature by the City Engineer):</p> <p>A. Provide all of the required information on the SUBDIVISION PLAT (i.e. signature blocks, signatures, certification statements, written legal description, required notes,</p>

- legend, scale, bearings and distances) that is required by the current Alabama State Board of Licensure for Professional Engineers and Land Surveyors.
- B. Show and label the POB.
 - C. Provide and label the monument set or found at each subdivision corner.
 - D. Label the proposed ROW with a name.
 - E. Label the proposed ROW as either PRIVATE or PUBLIC.
 - F. Dedicate the corner radius (25' minimum or as approved by the City Engineer and Traffic Engineer) at the east end of the UNNAMED PROPOSED ROW.
 - G. Designate each Common Area with an individual designator (i.e. #1, A)
 - H. Provide the Surveyor's, Owner's (notarized), Planning Commission, and Traffic Engineering signatures.
 - I. Add a note to the SUBDIVISION PLAT stating that a Land Disturbance permit will be required for any land disturbing activity in accordance with Mobile City Code, Chapter 17, Storm Water Management and Flood Control; the City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control.
 - J. Add a note to the Plat stating that the approval of all applicable federal, state, and local agencies (including all storm water runoff, wetland and floodplain requirements) will be required prior to the issuance of a Land Disturbance permit.
 - K. Add a note that sidewalk is required to be constructed, and/or repaired, along the frontage of each lot, or parcel, at time of new development or construction, unless a sidewalk waiver is approved.
 - L. Add a note to the plat stating that all proposed detention facilities, common areas, and wetlands shall be the responsibility of the Property Owner(s), and not the responsibility of the City of Mobile.
 - M. Add a note to the plat stating that all easements shall remain in effect until vacated through the proper Vacation process.
 - N. Provide a copy of the FINAL SUBDIVISION PLAT to the Engineering Dept. for review. No signatures are required on this drawing.
 - O. After addressing all of the FINAL SUBDIVISION PLAT review comments by the Engineering Dept. provide the red-line markup, the ORIGINAL (with all other signatures) and one (1) copy of the revised original (signatures not required) of the revised Final Plat to the Engineering Department.

Planned Unit Development: ADD THE FOLLOWING NOTES TO THE PUD SITE PLAN:

- 1. Any work performed in the existing ROW (right-of-way) such as driveways, sidewalks, utility connections, grading, drainage, irrigation, or landscaping will require a ROW permit from the City of Mobile Engineering Department (208-6070) and must comply with the City of Mobile Right-of-Way Construction and Administration Ordinance (Mobile City Code, Chapter 57, Article VIII).
- 2. A Land Disturbance Permit application shall be submitted for any proposed land disturbing activity with the property. A complete set of construction plans including, but not limited to, drainage, utilities, grading, storm water detention systems and paving will

- need to be included with the Land Disturbance permit. This Permit must be submitted, approved, and issued prior to beginning any of the construction work.
3. Any and all proposed land disturbing activity within the property will need to be submitted for review and be in conformance with Mobile City Code, Chapter 17, Storm Water Management and Flood Control); the City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control.
 4. Any existing or proposed detention facility shall be maintained as it was constructed and approved. The Land Disturbance Permit application for any proposed construction includes a requirement of a Maintenance and Inspection Plan (signed and notarized by the Owner) for the detention facility. This Plan shall run with the land and be recorded in the County Probate Office prior to the Engineering Department issuing their approval for a Final Certificate of Occupancy.
 5. The approval of all applicable federal, state, and local agencies (including all storm water runoff, wetland and floodplain requirements) will be required prior to the issuance of a Land Disturbance permit. The Owner/Developer is responsible for acquiring all of the necessary permits and approvals.
 6. The proposed development must comply with all Engineering Department design requirements and Policy Letters.

Rezoning: NO COMMENTS

TRAFFIC ENGINEERING

COMMENTS

Each lot is limited to one curb cut to the proposed right-of-way. The common areas are denied any direct access from East Drive. If the proposed roadway is public, corner radii dedication should be included in the common area lots at the intersection with East Drive. Consideration should be given to the building layouts relative to the building layouts across the street (within the development). Backing out of driveways becomes very problematic if on street parking occurs opposite driveways. Parking restrictions may have to be implemented by the City (if public ROW) if access for public services is hindered (ex. garbage, fire access).

URBAN FORESTRY

COMMENTS

Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 2015-116 and City Code Chapters 57 and 64). Private removal of trees in the right-of-way will require approval of the Mobile Tree Commission. Removal of heritage trees from a commercial site will require a tree removal permit.

FIRE DEPARTMENT

COMMENTS

Residential developments shall meet fire water and fire access requirements as set forth in the 2012 IFC, including section 503, and appendix D and C.

REMARKS

The applicant is requesting Subdivision approval to create 24 legal lots of record from two legal lots of record, Planned Unit Development approval to allow reduced front and side yard setbacks for a proposed subdivision, and Rezoning from R-1, Single-Family Residential to R-2, Two-Family Residential, to accommodate a new single-family residential development with increased site coverage and reduced lot sizes..

The site has been given a Mixed Density Residential land use designation per the recently adopted Future Land Use Plan and Map. The Future Land Use Plan and Map complements and provides additional detail to the Development Framework Maps in the Map for Mobile, adopted by the Planning Commission at its November 5, 2015 meeting.

This designation applies mostly to residential areas located between Downtown and the Beltline, where the predominant character is that of a traditional neighborhood laid out on an urban street grid.

These residential areas should offer a mix of single family homes, townhouses, 2- to 4-residential unit buildings, accessory dwellings, and low- and mid-rise multifamily apartment buildings. The density varies between 6 and 10 du/ac, depending on the mix, types, and locations of the housing as specified by zoning.

Like Low Density Residential areas, Mixed Density Residential areas may incorporate compatibly scaled and sited complementary uses such as neighborhood retail and office uses, schools, playgrounds and parks, and churches and other amenities that create a complete neighborhood fabric and provide safe and convenient access to daily necessities.

It should be noted that the Future Land Use Plan and Map components of the Map for Mobile Plan are meant to serve as a general guide, not a detailed lot and district plan. In many cases the designation on the new Future Land Use Map may match the existing use of land, but in others the designated land use may differ from what is on the ground today. As such, the Future Land Use Plan and Map allows the Planning Commission and City Council to consider individual cases based on additional information such as the classification request, the surrounding development, the timing of the request, and the appropriateness and compatibility of the proposed use and, where applicable, the zoning classification.

Subdivision review examines the site with regard to promoting orderly development, protecting general health, safety and welfare, and ensuring that development is correlated with adjacent developments and public utilities and services, and to ensure that the subdivision meets the minimum standards set forth in the Subdivision Regulations for lot size, road frontage, lot configuration, etc.

The applicant states:

We are submitting a subdivision, PUD, and rezoning application on the properties located at 162 and 186 East Drive. The applicants have met with the neighbors and city council representative to look at their concerns prior to this submittal. They have also

met with the city as part of a predevelopment meeting and we have taken their comments and addressed their concerns along with incorporating their recommendations.

The neighbors want my client to access an unpaved, unimproved public right of way at Center Drive that has no traffic lighting at Old Shell Rd. We are looking to access the existing public (developed road) on East Drive. This is the only logical location for access for the proposed development at this time.

We are proposing a 24-lot hammerhead configured subdivision that meets the code standards for this development. Their engineer has looked at the other components of the development and will look to place detention on the east side of the development and tie into public drainage system from there. They are asking for a waiver of the 60 foot wide lot to a 45 foot wide minimum, along with a reduction of the square and footage minimums on all lots. This is consistent with Sussex Place to the immediate south of our proposed development. On the PUD we are asking for a reduction of setbacks on the front to 20 feet, sides of 5 feet, and the rear to 5 feet to accommodate the homes intended to be built. We also have a drainage easement on the south side of the development that has not been designed yet. This will affect the sizes of these lots and will be the reason for additional reduction in the setbacks. Our building site coverage would not exceed 50% and we would ask that this be the allowable site coverage for this development. We have attached a site plan to illustrate the plans intended to be built on these lots, thus needed for reduction in traditional setbacks due to smaller lot sizes.

For the rezoning, during the pre-development meeting our client was told that we would need to request an R-2 designation based on the reduction in lot sizes. We fall under both (b) and (d) for rezoning justification due to changing conditions in the area that are having density changes to previously larger land tract to more consistent with Sussex Place and more intense multi-family developments. The subdivision is the other criteria for rezoning due to lot size reductions and we ask this be approved based on changing conditions in the area.

We thank you in advance for your consideration.

The site has frontage on East Drive and Center Drive which are both minor streets requiring 50' rights-of-way. As on the preliminary plat, the East Drive right-of-way width is a compliant 50'; therefore, no dedication would be required along East Drive. The 50' right-of-way indicated along Center Drive is erroneous, as it is currently indicated to be approximately 40' on the City GIS database. Staff has also verified this information with the City Engineering Department Right-of-Way Division. Therefore, dedication would be required to provide 25' from the centerline of Center Drive. If approved, the Final Plat should be revised to verify the current right-of-way width of Center Drive, and should indicate dedication sufficient to provide 25' from the centerline of Center Drive. A 25' corner radius curve should be dedicated on the North and South sides of the proposed street at the intersection with East Drive.

As the applicant states that the site is served by public water and sewer services, the minimum lot size allowed under Section V.D.2. of the Subdivision Regulations is 7,200 square feet. As this is

proposed as an innovative subdivision with reduced lot sizes for all lots, a waiver of this section would be required. As on the preliminary plat, the lot sizes in both square feet and acres should be retained on the Final Plat. The size of the two Common Areas should be revised to reflect the areas after any required corner radius dedications.

Increased site coverage to 50% is requested via the PUD. If increased site coverage is approved, the Final Plat should contain a note stating what maximum site coverage is allowed.

As a means of access management, the site should be limited to a single curb cut to East Drive for the new street, with the size, location and design to be approved by Traffic Engineering and conform to AASHTO standards. As Center Drive is a substandard right-of-way, access to Center Drive should be denied for Lots 10 through 15. A note should be required on the Final Plat stating that all lots and the Common Areas are limited to one curb cut to the proposed interior street, with the size, location and design to be approved by Traffic Engineering and conform to AASHTO standards. A note should also be placed on the Final Plat stating that the Common Areas are denied direct access to East Drive.

As reduced minimum building setback lines are proposed, a waiver of Section V.D.9. would be required. The preliminary plat indicates the proposed reduced minimum building setback lines along all street frontages. However, the setback line along the front of Lots 10 through 15 is not labeled or dimensioned. Therefore, the plat should be revised to label this line as the minimum building setback line and provide the dimension from the front property line. The minimum building setback line along all other street frontages should be retained on the Final Plat. The plat does not illustrate the proposed side and rear yard setbacks; therefore, the plat should be revised to illustrate any reduced side yard and rear yard setbacks. The rear setback line for Lots 16 through 24 should be adjusted to align with the 10' drainage easement.

It should be noted that the GIS database indicates residences and accessory structures are currently on the site. Therefore, prior to signing the Final Plat, all structures will need to be removed from the site with appropriate permits.

The plat indicates a drainage easement along the South of the site. Therefore, a note should be placed on the Final Plat stating that no structure may be constructed or placed in this easement. A note should also be required on the Final Plat stating that the maintenance of the Common Areas is the responsibility of the property owners.

Planned Unit Development review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating excess traffic along minor residential streets in residential districts outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD. PUD approval is *site plan specific*, thus if any new construction is anticipated that will change an approved site plan, an application to amend an existing, approved PUD must be made

prior to any construction activities. It should be noted that if no permits are pulled within a year of approval, the PUD will expire.

In pursuit of these purposes, the objectives to be met by a PUD are as follows: a) to encourage innovative and diversified design in building form and site development; b) flexibility, to permit greater flexibility in the location and arrangement of buildings and uses than is generally possible under district regulations; c) to encourage the most efficient and sustainable use of land, especially tracts in the inner part of the city that remain undeveloped or that are appropriate for re-development; d) to preserve and protect as urban amenities the natural features and characteristics of land; e) to encourage the provision of common open space through efficient site design; and, f) to encourage optimum use of available public utilities, streets and community facilities.

The PUD site plan mirrors the subdivision preliminary plat and revisions required of the preliminary plat would also be required of the PUD site plan. The site plan should also be revised to provide a public sidewalk along all street frontages, or a Sidewalk Waiver application should be submitted.

Two typical site plans are provided with the PUD, one for Lot 5, and one for Lot 20. They are identical, except that the Lot 20 plan indicates the 10' drainage easement along the rear (South) of the lot. However, the proposed 5' rear setback line is illustrated within the drainage easement. As no structure is allowed to be placed or constructed within an easement, the site plan for Lot 20 should be revised to remove the 5' minimum building setback line from within the easement and relocate it to coincide with the 10' drainage easement line. This revision should also be done for Lots 16 through 24. All other reduced minimum building setback lines should be illustrated on all lots on a revised site plan. As a maximum site coverage of 50% is proposed, a note should be required on a revised PUD site plan stating that the maximum allowable site coverage for all lots is 50%.

As numerous revisions are required of the site plan, two copies of a revised PUD site plan should be submitted to and approved by Planning and Zoning prior to signing the Final Plat for the Subdivision.

The proposed innovative subdivision is similar to the adjacent Sussex Place adjoining to the South, approved in 1977. Adjoining that development on its South side is another similar innovative subdivision, Magnolia Place, approved in 2004. To the rear across Center Drive are two other innovative subdivisions with reduced lot sizes and reduced setbacks, Bradford Place Townhomes and Ridgefield Commons. Bradford Place Townhomes was approved in 2005, and Ridgefield Commons was approved in 2002. Therefore, there is a propensity for this type of development within the immediate area.

As stated in Section 64-9. of the Zoning Ordinance, the intent of the Ordinance and corresponding Zoning Map is to carry out the comprehensive planning objective of sound, stable and desirable development. While changes to the Ordinance are anticipated as the city grows, the established public policy is to amend the ordinance only when one or more of the following conditions prevail: 1) there is a manifest error in the Ordinance; 2) changing conditions in a

particular area make a change in the Ordinance necessary and desirable; 3) there is a need to increase the number of sites available to business or industry; or 4) the subdivision of land into building sites makes reclassification of the land necessary and desirable.

Rezoning is required for the proposed development to allow a denser concentration in a proposed single-family residential subdivision. The site is bounded to the North by R-1, Single-Family Residential District; to the East by R-3, Multi-Family Residential District and R-1; to the South by R-2, Two-Family Residential District; and to the West by R-1. As the surrounding area is currently a mixture of residential zonings, and as the area is classified as Mixed Density Residential on the FLUM, it would seem that the rezoning is appropriate for this area and would be compatible with current and intended future uses. Also, the proposed subdivision of the property into building sites, and changing residential conditions in the area make reclassification of the land necessary and desirable.

Given the size of the subject property, and the surrounding zoning classifications, if the proposed rezoning were approved, it would not result in what is considered spot zoning. Furthermore, East Drive is accessed by all residential zoning classifications and uses mentioned above.

RECOMMENDATION

Subdivision: With waivers of Sections V.D.2. and V.D.9. of the Subdivision Regulations, the Subdivision request is recommended for Tentative Approval, subject to the following conditions:

- 1) verification of the right-of-way width of Center Drive and dedication of right-of-way sufficient to provide 25' from the centerline of Center Drive if currently less than 25';
- 2) dedication to provide a 25' corner radius on the North and South sides of the proposed street at the intersection with East Drive;
- 3) retention of the lot sizes in both square feet and acres on the Final Plat, adjusted for any required dedication;
- 4) revision of the size of the Common Areas after any required corner radius dedications;
- 5) placement of a note on the Final Plat stating that all lots are limited to a maximum site coverage of 50%;
- 6) placement of a note on the Final Plat stating that the site is limited to one curb cut to East Drive for the proposed street, with the size, location and design to be approved by Traffic Engineering and conform to AASHTO standards;
- 7) placement of a note on the Final Plat stating that Lots 10 through 15 are denied direct access to Center Drive;
- 8) placement of a note on the Final Plat stating that all lots and the Common Areas are limited to one curb cut each, with the size, location and design to be approved by Traffic Engineering and conform to AASHTO standards;
- 9) placement of a note on the Final Plat stating that the Common Areas are denied direct access to East Drive;
- 10) revision of the Final Plat to label and dimension the minimum building setback line along the fronts of Lots 10 through 15;

- 11) retention of the minimum building setback line along all street frontages on the Final Plat;
- 12) revision of the Final Plat to illustrate, label and dimension any reduced side and rear setback lines;
- 13) revision of the Final Plat to adjust the rear setback line for Lots 16 through 24 to be out of the 10' drainage easement;
- 14) removal of all existing structures on the site after obtaining all necessary demolition permits prior to signing the Final Plat;
- 15) placement of a note on the Final Plat stating that no structure may be constructed or placed within any easement;
- 16) placement of a note on the Final Plat stating that the maintenance of the Common Areas is the responsibility of the property owners;
- 17) compliance with the Engineering comments: *[FINAL PLAT COMMENTS (should be addressed prior to submitting the FINAL PLAT for review and/or signature by the City Engineer): A. Provide all of the required information on the SUBDIVISION PLAT (i.e. signature blocks, signatures, certification statements, written legal description, required notes, legend, scale, bearings and distances) that is required by the current Alabama State Board of Licensure for Professional Engineers and Land Surveyors. B. Show and label the POB. C. Provide and label the monument set or found at each subdivision corner. D. Label the proposed ROW with a name. E. Label the proposed ROW as either PRIVATE or PUBLIC. F. Dedicate the corner radius (25' minimum or as approved by the City Engineer and Traffic Engineer) at the east end of the UNNAMED PROPOSED ROW. G. Designate each Common Area with an individual designator (i.e. #1, A) H. Provide the Surveyor's, Owner's (notarized), Planning Commission, and Traffic Engineering signatures. I. Add a note to the SUBDIVISION PLAT stating that a Land Disturbance permit will be required for any land disturbing activity in accordance with Mobile City Code, Chapter 17, Storm Water Management and Flood Control; the City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control. J. Add a note to the Plat stating that the approval of all applicable federal, state, and local agencies (including all storm water runoff, wetland and floodplain requirements) will be required prior to the issuance of a Land Disturbance permit. K. Add a note that sidewalk is required to be constructed, and/or repaired, along the frontage of each lot, or parcel, at time of new development or construction, unless a sidewalk waiver is approved. L. Add a note to the plat stating that all proposed detention facilities, common areas, and wetlands shall be the responsibility of the Property Owner(s), and not the responsibility of the City of Mobile. M. Add a note to the plat stating that all easements shall remain in effect until vacated through the proper Vacation process. N. Provide a copy of the FINAL SUBDIVISION PLAT to the Engineering Dept. for review. No signatures are required on this drawing. O. After addressing all of the FINAL SUBDIVISION PLAT review comments by the Engineering Dept. provide the red-line markup, the ORIGINAL (with all other signatures) and one (1) copy of the revised original (signatures not required) of the revised Final Plat to the Engineering Department.];*
- 18) placement of a note on the Final Plat stating the Traffic Engineering comments: *[Each lot is limited to one curb cut to the proposed right-of-way. The common areas are denied any direct access from East Drive. If the proposed roadway is public, corner radii*

dedication should be included in the common area lots at the intersection with East Drive. Consideration should be given to the building layouts relative to the building layouts across the street (within the development). Backing out of driveways becomes very problematic if on street parking occurs opposite driveways. Parking restrictions may have to be implemented by the City (if public ROW) if access for public services is hindered (ex. garbage, fire access).];

- 19) compliance with the Urban Forestry comments: *[Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 2015-116 and City Code Chapters 57 and 64). Private removal of trees in the right-of-way will require approval of the Mobile Tree Commission. Removal of heritage trees from a commercial site will require a tree removal permit.];*
- 20) compliance with the Fire Department comments: *[All projects within the City Limits of Mobile shall comply with the requirements of the City of Mobile Fire Code Ordinance. (2012 International Fire Code)];* and
- 21) submittal to and approval by Planning and Zoning of a revised PUD site plan, prior to signing the Final Plat.

Planned Unit Development: Staff recommends the following Findings of Fact for Approval of the Planned Unit Development:

- a. the proposal promotes the objective of Creative design (to encourage innovative and diversified design in building form and site development), because it allows for the configuration of a site to meet housing needs;
- b. the proposal promotes the objective of Efficient land use (to encourage the most efficient and sustainable use of land, especially tracts in the inner part of the city that remain undeveloped or that are appropriate for redevelopment), because of utilizing infill development practices;
- c. the proposal promotes the objective of Environment (to preserve and protect as urban amenities the natural features and characteristics of the land), because the project does not eliminate any existing urban amenities or natural features, and the existing urban fabric is retained;
- d. the proposal promotes the objective of Public services (to encourage optimum use of available public utilities, streets and community facilities), because no new public infrastructure must be constructed to develop the site as planned.

Based upon the preceding, the application is recommended for Approval, subject to the following:

- 1) verification of the right-of-way width of Center Drive and illustration of any dedication of right-of-way sufficient to provide 25' from the centerline of Center Drive if currently less than 25';
- 2) revision of the site plan to provide a 25' corner radius on the North and South sides of the proposed street at the intersection with East Drive;
- 3) retention of the lot sizes in both square feet and acres on the site plan, adjusted for any required dedication;

- 4) revision of the size of the Common Areas after any required corner radius dedications;
- 5) placement of a note on the site plan stating that all lots are limited to a maximum site coverage of 50%;
- 6) placement of a note on the site plan stating that the site is limited to one curb cut to East Drive for the new street, with the size, location and design to be approved by Traffic Engineering and conform to AASHTO standards;
- 7) placement of a note on the site plan stating that Lots 10 through 15 are denied direct access to Center Drive;
- 8) placement of a note on the site plan stating that all lots and the Common Areas are limited to one curb cut each, with the size, location and design to be approved by Traffic Engineering and conform to AASHTO standards;
- 9) placement of a note on the site plan stating that the Common Areas are denied direct access to East Drive;
- 10) revision of the site plan to label and dimension the minimum building setback line along the fronts of Lots 10 through 15;
- 11) retention of the minimum building setback line along all street frontages on the site plan;
- 12) revision of the site plan to illustrate, label and dimension any reduced side and rear setback lines;
- 13) revision of the site plan for Lot 20 to remove the 5' minimum building setback line along the rear from within the drainage easement and relocate it to coincide with the 10' easement line;
- 14) revision of the site plan to adjust the rear setback line for Lots 16 through 24 to be out of the 10' drainage easement;
- 15) placement of a note on the site plan stating that no structure may be constructed or placed within any easement;
- 16) placement of a note on the site plan stating that the maintenance of the Common Areas is the responsibility of the property owners;
- 17) revision of the site plan to provide a public sidewalk along all street frontages, or the approval of a Sidewalk Waiver;
- 18) compliance with the Engineering comments: *[ADD THE FOLLOWING NOTES TO THE PUD SITE PLAN: 1. Any work performed in the existing ROW (right-of-way) such as driveways, sidewalks, utility connections, grading, drainage, irrigation, or landscaping will require a ROW permit from the City of Mobile Engineering Department (208-6070) and must comply with the City of Mobile Right-of-Way Construction and Administration Ordinance (Mobile City Code, Chapter 57, Article VIII). 2. A Land Disturbance Permit application shall be submitted for any proposed land disturbing activity with the property. A complete set of construction plans including, but not limited to, drainage, utilities, grading, storm water detention systems and paving will need to be included with the Land Disturbance permit. This Permit must be submitted, approved, and issued prior to beginning any of the construction work. 3. Any and all proposed land disturbing activity within the property will need to be submitted for review and be in conformance with Mobile City Code, Chapter 17, Storm Water Management and Flood Control); the City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control. 4. Any existing or proposed detention facility shall be maintained as it was constructed and approved. The Land Disturbance Permit application for any proposed construction includes a*

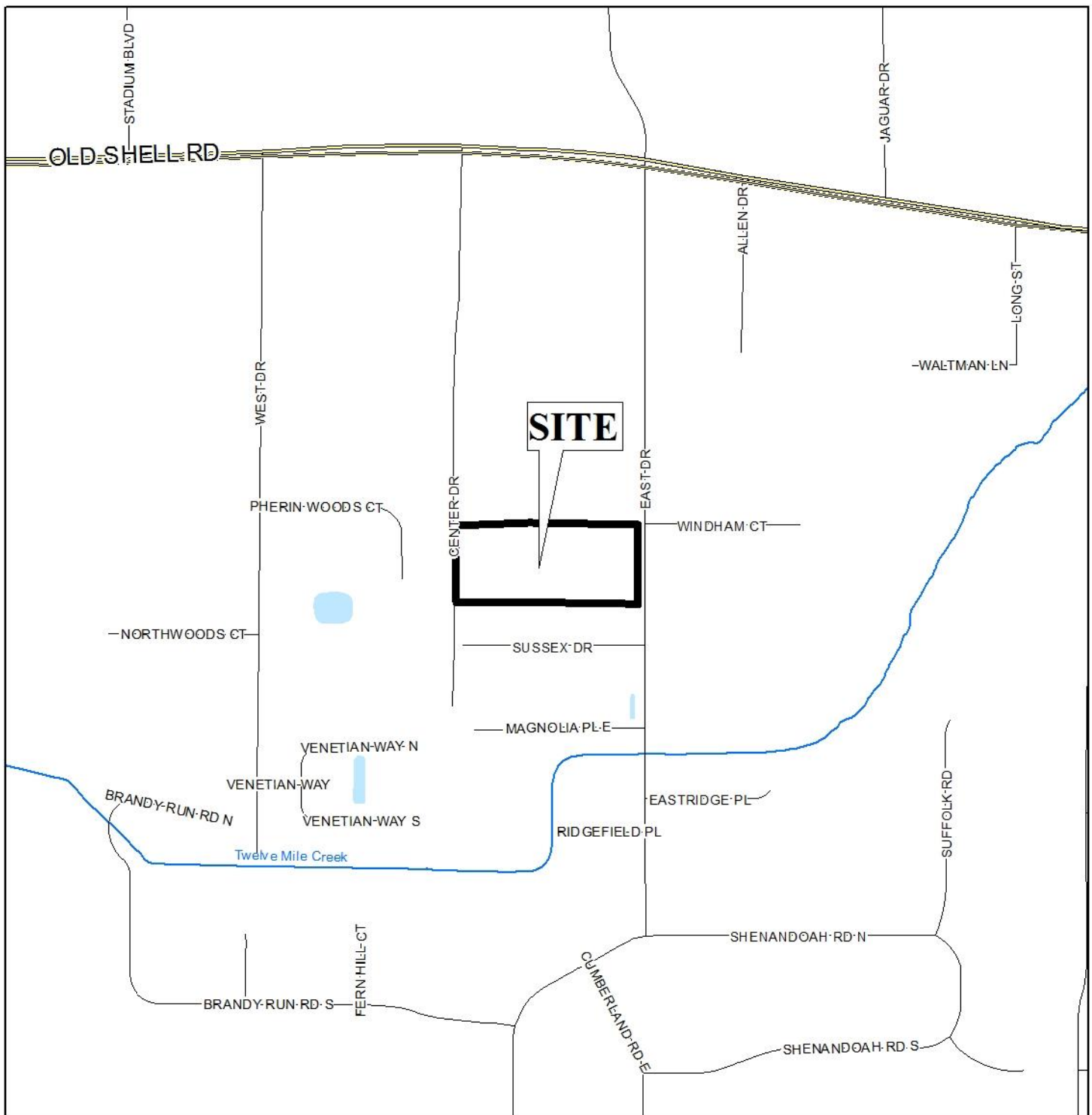
requirement of a Maintenance and Inspection Plan (signed and notarized by the Owner) for the detention facility. This Plan shall run with the land and be recorded in the County Probate Office prior to the Engineering Department issuing their approval for a Final Certificate of Occupancy. 5. The approval of all applicable federal, state, and local agencies (including all storm water runoff, wetland and floodplain requirements) will be required prior to the issuance of a Land Disturbance permit. The Owner/Developer is responsible for acquiring all of the necessary permits and approvals. 6. The proposed development must comply with all Engineering Department design requirements and Policy Letters.];

- 19) *placement of a note on the site plan stating the Traffic Engineering comments: [Each lot is limited to one curb cut to the proposed right-of-way. The common areas are denied any direct access from East Drive. If the proposed roadway is public, corner radii dedication should be included in the common area lots at the intersection with East Drive. Consideration should be given to the building layouts relative to the building layouts across the street (within the development). Backing out of driveways becomes very problematic if on street parking occurs opposite driveways. Parking restrictions may have to be implemented by the City (if public ROW) if access for public services is hindered (ex. garbage, fire access).];*
- 20) *compliance with the Urban Forestry comments: [Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 2015-116 and City Code Chapters 57 and 64). Private removal of trees in the right-of-way will require approval of the Mobile Tree Commission. Removal of heritage trees from a commercial site will require a tree removal permit.];*
- 21) *compliance with the Fire Department comments: [All projects within the City Limits of Mobile shall comply with the requirements of the City of Mobile Fire Code Ordinance. (2012 International Fire Code)]; and*
- 22) *submittal to and approval by Planning and Zoning of a revised PUD site plan, prior to signing the Final Plat for the Subdivision.*

Rezoning: The rezoning request is recommended for Approval, subject to the following conditions:

- 1) Completion of the Subdivision process; and
- 2) Full compliance with all municipal codes and ordinances.

LOCATOR MAP



APPLICATION NUMBER 8 DATE November 15, 2018
 APPLICANT Villas at Spring Hill Subdivision
 REQUEST Subdivision, Planned Unit Development, Rezoning from R-1 to R-2



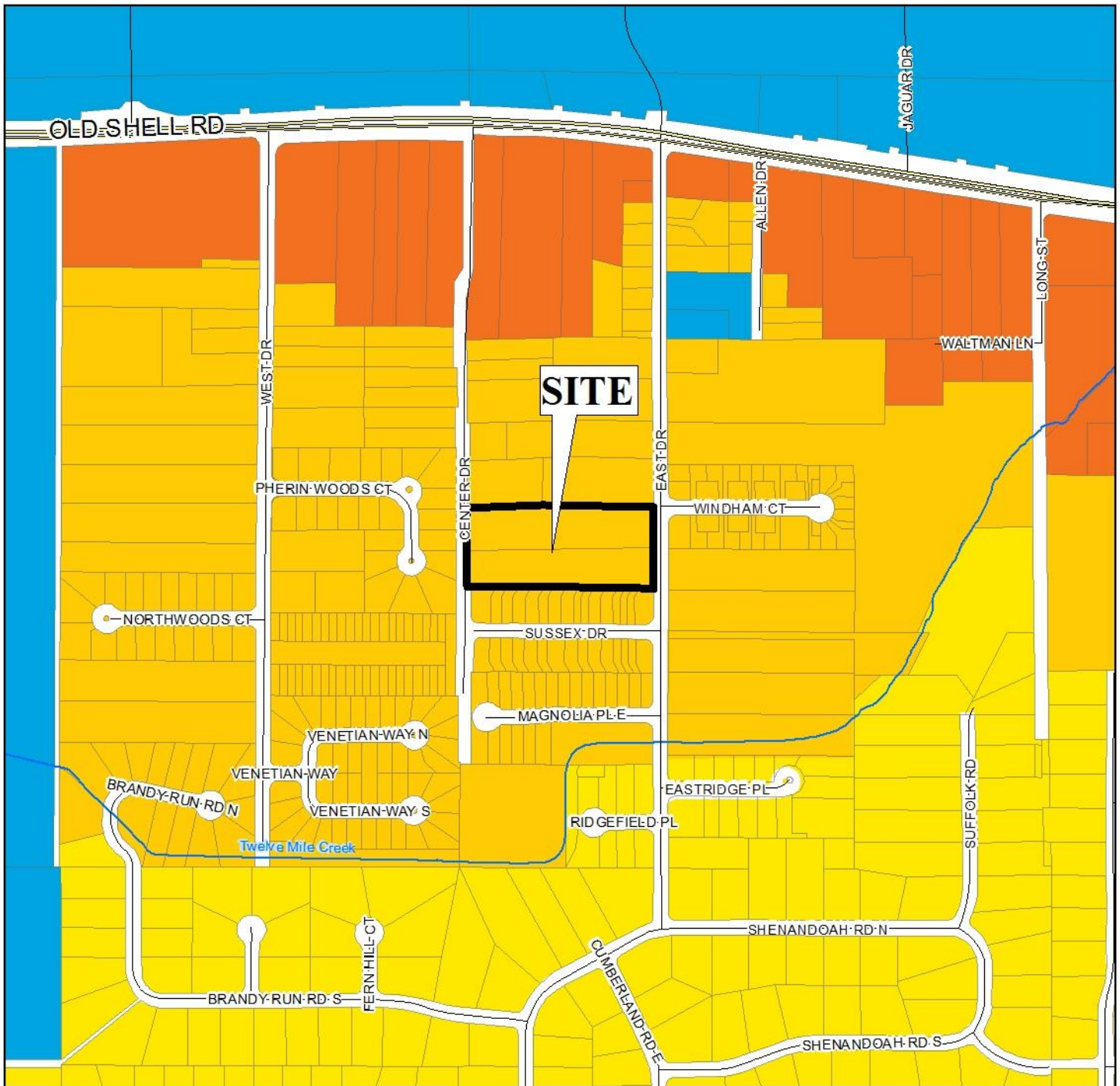
LOCATOR ZONING MAP



APPLICATION NUMBER 8 DATE November 15, 2018
 APPLICANT Villas at Spring Hill Subdivision
 REQUEST Subdivision, Planned Unit Development, Rezoning from R-1 to R-2



FLUM LOCATOR MAP



APPLICATION NUMBER 8 DATE November 15, 2018

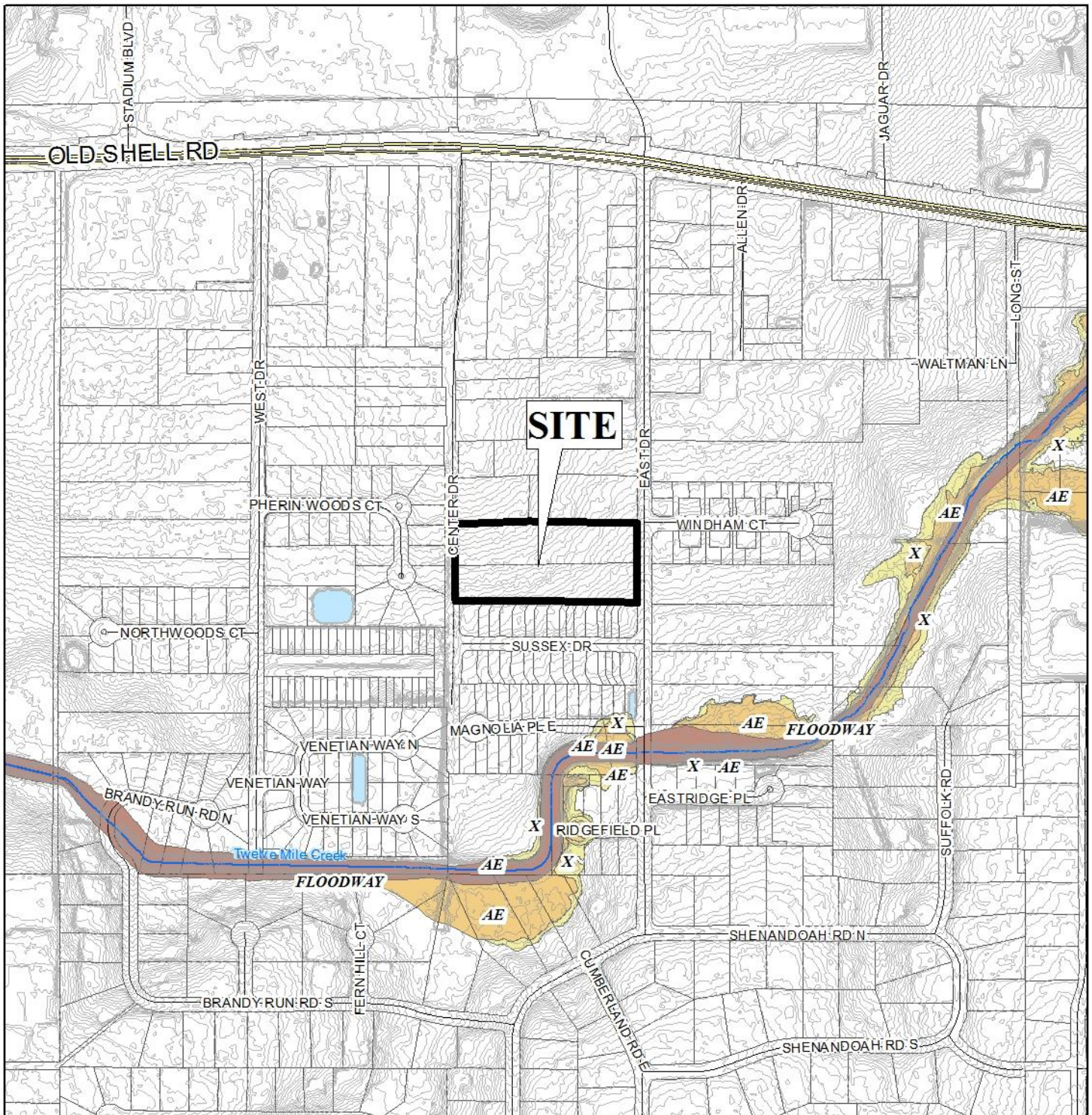
APPLICANT Villas at Spring Hill Subdivision

REQUEST Subdivision, Planned Unit Development, Rezoning from R-1 to R-2

Low Density Residential	Neighborhood Center - Traditional	Downtown Waterfront	Parks & Open Space
Mixed Density Residential	Neighborhood Center - Suburban	Light Industry	Water Dependent
Downtown	Traditional Corridor	Heavy Industry	
District Center	Mixed Commercial Corridor	Institutional	



ENVIRONMENTAL LOCATOR MAP



APPLICATION NUMBER 8 DATE November 15, 2018
APPLICANT Villas at Spring Hill Subdivision
REQUEST Subdivision, Planned Unit Development, Rezoning from R-1 to R-2



PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The site is surrounded by residential units.

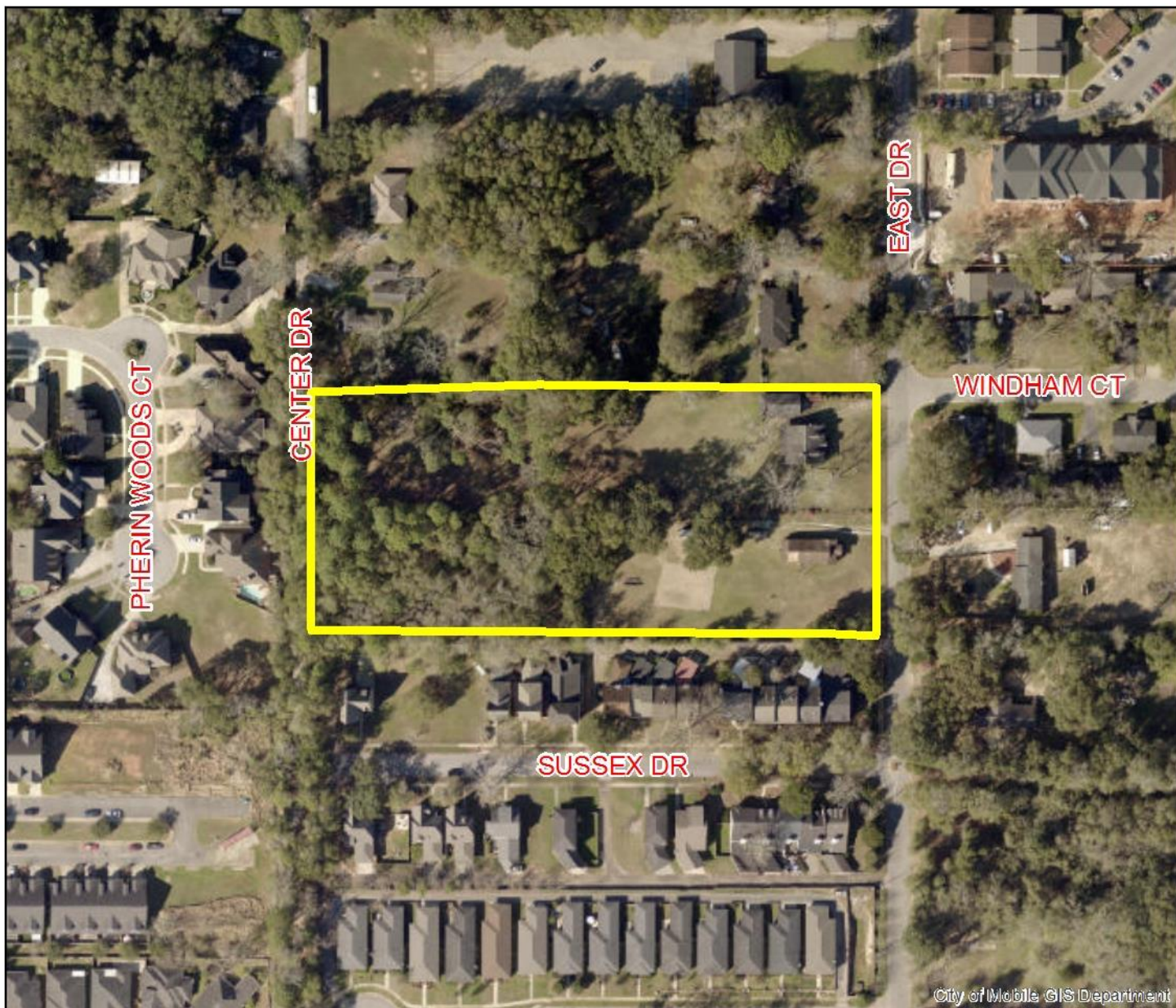
APPLICATION NUMBER 8 DATE November 15, 2018
 APPLICANT Villas at Spring Hill Subdivision
 REQUEST Subdivision, Planned Unit Development, Rezoning from R-1 to R-2

R-A	R-3	T-B	B-2	B-5	MUN	SD-WH	T5.1
R-1	R-B	B-1	B-3	I-1	OPEN	T3	T5.2
R-2	H-B	LB-2	B-4	I-2	SD	T4	T6



PLANNING COMMISSION

VICINITY MAP - EXISTING AERIAL

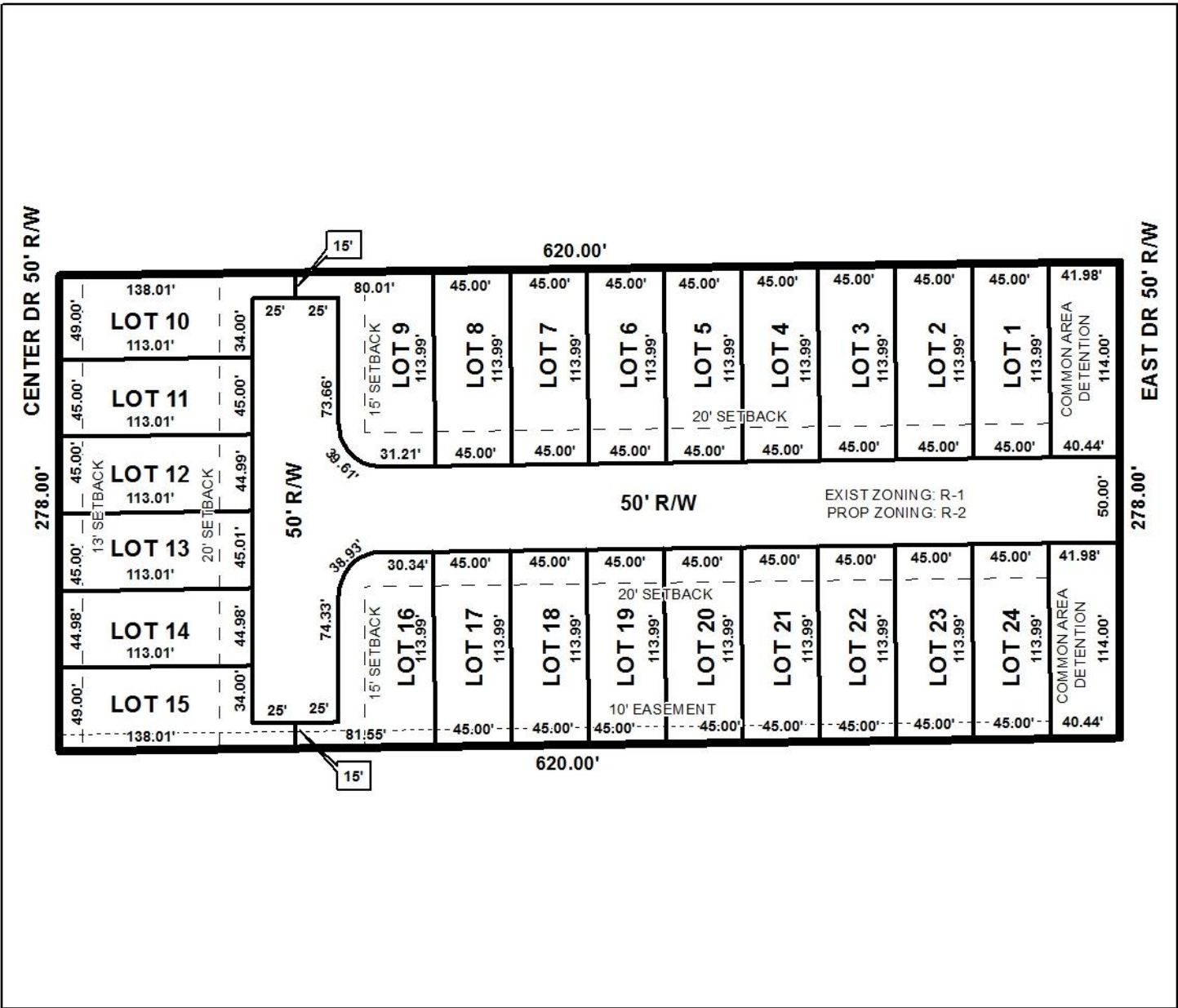


The site is surrounded by residential units.


APPLICATION NUMBER 8 DATE November 15, 2018
 APPLICANT Villas at Spring Hill Subdivision
 REQUEST Subdivision, Planned Unit Development, Rezoning from R-1 to R-2



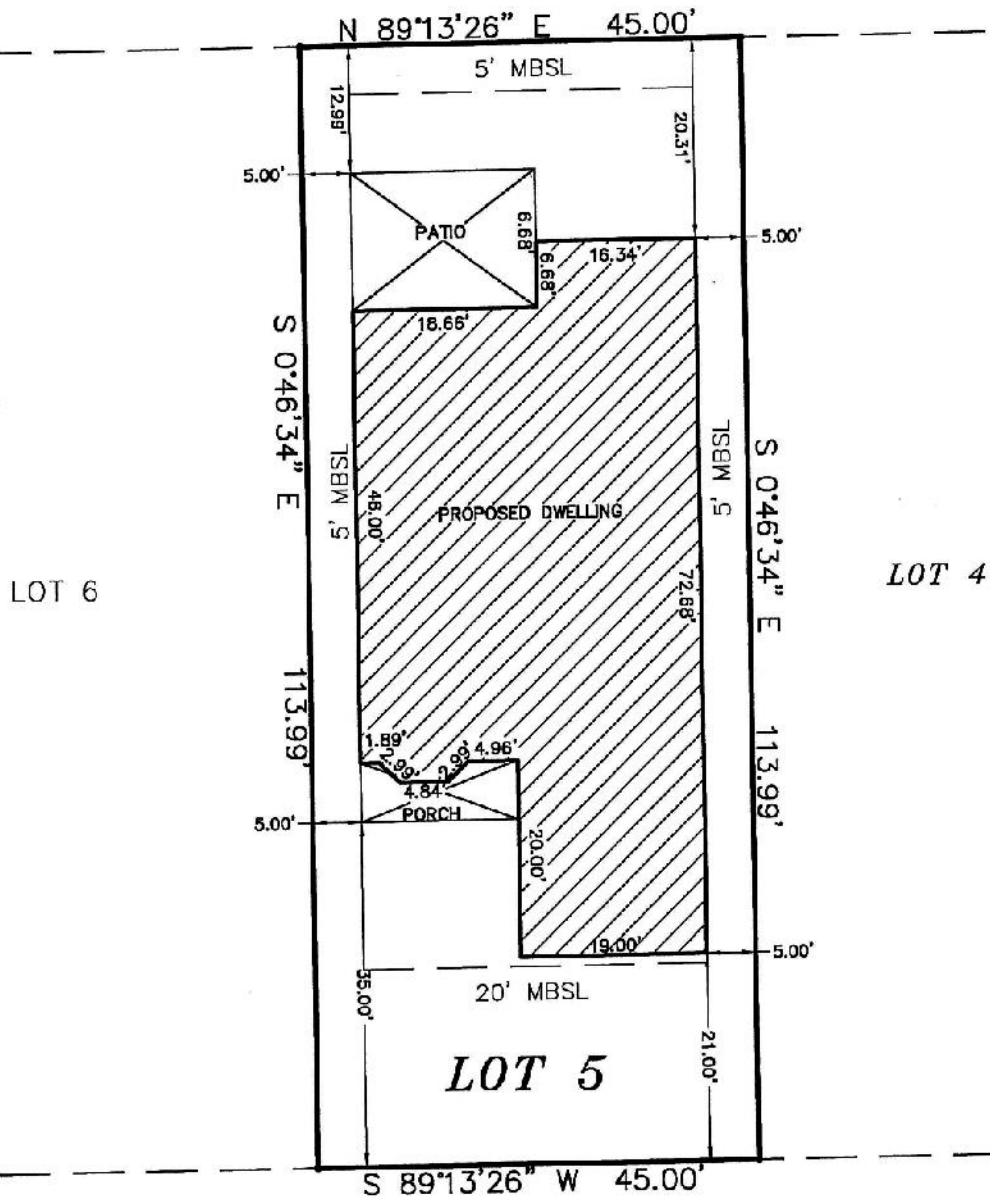
SITE PLAN



The site plan illustrates the proposed lots, setbacks, and easement.

APPLICATION NUMBER <u>8</u> DATE <u>November 15, 2018</u>	
APPLICANT <u>Villas at Spring Hill Subdivision</u>	
REQUEST <u>Subdivision, Planned Unit Development, Rezoning from R-1 to R-2</u>	

DETAIL SITE PLAN



(50' RIGHT-OF-WAY)

APPLICATION NUMBER 8 DATE November 15, 2018

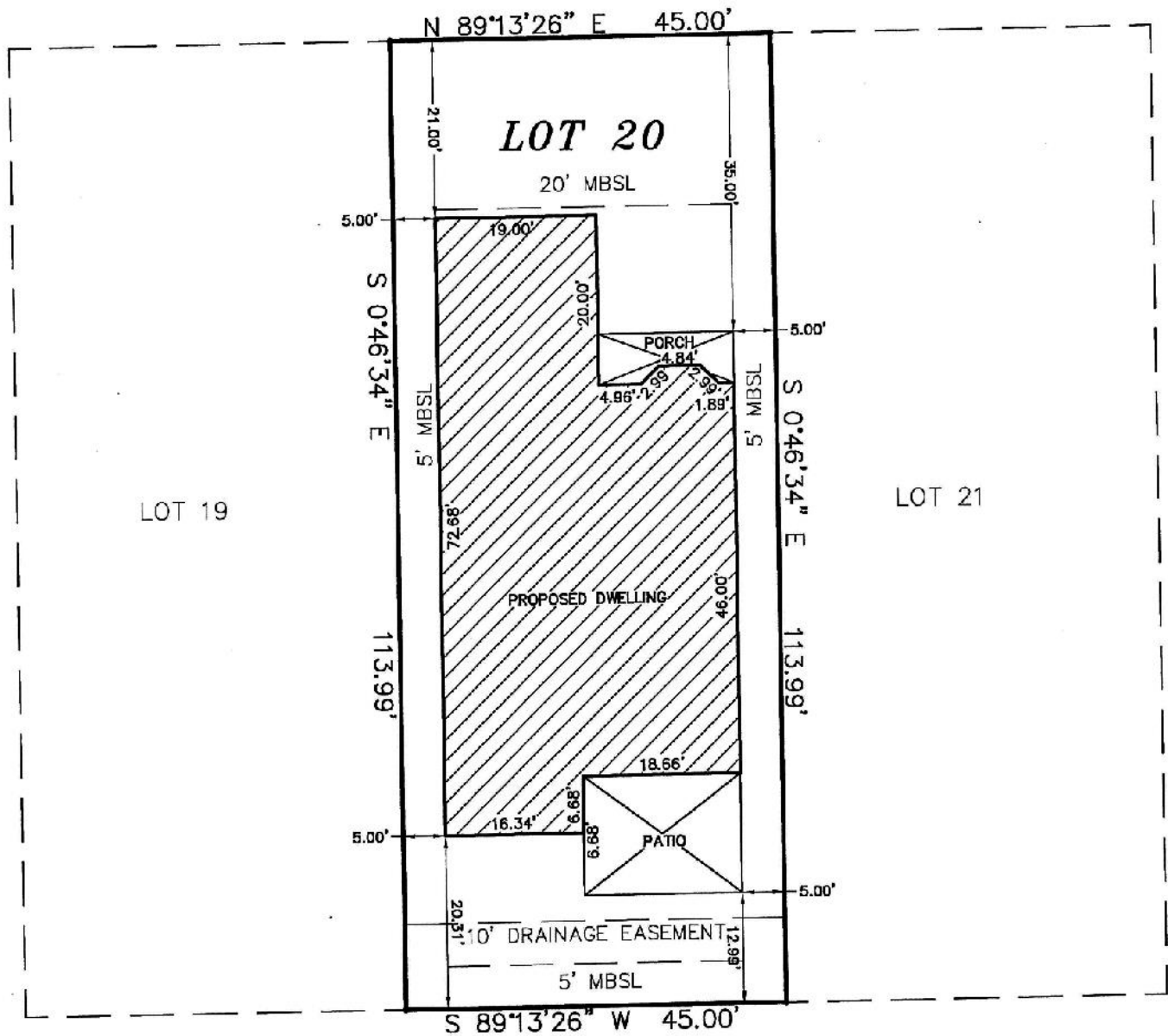
APPLICANT Villas at Spring Hill Subdivision

REQUEST Subdivision, Planned Unit Development, Rezoning from R-1 to R-2



DETAIL SITE PLAN

(50' RIGHT-OF-WAY)



APPLICATION NUMBER 8 DATE November 15, 2018
 APPLICANT Villas at Spring Hill Subdivision
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