

WOODBERRY FOREST SUBDIVISION ADDITIONS

Engineering Comments: Must comply with the Mobile County Flood Damage Prevention Ordinance. Development shall be designed to comply with the storm water detention and drainage facility requirements of the City of Mobile storm water and flood control ordinances, and requiring submission of certification from a licensed engineer certifying that the design complies with the storm water detention and drainage facility requirements of the City of Mobile storm water and flood control ordinances prior to the issuance of any permits.

Fire-Rescue Department Comments: Subdivision layout and design must comply with the requirements of the 2003 International Fire Code, including Appendices B through D, as adopted by the City of Mobile. Commercial buildings and sites within the City of Mobile Fire Jurisdiction must comply with the requirements of the 2003 International Fire Code, including Appendices B through D, as adopted by the City of Mobile, and the 2003 International Existing Building Code, as appropriate. Fire hydrants shall be provided per Section 508.5.1 of the 2003 IFC. Access to building shall be provided per Section 503.1.1 & 503.2.1 of the 2003 IFC.

MAWWS Comments: No comments.

The plat illustrates the proposed 78 lot, 47.9 ± acre subdivision which is located on the West side of Wilson Road West at its South terminus, extending to the North terminus of Selby Phillips Drive. The applicant states that the subdivision is served by public water and sanitary sewer systems.

The purpose of this application is to subdivide multiple metes and bounds parcels into 78 lots. There are several concerns regarding this application. First, it appears that a portion of a parcel was divided off in 1999 (parcel R022707260000011.001.), without benefit of going through the Subdivision process; the parcel is also land-locked. Secondly, it appears that the application does not include the segment of parcel R022707260000011. that extends to Twelve Oaks Drive. Finally, the proposed subdivision appears to abut two land-locked parcels to the North and/or West. Several hours of additional research by several staff members resulted in the discovery that Planning Commission approval for the Springfield Subdivision in October 1999 – since expired - required the applicant at the time to submit a deed creating R022707260000011.001., thus connecting the larger land-locked parcel to the strip to Twelve Oaks Drive – which may have been recorded a right-of-way, but never built to County standards. The applicant should verify this information to support their subdivision application, and so revise the plat to clearly indicate why the parcels or land are not included in the application, and why street-stubs to adjacent land-locked property may not be required.

The site appears to have frontage on two proposed major streets and two minor streets. The East side of the site appears to be bounded by the proposed Dawes Road major street, which will connect from the existing Dawes Road up to Airport Boulevard, utilizing the alignment of Wilson Road West (labeled as Lowry Road on the plat). A 100-foot wide right-of-way is required by the Major Street Plan component of the Comprehensive Plan, thus the subdivision should be redesigned to provide 50-feet of right-of-way, as measured from the centerline of Wilson Road West. It should also be pointed out that Wilson Road West appears to be unpaved, and lacks paved access to the existing Dawes Road. The South side of the site is bounded by the

proposed alignment of the Grelot Road Extension, and the preliminary plat depicts right-of-way sufficient to provide 50-feet, as measured from the centerline of the proposed alignment. Previous approvals for the site required the construction of Grelot Road Extension where it occurs adjacent to the site, and as access to the subdivision is limited, this condition is valid for the application at hand.

The site also has frontage to the street-stub for Selby Phillips Drive, a minor street with adequate right-of-way, and potentially frontage on an unbuilt street-stub connecting to Twelve Oaks Drive. The unbuilt street-stub has a right-of-way width of 60-feet, thus is adequate.

Access management is a concern, as Wilson Road West appears to be unpaved. The applicant should indicate how development will be phased, as the bulk of the development should not occur until access to an improved Wilson Road West and the existing Dawes Road can be provided, or until Grelot Road Extension is built where it is adjacent to the site.

It appears that most of the development will be sized for single-family residential development, however, proposed lots 38 and 78 will be approximately 5 or more acres each, compared to the average 10,000 square foot lot proposed for the rest of the development. Lot 38 includes wetlands and floodplains associated with Miller Creek, while Lot 78 has frontage on the right-of-way for Wilson Road West, and includes a pond and creek. Both lots 38 and 78 appear to front right-of-way to be dedicated for the extension of Grelot Road.

The development includes cul-de-sacs, thus the length of the cul-de-sacs and the diameter of the cul-de-sacs should comply with the minimum requirements of Appendix D of the 2003 International Fire Code, as adopted by the City of Mobile. The design of the subdivision should be revised, if necessary, to comply with the design requirements of Appendix D of the International Fire Code, as adopted by the City of Mobile.

It appears that each proposed lot will meet the minimum area requirements of the Subdivision Regulations for lots with public water and sanitary sewer facilities. Access management is a concern, however, due to the size of the lots and the fact that the development will also have corner lots. All lots, with the exception of Lot 78, should be denied direct access to Wilson Road West and the proposed Grelot Road, and should be otherwise limited to one curb-cut. Lot 78, due to its frontage length, should be limited to a maximum of three curb-cuts onto Wilson Road West, once it is improved to County standards, and three curb-cuts onto the future Grelot Road, once it is improved to County standards. The size, design and location of all curb-cuts are to be approved by the Mobile County Engineering Department.

The plat illustrates that floodplains and other water features are present on the property, thus some parts of the site may be considered environmentally sensitive. Therefore, approvals from federal, state, and local agencies will be required for potential floodplain and wetland issues prior to the issuance of any permits or land disturbance activities.

The geographic area defined by the city of Mobile and its planning jurisdiction, including this site, may contain Federally-listed threatened or endangered species as well as protected non-game species. Development of the site must be undertaken in compliance with all local, state and Federal regulations regarding endangered, threatened or otherwise protected species.

The 25-foot minimum building setback is not shown on the plat, but is required. The setback line should be depicted from all existing and proposed street frontages and labeled.

This site is located in the County, thus a note should be placed on the final plat stating that any lots which are developed commercially and adjoin residentially developed property must provide a buffer, in compliance with Section V.A.7. of the Subdivision Regulations.

The site will have to comply with the City of Mobile storm water and flood control ordinances. A note should be placed on the final plat stating that the development will be designed to comply with the storm water detention and drainage facility requirements of the City of Mobile storm water and flood control ordinances, and requiring submission of certification from a licensed engineer certifying that the design complies with the storm water detention and drainage facility requirements of the City of Mobile storm water and flood control ordinances prior to the signing of the final plat. Certification is to be submitted to the Planning Section of Urban Development and County Engineering.

Detention common areas proposed for the subdivision. All common areas should be labeled, and a note should be placed on the Final Plat, if approved, stating that maintenance of the common areas, including detention areas and any other common areas, are the responsibility of the subdivision's property owners

The plat should be revised to show lot size in square feet, or a table provided on the plat with the same information.

Based on the preceding, the application is recommended for Holdover until March 6th for the following reasons:

- 1) revision of the plat to clearly indicate that access to adjacent land-locked parcels is addressed through deeds or public rights-of-way, and provision of supporting documentation;
- 2) revision of the plat to depict required dedication along Wilson Road West to provide 50-feet, as measured from the centerline of Wilson Road West;
- 3) revision of the plat to depict 120-foot diameter cul-de-sac right-of-ways;
- 4) revision of the lot layout and size as necessary per items #2 and 3;
- 5) labeling of all lots with their size in square feet;
- 6) depiction of the 25-foot minimum building setback lines from all street frontages – existing and proposed; and
- 7) provision of 6 revised plats and supporting documentation for deeded parcels for adjacent land-locked properties to the Planning Section of Urban Development by February 13th.

Revised for the March 6th meeting:

A revised plat was submitted by the applicant, addressing most of the issues previously identified, but not all of the issues were adequately addressed. Specifically, documentation was not provided regarding access to the land-locked parcels to the North of the site (which were documented as being under family ownership). The plat was also not fully revised to depict required dedication for all lots fronting Wilson Road West / Lowry Road (future Dawes Road),

nor was the minimum building setback line depicted from all existing and proposed street frontages.

Due to lack of frontage on paved public streets, the proposed development should be phased. It is recommended that proposed lots 1-16 and 46-78 be prohibited from recording until such time as Grelot Road is built from Dawes Road to Selby Phillips Drive, or until the new Dawes Road is constructed from the existing Dawes Road to Airport Boulevard along the Wilson Road West / Lowry Road alignment. Therefore, only proposed lots 17-45 should be recorded due to lack of access for emergency vehicles, until additional access is provided or available.

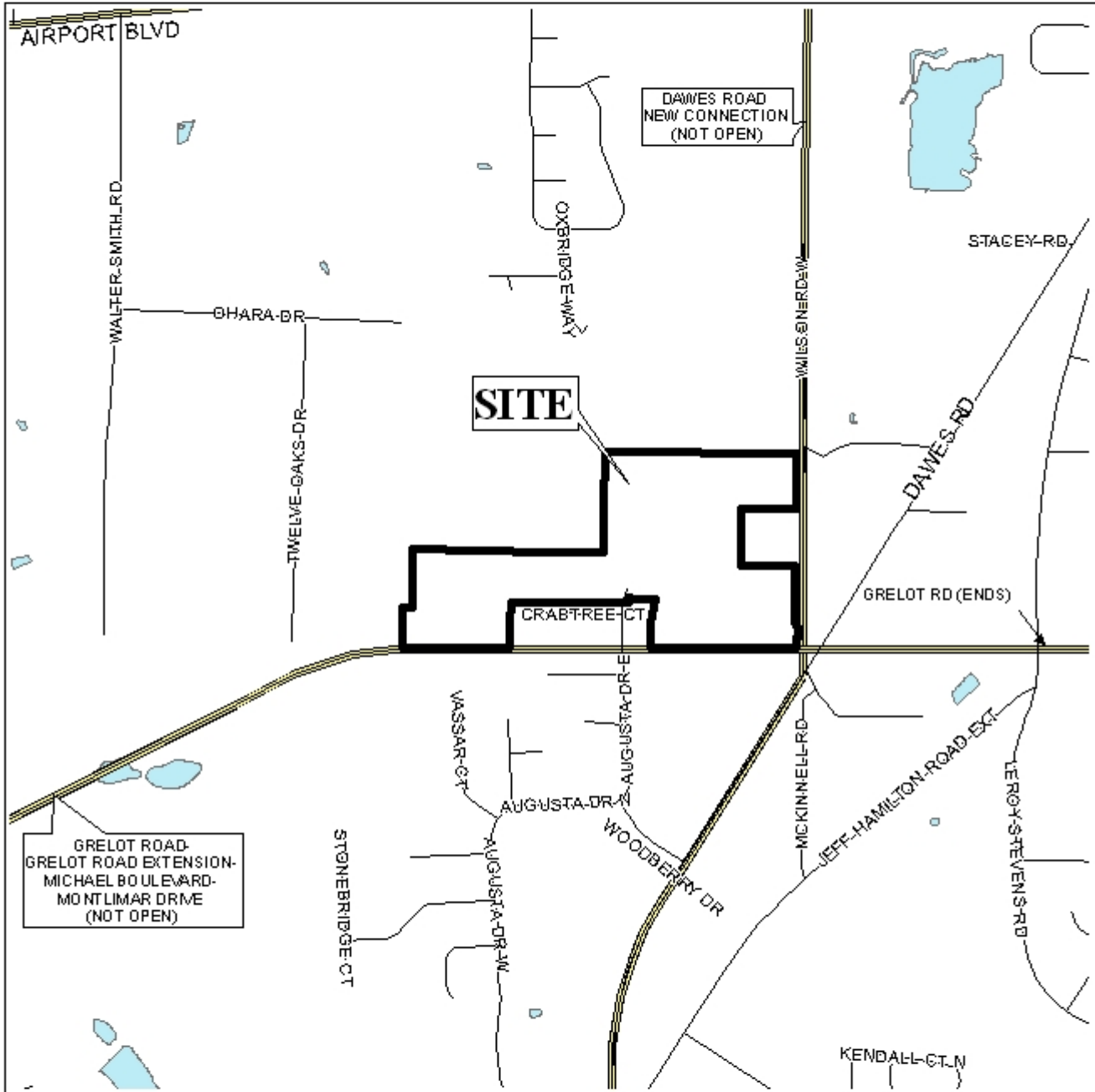
It should also be pointed out that the topographic and natural features of the site and the land-locked property to the North indicate that the provision of a street-stub to the East side of the land-locked properties would be advisable, as the land-locked property is bisected by Miller Creek.

Based upon the revised plat, the application is recommended for Tentative Approval, subject to the following conditions:

- 1) Provision of documentation regarding parcel R022707260000011.001., and its establishment as a future road to serve adjacent land-locked parcels, or provision of a street-stub to the land-locked parcels in the vicinity of the proposed detention area near lots 61 and 62;*
- 2) Placement of a note on the plat stating that development of the site is limited to lots 17-45, and that no additional lots may be recorded until additional access to a public road is provided via construction of Grelot Road from Dawes Road to Selby Phillips Drive, or construction of the new Dawes Road along the Wilson Road West / Lowry Road alignment;*
- 3) Dedication and labeling of right-of-way sufficient to provide 50-feet, as measured from the centerline of Wilson Road West / Lowry Road, including where it abuts proposed lot 78;*
- 4) Depiction and labeling of the 25-foot minimum building setback line from all existing and proposed road frontages, including where lot 78 abuts proposed Grelot Road and Dawes Roads, and where lot 38 abuts proposed Grelot Road and the proposed "future street" on the western boundary of the lot;*
- 5) Placement of a note on the final plat stating that all lots, with the exception of Lot 78, are denied direct access to Wilson Road West / Lowry Road (future Dawes Road) and the proposed Grelot Road, and are otherwise limited to one curb-cut, that Lot 78 is limited to a maximum of three curb-cuts onto Wilson Road West / Lowry Road (future Dawes Road), once it is improved to County standards, and three curb-cuts onto the future Grelot Road, once it is improved to County standards, and that the size, design and location of all curb-cuts are to be approved by the Mobile County Engineering Department;*
- 6) Construction of all streets to Mobile County and 2003 International Fire Code standards (as adopted by the City of Mobile), and acceptance of the streets by Mobile County;*
- 7) Placement of a note on the plat / site plan stating that the site must be developed in compliance with all local, state and Federal regulations regarding endangered, threatened or otherwise protected species;*

- 8) *Placement of a note on the plat stating that approval of all applicable Federal, state and local agencies is required for wetland and floodplain issues, if any, prior to the issuance of any permits or land disturbance activities;*
- 9) *Labeling of all common areas and detention common areas, and placement of a note on the Final Plat stating that maintenance of the common areas, including detention areas and any other common areas, is the responsibility of the subdivision's property owners;*
- 10) *Provision of a letter from a licensed engineer to the Planning Section of Urban Development and Mobile County Engineering, certifying that the storm water detention, drainage facilities, and release rate comply with the City of Mobile storm water and flood control ordinances, prior to the signing and recording of the final plat for any phase;*
- 11) *Placement of a note on the Final Plat stating that any lots which are developed commercially and adjoin residentially developed property must provide a buffer, in compliance with Section V.A.7. of the Subdivision Regulations; and*
- 12) *The labeling of all lots with their size in square feet.*

LOCATOR MAP



APPLICATION NUMBER 5 DATE March 6, 2008

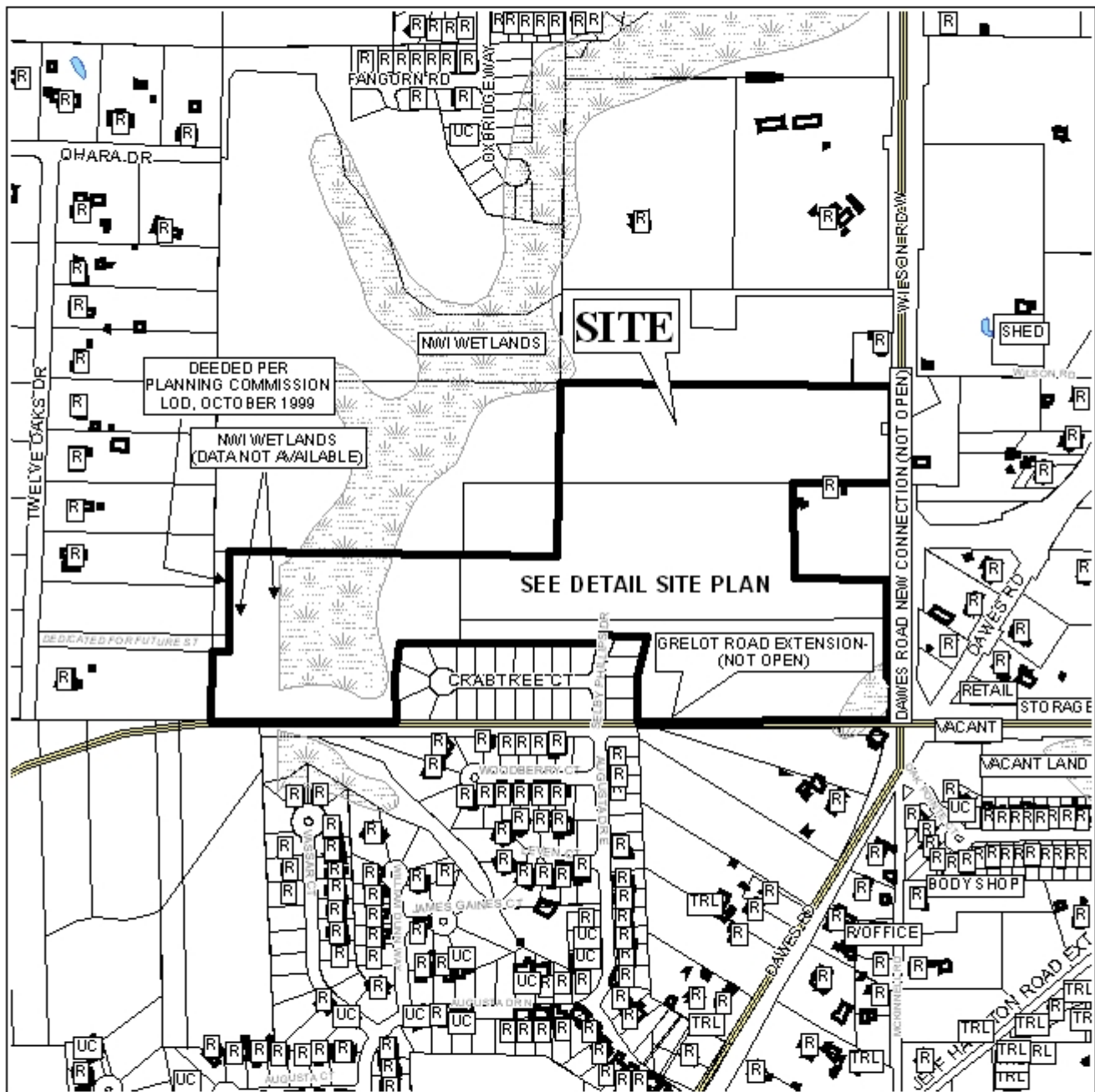
APPLICANT Woodberry Forest Subdivision Additions

REQUEST Subdivision



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WOODBERRY FOREST SUBDIVISION ADDITIONS



APPLICATION NUMBER 5 DATE March 6, 2008

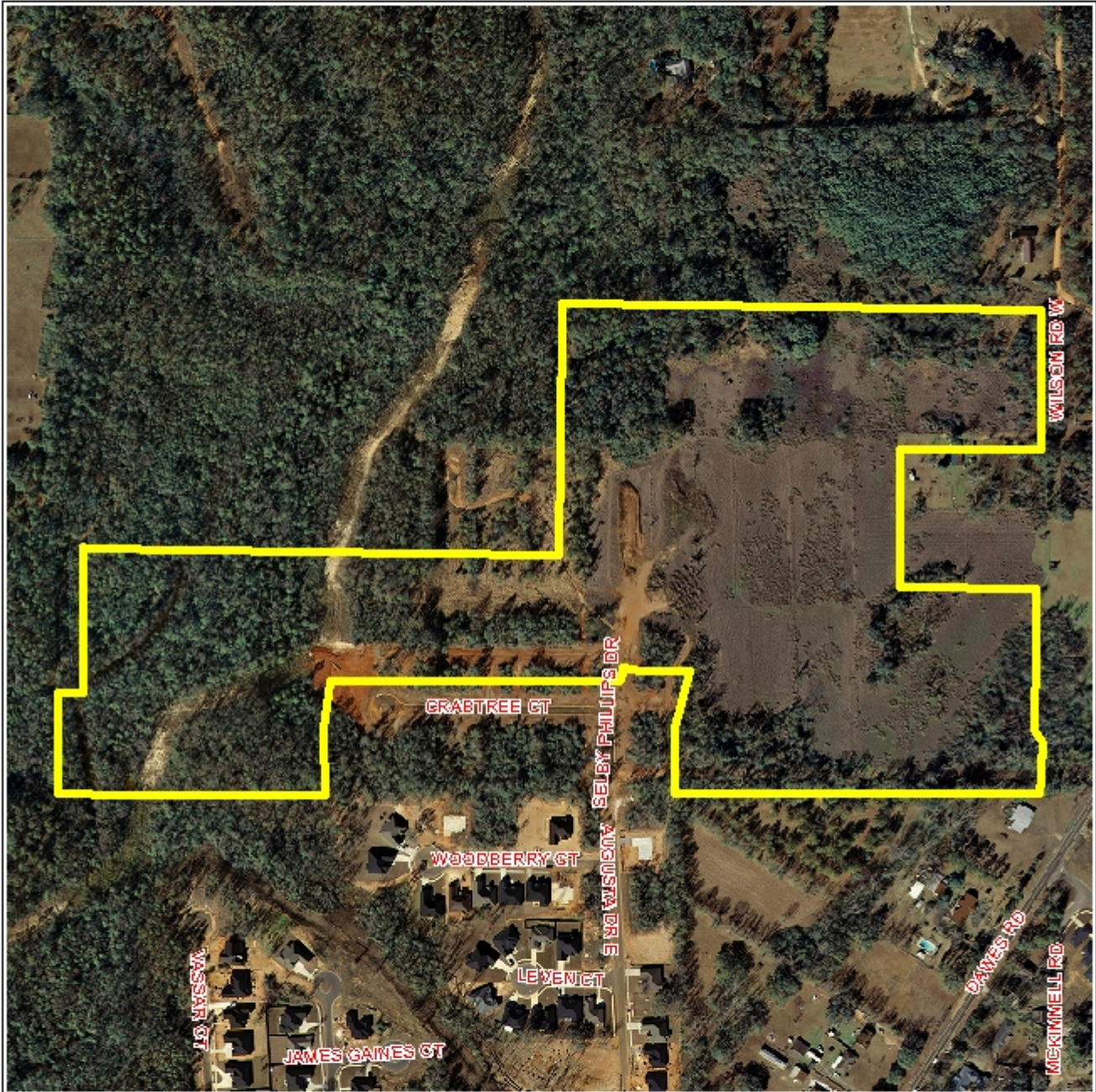
LEGEND

R-1	R-2	R-3	R-A	R-B	H-B	T-B	B-1	LB-2	B-2	B-3	B-4	B-5	I-1	I-2
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APPLICATION NUMBER 5 DATE March 6, 2008



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