

**PLANNED UNIT DEVELOPMENT
STAFF REPORT****Date: October 7, 2021**

<u>DEVELOPMENT NAME</u>	Hunter Development
<u>LOCATION</u>	6480, 6485, 6490, 6500, and 6510 Dockside Drive (West terminus of Dockside Drive, extending to the South side of Dockside Drive, 330'± East of its West terminus.)
<u>PRESENT ZONING</u>	I-2, Heavy Industry District
<u>CITY COUNCIL DISTRICT</u>	District 4
<u>AREA OF PROPERTY</u>	4 Lots / 8.41± Acres
<u>CONTEMPLATED USE</u>	Planned Unit Development Approval to allow shared access between multiple building sites.
<u>TIME SCHEDULE FOR DEVELOPMENT</u>	Within one (1) year of approval.

ENGINEERING**COMMENTS**

1. Add the following note to the PUD site plan – “A 25' riparian buffer may be required, during development, along the edge of anything considered by ADEM to be a water of the state.”

2. Retain NOTES #1 - #6, as shown on the submitted PUD drawing dated 9-2-21.

TRAFFIC ENGINEERING**COMMENTS**

Lot is limited to one curb cut with any changes in size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards. Any required on-site parking, including ADA handicap spaces, shall meet the minimum standards as defined in Section 64-6 of the City's Zoning Ordinance.

URBAN FORESTRY**COMMENTS**

Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties [Act 929 of the 1961 Regular Session of the Alabama Legislature (Acts 1961, p. 1487), as amended, and City Code Chapters 57 and 65]. Private removal of trees in the right-of-way will require approval of the Mobile Tree Commission. Removal of heritage trees from undeveloped residential sites, developed residential sites in historic districts, and all commercial sites will require a tree removal permit.

FIRE DEPARTMENT**COMMENTS**

All projects within the City Limits of Mobile shall comply with the requirements of the City of Mobile Fire Code Ordinance (2012 International Fire Code). Fire apparatus access is required to be within 150' of all commercial and residential buildings. A fire hydrant is required to be within 400' of non-sprinkled commercial buildings and 600' of sprinkled commercial buildings.

REMARKS

The applicant is requesting Planned Unit Development Approval to allow shared access between multiple building sites.

PUD approval is site plan-specific, thus any changes to the site plan will require Administrative PUD approval. Also, PUD approvals expire within one (1) year if no permits for the development are obtained.

The site has been given a Heavy Industry (HI) land use designation within the Future Land Use Plan and Map, adopted on May 18, 2017 by the Planning Commission. The Future Land Use Plan and Map complements and provides additional detail to the Development Framework Maps in the Map for Mobile, adopted by the Planning Commission at its November 5, 2015 meeting.

The HI designation applies to larger parcels primarily devoted to high-impact industrial activity which is preferably removed from residential and commercial uses. Light industry, industrial business or heavy commercial lands may separate heavy industry from other land uses.

Heavy industrial areas include collection, treatment, and manufacturing processes which use raw materials, are distinguished by the presence of noise, vibration, and/or odors, and benefit from easy access to a multimodal freight transportation network. Certain types of heavy industry are characterized by low building coverage and activities that rely on large areas of outdoor storage of raw material stockpiles and/or waste-product disposal areas, storage tanks, pipelines, and transportation yards to handle the transfer of heavy materials. The outdoor storage areas should be screened as much as possibly by the nature of the stored materials.

Land designated as HI may be underdeveloped due to the presence of wetlands on portions of the parcel. In these cases, the wetlands may serve to buffer surrounding uses from the potential impacts of the heavy industrial use. Underdeveloped areas of HI parcels that have tree cover may be used as buffering between the heavy industrial use and other uses. Where buffers do not exist naturally, they should be provided as spelled out in the zoning and subdivision regulations. Open area reserved for dredge disposal are also designated as HI and may contain wetlands.

In Mobile, port terminal facilities, docks, shipyards, drydocks, etc., are mostly owned by the State of Alabama. Although not subject to local zoning, these facilities are shown as heavy industrial uses in the FLUM.

It should be noted that the Future Land Use Plan and Map components of the Map for Mobile Plan are meant to serve as a general guide, not a detailed lot and district plan. In many cases the designation on the new Future Land Use Map may match the existing use of land, but in others the designated land use may differ from what is on the ground today. As such, the Future Land

Use Plan and Map allows the Planning Commission and City Council to consider individual cases based on additional information such as the classification request, the surrounding development, the timing of the request, and the appropriateness and compatibility of the proposed use and, where applicable, the zoning classification.

Planned Unit Development (PUD) review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating excess traffic along minor residential streets in residential districts outside the PUD; and, that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties, as well as provide protection of adjacent properties from adverse effects from the PUD.

In pursuit of these purposes, the objectives to be met by a PUD are as follows: a) to encourage innovative and diversified design in building form and site development; b) flexibility, to permit greater flexibility in the location and arrangement of buildings and uses than is generally possible under district regulations; c) to encourage the most efficient and sustainable use of land, especially tracts in the inner part of the city that remain undeveloped or that are appropriate for re-development; d) to preserve and protect as urban amenities the natural features and characteristics of land; e) to encourage the provision of common open space through efficient site design; and, f) to encourage optimum use of available public utilities, streets and community facilities.

The site was originally part of the 12-lot, Rangeline Hamilton Commercial park Subdivision, approved in 2004 when the site was within the Planning Jurisdiction. The area was annexed into the City in 2009 with an I-1, Light Industry District, zoning designation. Rezoning of the property to I-2, Heavy Industry District, was adopted by City Council at its February 4, 2014 meeting. Most recently, at its October 1, 2020 meeting, the Planning Commission approved resubdivision of two (2) of the 12 lots into a single legal lot, Lot A of the Resubdivision of Lots 6 & 7 of Rangeline Hamilton Commercial Park.

If approved, the PUD will overlay four (4) lots: Lot A of the Resubdivision of Lots 6 & 7 of Rangeline Hamilton Commercial Park Subdivision; and Lots 3, 4, and 5, of the original Rangeline Hamilton Commercial Park Subdivision.

Lot A is proposed to be developed with a 6,000 square-foot building for use as an office and warehouse. The remaining lots are proposed to be developed with a laydown yard with aggregate surfacing for storage of materials associated with the warehouse. The specific use of the office and warehouse building is not provided, and it is important to note that rezoning of the site to I-2 in 2014 was contingent upon the prohibition of the following uses: 1) Abrasive manufacture; 2) Aircraft and aircraft engines manufacture; 3) Aircraft parts manufacture; 4) Animal shelter; 5) Asphalt products manufacture; 6) Battery manufacture; 7) Boiler or tank works; 8) Brick and clay products manufacture; 9) Carpet manufacture (from purchased thread, yarn, jute, etc.); 10) Celluloid manufacture; 11) Cement manufacture; 12) Concrete and concrete products manufacture; 13) Corn sugars and starches manufacture; 14) Cotton ginning and bailing; 15)

Foundry (casting); 16) Glass manufacture (from raw materials); 17) Graphite manufacture; 18) Guided missiles, space vehicles, and propulsion unit manufacture; 19) Industrial engine and turbine manufacture; 20) Insulation manufacture or fabrication; 21) Junk yard; 22) Leather tanning and fishing; 23) Motor vehicles manufacture; 24) Oils and fats manufacture (animal and vegetable); 25) Polishes (furniture, shoe, automobile) manufacture; 26) Sawmill, planning mill; 27) Shipbuilding and repairing; 28) Soap, detergents and disinfectants manufacture; 29) Syrup manufacture. As such, if the site is proposed to be developed for any of the prohibited uses, rezoning of the property to amend the conditions of the previous rezoning approval will be required.

Aggregate surfacing is an acceptable surfacing material for the laydown yard in the I-2 zoning district, but adequate care should be taken to ensure aggregate material does not extend into the right-of-way.

The site plan illustrates the proposed 6,000 square-foot office/commercial building. A table on the plan depicts that four (4) spaces are required to accommodate 1,200 square feet of office space, and one (1) space is required to accommodate three (3) warehouse employees; ten (10) parking spaces are provided, one (1) of which is depicted as van accessible in compliance with IBC regulations. While an adequate amount of parking is illustrated on the site plan, staff cannot determine if the parking stalls meet the minimum size requirements of the Zoning Ordinance. As such, the plan should be revised to depict that each of the parking stalls meets the minimum size requirements of Section 64-6.A.2. of the Zoning Ordinance.

Two-way circulation is presumably proposed for on-site traffic, but staff cannot determine if the drive aisles are equipped with adequate widths for such circulation (24' being the minimum width requirement for two-way traffic). Therefore, the plan should be revised to depict that the widths of the drive aisles will comply with Section 64-6.A.2. of the Zoning Ordinance.

Two-way access to/from the site also appears to be proposed along Dockside Drive, providing shared access between Lot 5 of Rangeline Hamilton Commercial Park Subdivision and Lot A of the resubdivision. Again, the site plan does not provide enough detail to illustrate if the proposed driveway is wide enough to accommodate two-way access; thus, the plan should be revised to depict that the width of the driveway will comply with Section 64-6.A.2. of the Zoning Ordinance.

No additional access to/from the public right-of-way is proposed at this time, with cross-access between Lots 3 and 4 of Rangeline Hamilton Commercial Park Subdivision facilitated by the proposed laydown yard.

With respect to access management, a note should be placed on a revised PUD site plan stating Traffic Engineering comments.

An additional building is illustrated on the site plan and is labeled as future development; however, it is unclear if the parking proposed as part of this request will also satisfy the minimum amounts of off-street parking required for the future building. The plan should

therefore be revised to clarify this information, otherwise revision of the PUD may be required at the time of future development to account for additional parking.

Due to the limited amount of parking, a photometric plan depicting parking lot illumination is not required at this time; however, any site lighting provided should comply with the illumination standards of Section 64-6.A.8. of the Zoning Ordinance, and should be directed in such a way as to not directly illuminate neighboring properties or any rights-of-way.

No off-street loading facility is depicted on the site plan. Section 64-6.B.5. of the Zoning Ordinance requires one small off-street loading facility for buildings with a gross floor area not exceeding 12,500 square feet. As such, the PUD site plan should be revised to depict at least one off-street loading facility, the dimensions of which should be a minimum of 10' in width, 20' in length, and have a minimum overhead clearance of 10' (See Section 64-6.B.2.).

Landscaping calculations for the site are provided on the site plan and exceed the minimum landscape area requirements of Section 64-4.E.3.a.(1). of the Zoning Ordinance. No tree plantings are illustrated, but are not required to be depicted on the PUD site plan. A note on the site plan states "Complete compliance with Section 64-4.E. of the Zoning Ordinance regarding tree and landscaping will be shown on the Engineering site plan for construction." This note should be retained on the revised site plan, and a compliant tree planting/landscape area plan should be submitted for concurrent review of the applicable Building and Engineering Permits. Required frontage tree plantings should be coordinated with staff to insure the trees have sufficient room to grow and thrive. It should be noted that perimeter tree plantings are not required for sites in the I-2 zoning district.

A dumpster pad is not illustrated on the site plan, and no note is provided on the site plan stating curbside trash service will be utilized. As such, a note should be placed on the revised site plan stating that any dumpster pad constructed on the property shall meet the enclosure and sanitary sewer connection requirements of Section 64-4.D.9. of the Zoning Ordinance. Such compliance shall be illustrated on the site plans required for review of the applicable Building and Engineering Permits. If curbside waste removal is instead proposed to be utilized, such information should be revised on the applicable site plans accordingly.

No sidewalk is illustrated along Dockside Drive, and no Sidewalk Waiver has been approved by the Planning Commission. As such, the plan should be revised to illustrate a compliant sidewalk; or, the applicant may submit a Sidewalk Waiver for review by the Planning Commission.

The site abuts residentially zoned property to the West, thus requiring a protection buffer in the form of a wall, wooden privacy fence, or the inclusion of a screen planting strip in compliance with Section 64-4.D.1. of the Zoning Ordinance. The applicant as part of their PUD request is also requesting a waiver of the protection buffer requirement, citing as justification that the area west of their property is undevelopable due to the presence of a creek and wetlands. In a recent case, the Commission waived the buffer requirement until such time the adjoining residentially zoned property was developed. Based on the creek, wetlands, large area of property and surrounding development, a similar condition would be appropriate here.

A 15'-wide drainage easement is illustrated toward the rear of Lots 3, 4, and 5 of Rangeline Hamilton Commercial Park Subdivision. As such, a note should be placed on the revised PUD site plan stating no structures shall be constructed or placed within any easement without permission of the easement holder.

No signage is depicted on the site plan, and no information regarding signage was provided to staff for review. All proposed signage for the site requires separate reviews by, and permits from, the Planning and Zoning Department. Illuminated signage also requires its design and construction to be completed by a licensed and bonded sign contractor, along with additional Electric Permits from the Permitting Department.

Finally, a copy of the revised PUD site plan should be submitted to, and approved by, the Planning and Zoning Department prior to any requests for Building or Land Disturbance permits.

RECOMMENDATION

Staff recommends the following Findings of Fact for Approval of the Planned Unit Development:

- a. the proposal promotes the objective of Creative design (to encourage innovative and diversified design in building form and site development), because greater flexibility is required due to the scope and scale of the site;
- b. the proposal promotes the objective of Flexibility (to permit greater flexibility in the location and arrangement of buildings and uses than is generally possible under district regulations), by allowing cross-access between multiple building sites;
- c. the proposal promotes the objective of Efficient land use (to encourage the most efficient and sustainable use of land, especially tracts in the inner part of the city that remain undeveloped or that are appropriate for redevelopment), because the applicant is endeavoring to maintain the economic viability of an industrial corridor of the city;
- d. the proposal promotes the objective of Environment (to preserve and protect as urban amenities the natural features and characteristics of the land), because the development is limited to only a portion of the site thus maintaining a majority of existing environmental conditions while also providing additional landscape area and tree plantings;
- e. the proposal promotes the objective of Open space (to encourage the provision of common open space through efficient site design), because additional greenspace will be preserved within the development; and,
- f. the proposal promotes the objective of Public services (to encourage optimum use of available public utilities, streets and community facilities), because the development will utilize existing public infrastructure.

The Approval should be subject to the following conditions:

- 1) Compliance with the use restrictions of the rezoning of the site to I-2, Heavy Industry District;
- 2) Revision of the site plan to depict that the driveway, drive aisles, and off-street parking spaces will comply with the design standards of Section 64-6.A.2. of the Zoning Ordinance;

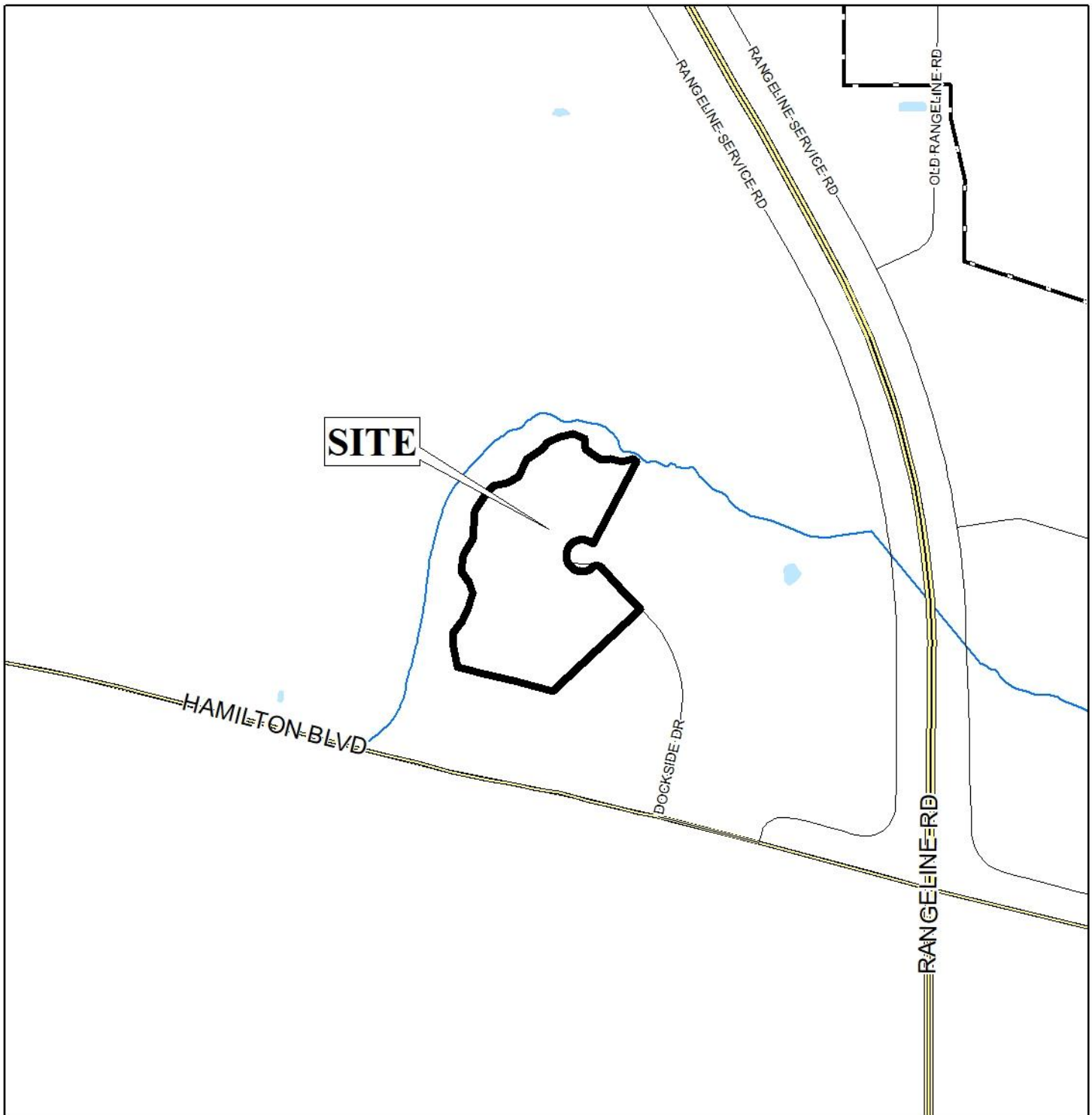
- 3) Revision of the site plan to illustrate an off-street loading facility in compliance with Sections 64-6.B.2. and 64-6.B.5. of the Zoning Ordinance;
- 4) Retention of a note on the site plan stating the site will comply with the tree planting and landscape area requirements of Section 64-4.E. of the Zoning Ordinance, and provision of a tree planting and landscape area plan in compliance with these requirements at the time of permitting;
- 5) Placement of a note on the site plan stating that any dumpster pad constructed on the property shall meet the enclosure and sanitary sewer connection requirements of Section 64-4.D.9. of the Zoning Ordinance, or provision of a note on the site plan stating curbside waste removal service will be utilized;
- 6) Revision of the site plan to illustrate a compliant sidewalk along Dockside Drive, or submission of a Sidewalk Waiver application for review by the Planning Commission;
- 7) Placement of a note on the site plan stating that a protection buffer will be required where the site adjoins residentially zoned property at such time the adjoining property is developed for residential use;
- 8) Placement of a note on the site plan stating no structures shall be constructed or placed within any easement without permission of the easement holder;
- 9) Compliance with Engineering comments: *(1. Add the following note to the PUD site plan – “A 25' riparian buffer may be required, during development, along the edge of anything considered by ADEM to be a water of the state.” 2. Retain NOTES #1 - #6, as shown on the submitted PUD drawing dated 9-2-21.);*
- 10) Placement of a note on the site plan stating Traffic Engineering comments: *(Lot is limited to one curb cut with any changes in size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards. Any required on-site parking, including ADA handicap spaces, shall meet the minimum standards as defined in Section 64-6 of the City's Zoning Ordinance.);*
- 11) Compliance with Urban Forestry comments: *(Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties [Act 929 of the 1961 Regular Session of the Alabama Legislature (Acts 1961, p. 1487), as amended, and City Code Chapters 57 and 65]. Private removal of trees in the right-of-way will require approval of the Mobile Tree Commission. Removal of heritage trees from undeveloped residential sites, developed residential sites in historic districts, and all commercial sites will require a tree removal permit.);*
- 12) Compliance with Fire Department comments: *(All projects within the City Limits of Mobile shall comply with the requirements of the City of Mobile Fire Code Ordinance (2012 International Fire Code). Fire apparatus access is required to be within 150' of all commercial and residential buildings. A fire hydrant is required to be within 400' of non-sprinkled commercial buildings and 600' of sprinkled commercial buildings.); and,*
- 13) Provision of a copy of the revised PUD site plan for review by the Planning and Zoning Department prior to the approval of any permits.

In order for any concerns to be considered by the Planning Commission you must email your concerns to planning@cityofmobile.org or they may be uploaded to the website via the portal under the meeting date and case.

Comments must be entered by 2:00 PM on Wednesday, October 6th, before the meeting, in order to be considered by the Commission.

If you wish to participate in the meeting, you must email planning@cityofmobile.org by 2:00 PM on Wednesday, October 6th, before the meeting. In accordance with Planning Commission by-laws, no more than four (4) speakers are allowed to speak for or against an application. Speaking time is limited to five (5) minutes per speaker.

LOCATOR MAP



APPLICATION NUMBER 8 DATE October 7, 2021

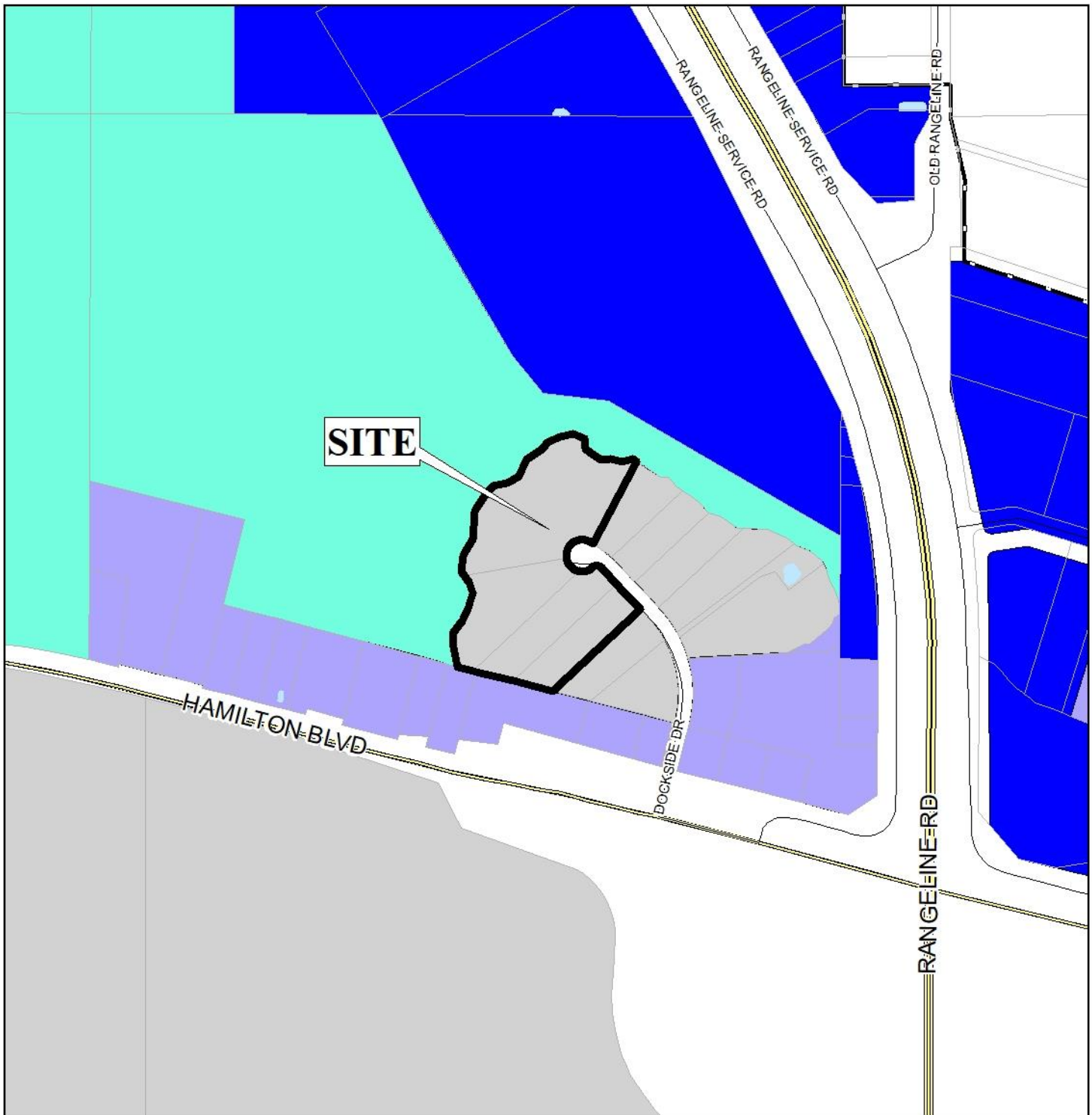
APPLICANT Hunter Development

REQUEST Planned Unit Development



NTS

LOCATOR ZONING MAP



APPLICATION NUMBER 8 DATE October 7, 2021

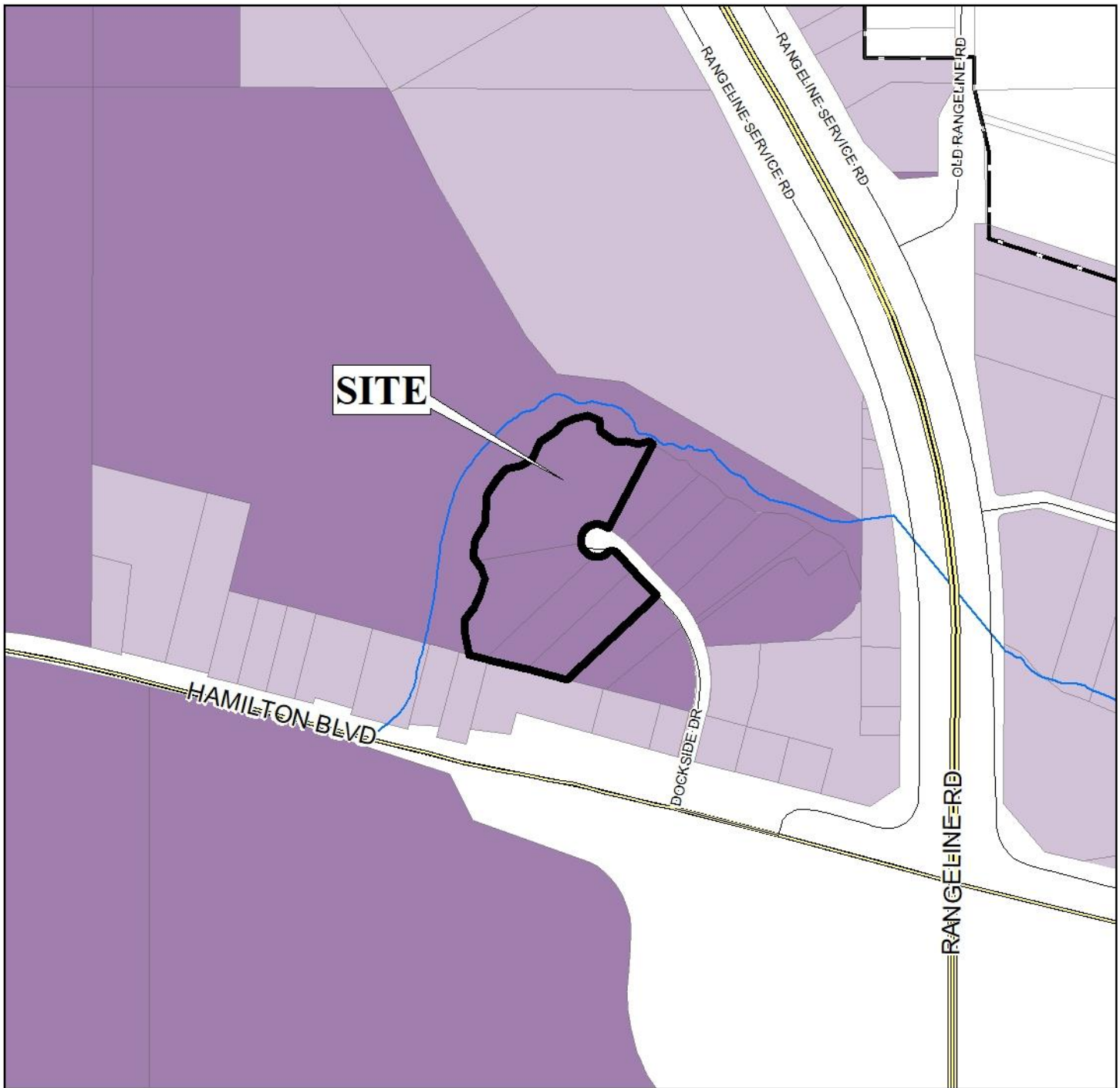
APPLICANT Hunter Development

REQUEST Planned Unit Development



NTS

FLUM LOCATOR MAP



APPLICATION NUMBER 8 DATE October 7, 2021

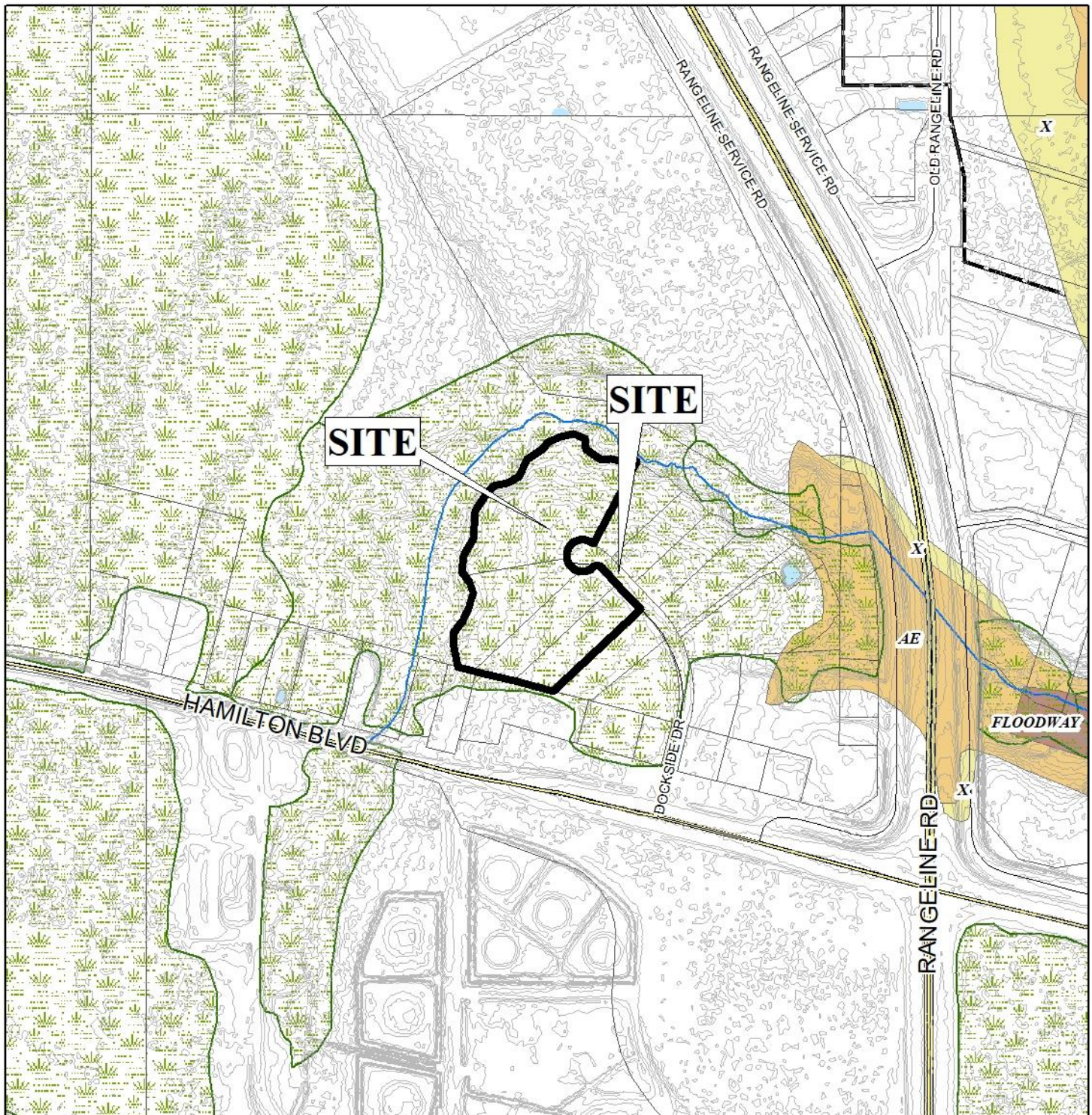
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Low Density Residential	Neighborhood Center - Traditional	Downtown Waterfront	Parks & Open Space
Mixed Density Residential	Neighborhood Center - Suburban	Light Industry	Water Dependent
Downtown	Traditional Corridor	Heavy Industry	
District Center	Mixed Commercial Corridor	Institutional	



ENVIRONMENTAL LOCATOR MAP



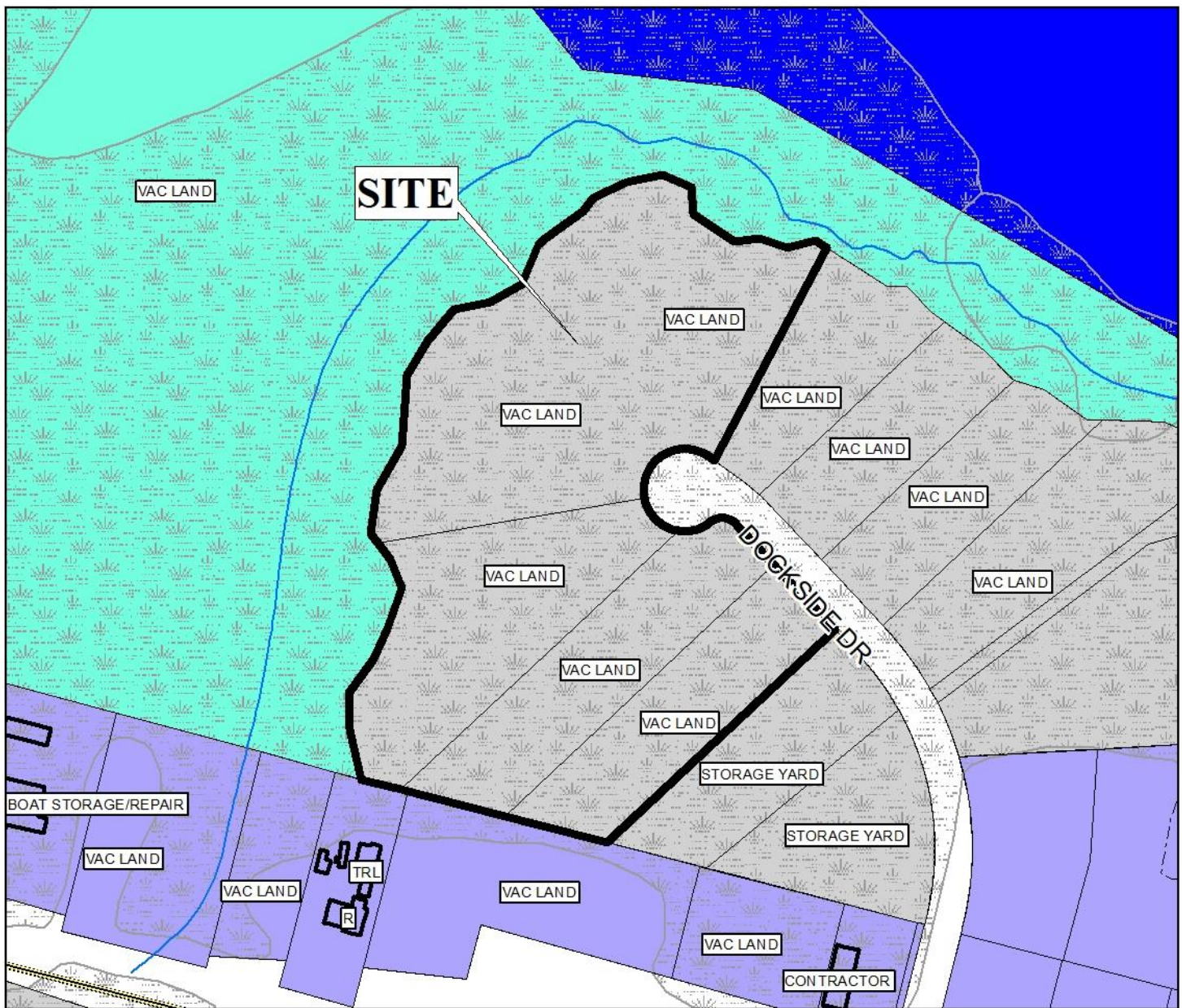
APPLICATION NUMBER 8 DATE October 7, 2021

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PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The site is surrounded by vacant land. Residential and industrial units lie south of the site.

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R-A	R-3	T-B	B-2	B-5	MUN	SD-WH	T5.1
R-1	R-B	B-1	B-3	I-1	OPEN	T3	T5.2
R-2	H-B	LB-2	B-4	I-2	SD	T4	T6



PLANNING COMMISSION VICINITY MAP - EXISTING AERIAL



The site is surrounded by vacant land. Residential and industrial units lie south of the site.

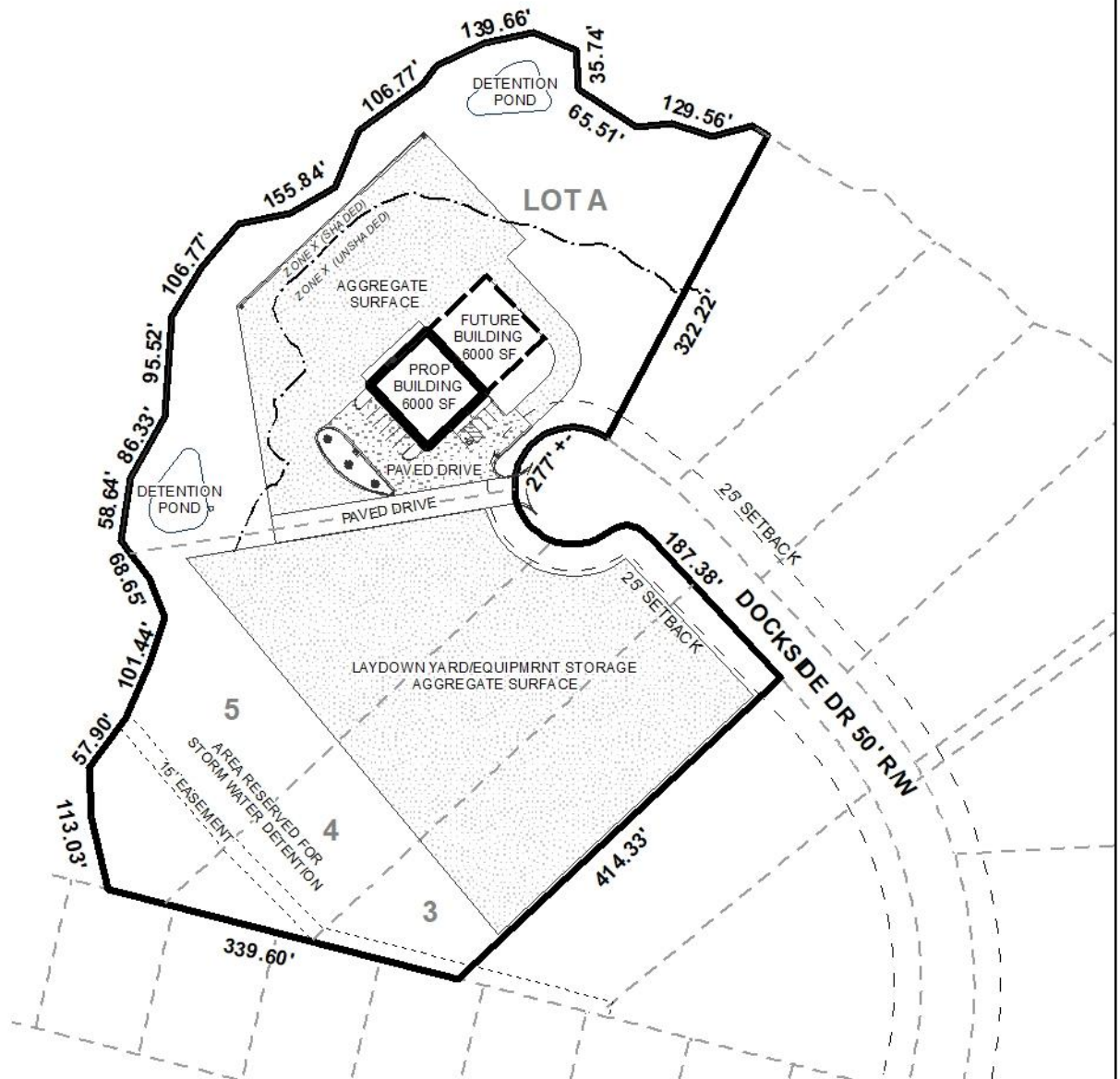
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SITE PLAN



The site plan illustrates the proposed building, future building, parking, and detention ponds.

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