

## **KILPATRICK WATERVIEW SUBDIVISION**

Engineering Comments: 1. Any work performed in the existing ROW (right-of-way) such as driveways, sidewalks, utility connections, drainage, irrigation, or landscaping will require a ROW permit from the City of Mobile Engineering Department (208-6070) and must comply with the City of Mobile ROW code and ordinances. 2. According to the FEMA flood map information, this property is located within a Special Flood Hazard Area – AE (el 10). You will need to show and label the flood hazard area(s) on your plat and plans. 3. Show the Minimum Finished Floor Elevation on each lot on the Plat. 4. Need to dedicate 60' Right-of-Way along Lipscomb Landing Road. The location of the ROW shall be approved by the City Engineer.

Traffic Engineering Comments: Driveway number, size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards.

Urban Forestry Comments: Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

Fire Department Comments: All projects within the City of Mobile Fire Jurisdiction must comply with the requirements of the 2009 International Fire Code, as adopted by the City of Mobile.

MAWWS Comments: MAWSS has water and sewer services available, but a Capacity Assurance application for sewer service has not been applied for. MAWSS cannot guarantee sewer service until the Capacity application is approved by Volkert Engineering, Inc.

The plat illustrates the proposed 2-lot, 1.0± acre subdivision, which is located on the North and South sides of Lipscombs Landing (prescriptive right-of-way), 3/10± mile East of Lees Lane, in Council District 4. The applicant states the site is served by city water and sewer services.

The purpose of this application is to subdivide an existing metes-and-bounds parcel into two legal lots of record.

As proposed, each lot would meet the minimum lot size requirements of the Subdivision Regulations. But as Lipscombs Landing is a City-maintained prescriptive right-of-way with substandard roadway and right-of-way widths, dedication would be required to provide a 60' right-of-way across the site. In a previous one-lot subdivision in 2004 on Lipscombs Landing to the East of this site, no dedication for right-of-way was required; however Lipscombs Landing was a private street at that time. At some point in time after that, Lipscombs Landing became a prescriptive right-of-way. The dedication would then effectively divide the two proposed lots in half with two half lots on each side of Lipscombs Landing. It appears that the Northern portion of proposed Lot 1 and possibly the Southern portion of proposed Lot 2 would not contain the minimum 7,200 square feet after the right-of-way dedication. A 1993 subdivision approval along Bay Front Road similar to the current application where the lots were split by the road set a precedence, and the lots were designated as Lot 1 and Lot 1A, and Lot 2 and Lot 2A, with the

condition that the portion on one side of the road could only be utilized and sold with the corresponding portion on the other side of the road. In such an instance, the combined lots would meet the minimum size standards of the Subdivision Regulations.

The plat should be revised to dedicate a 60' right-of-way centered along Lipscomb's Landing with a 25' minimum building setback line indicated along both sides of the right-of-way as measured from the required dedication. The plat should be revised to label each of the four lots with its size in both square feet and acres, after dedication, with a total square footage for the combined Lot 1 and Lot 1A, and Lot 2 and Lot 2A after dedication, or a table should be furnished on the Final Plat providing the same information.

As a means of access management, a note should be required on the Final Plat stating that each portion of each lot is limited to one curb cut to Lipscombs Landing, with the size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards.

It should be noted that the site is located within the X-unshaded, X-shaded and AE flood zones as shown on FEMA Flood Insurance Rate Maps. The site borders on Moore Creek which is a drainage canal associated with Wragg Swamp. The presence of floodplains would indicate that the area may be environmentally sensitive; therefore, if approved, the approval of all applicable federal, state and local environmental agencies would be required prior to the issuance of any permits or land disturbance activities.

The geographic area defined by the City of Mobile and its planning jurisdiction, including this site, may contain Federally-listed threatened or endangered species as well as protected non-game species. Development of this site must be undertaken in compliance with all local, state and Federal regulations regarding endangered, threatened or otherwise protected species.

The plat meets the minimum requirements of the Subdivision Regulations and is recommended for tentative approval, subject to the following conditions:

- 1) revision of the plat to dedicate a 60' right-of-way along Lipscombs Landing;
- 2) revision of the plat to indicate a 25' minimum building setback line along both sides of Lipscombs Landing, as measured from the required dedication;
- 3) the portions of the lots lying on the South side of Lipscombs Landing be denoted as Lots 1 and 2, and the portions lying on the North side of Lipscombs Landing be denoted as Lots 1A and 2A;
- 4) revision of the plat to label each of the four lots with its size in both square feet and acres, after dedication, with a total square footage for the combined Lot 1 and Lot 1A, and Lot 2 and Lot 2A after dedication, or the furnishing of a table on the Final Plat providing the same information;
- 5) placement of a note on the Final Plat stating that each portion of each lot is limited to one curb cut to Lipscombs Landing, with the size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards;
- 6) placement of a note on the Final Plat stating that the approval of all applicable federal, state and local agencies for wetland and floodplain issues would be required prior to the issuance of any permits or land disturbance activities;

- 7) placement of a note on the Final Plat stating that development of this site must be undertaken in compliance with all local, state, and Federal regulations regarding endangered, threatened or otherwise protected species;
- 8) subject to the Engineering Comments: *(1. Any work performed in the existing ROW (right-of-way) such as driveways, sidewalks, utility connections, drainage, irrigation, or landscaping will require a ROW permit from the City of Mobile Engineering Department [208-6070] and must comply with the City of Mobile ROW code and ordinances. 2. According to the FEMA flood map information, this property is located within a Special Flood Hazard Area – AE [el 10]. You will need to show and label the flood hazard area(s) on your plat and plans. 3. Show the Minimum Finished Floor Elevation on each lot on the Plat. 4. Need to dedicate 60' Right-of-Way along Lipscomb Landing Road. The location of the ROW shall be approved by the City Engineer);*
- 9) placement of a note on the Final Plat stating that Lots 1A and 2A may only be utilized and sold with the corresponding lots on the South side of Lipscombs Landing (Lots 1 and 2); and
- 10) placement of a note on the Final Plat stating that no permits are to be issued until the Final Plat is recorded.

## LOCATOR MAP



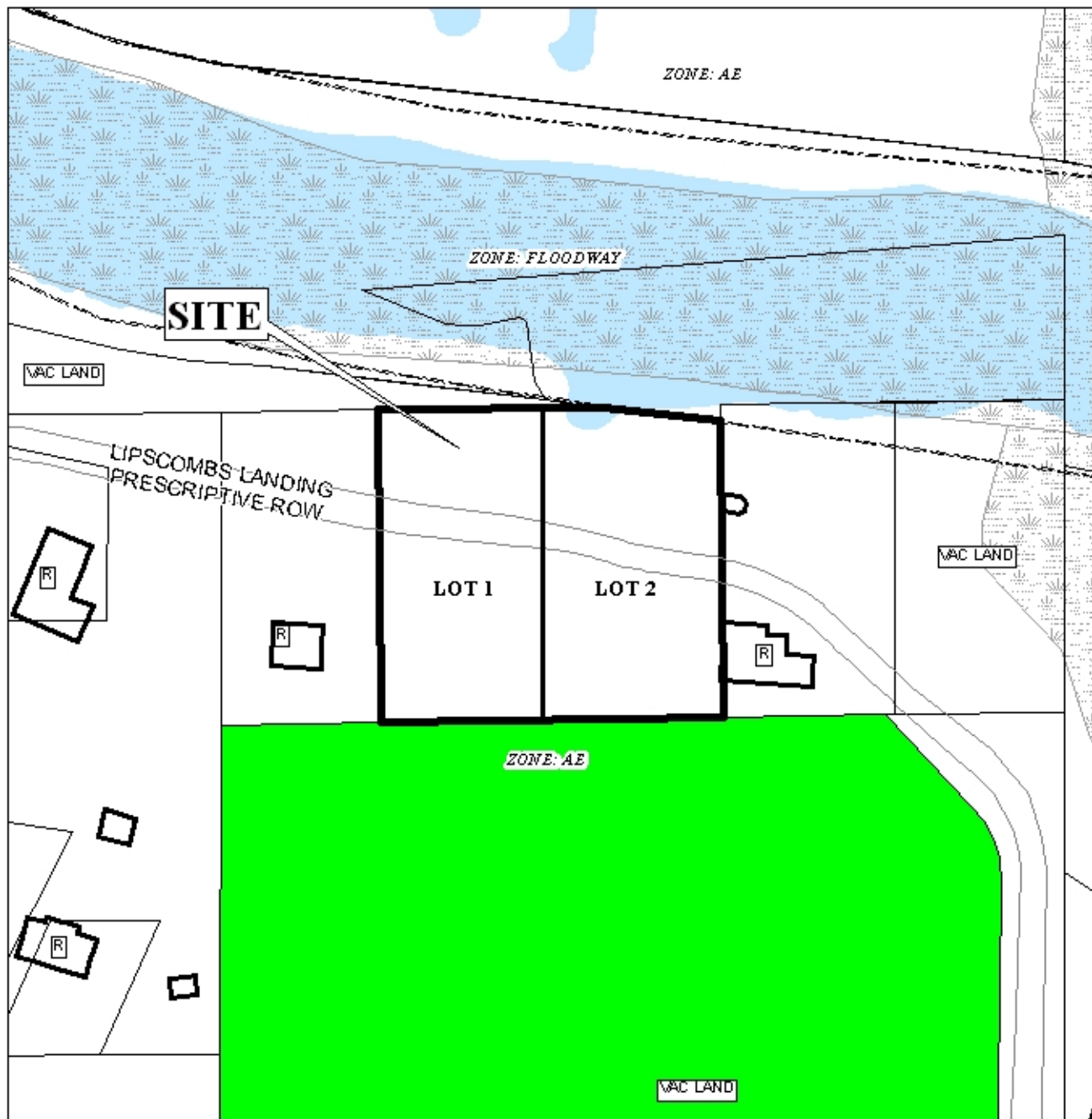
APPLICATION NUMBER 5 DATE May 3, 2012

APPLICANT Kilpatrick Waterview Subdivision

REQUEST Subdivision



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