

ZONING AMENDMENT,
PLANNING APPROVAL &
PLANNED UNIT DEVELOPMENT
STAFF REPORT

Date: April 17, 2003

<u>NAME</u>	Radcliff/Economy Marine Services
<u>DEVELOPMENT NAME</u>	Radcliff/Economy Marine Services
<u>LOCATION</u>	115 Cochrane Bridge Causeway (West side of Cochrane Bridge Causeway, 160'± North of Dunlap Drive)
<u>PRESENT ZONING</u>	R-1, Single-Family Residential and I-2, Heavy Industry
<u>PROPOSED ZONING</u>	I-2, Heavy Industry
<u>AREA OF PROPERTY</u>	10± Acres
<u>CONTEMPLATED USE</u>	Petroleum storage facility consisting of multiple building sites, with shared parking and access It should be noted, however, that any use permitted in the proposed district would be allowed at this location if the zoning is changed. Furthermore, the Planning Commission may consider zoning classifications other than that sought by the applicant for this property.
<u>TIME SCHEDULE FOR DEVELOPMENT</u>	Immediately
<u>ENGINEERING COMMENTS</u>	Must comply with all stormwater and flood control ordinances. Any work performed in the right of way will require a right of way permit.
<u>TRAFFIC ENGINEERING COMMENTS</u>	All driveway widths and locations to be approved by Traffic Engineering; and design to meet AASHTO Standards

REMARKS

The applicant is requesting rezoning from R-1, Single-Family Residential and I-2, Heavy Industry, to I-2, Heavy Industry to eliminate split zoning at an existing petroleum products storage facility. Planning Approval is required the expansion of the petroleum storage facilities, and Planned Unit Development (PUD) approval is required for multiple buildings, as well as shared parking and access between two lots.

The site is illustrated as industrial on the General Land Use Component of the Comprehensive Plan, which is meant to serve as a general guide, not a detailed lot and district plan or mandate for development. Moreover, the General Land Use component allows the Planning Commission and City Council to consider individual cases based on additional information such as the classification requested, the surrounding development, the timing of the request, and the appropriateness and compatibility of the proposed use and zoning classification.

Although a portion of the property adjacent to the South is zoned residentially, it adjoins the entrance to the Bankhead Tunnel and a large shipyard, thus it is unlikely the property would ever be used residentially. Additionally, the remainder of the area is zoned and utilized industrially and the rezoning of the site would simply be considered infill.

The review required for Planning Approval examines the applicant's location and site plan with regard to transportation, parking and access, public utilities and facilities, traffic congestion and hazard, and to determine if the proposal is in harmony with the orderly and appropriate development of the district.

Petroleum storage requires Planning Approval in an I-2 district. The area is highly industrialized with tank storage of similar products, and the location could be considered appropriate due to its compatibility with the surrounding land uses, location on a major street, the ease of access to I-10, I-165 and the Mobile River. The applicant states that new warehouse, within which the petroleum storage will be located, would be totally contained within a diked area. It should be noted that petroleum storage adjacent to the Mobile River may require additional approvals from federal, state and local agencies prior to the development of the site.

Planned Unit Development review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating excess traffic along minor residential streets in residential districts outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD.

The applicant has existing storage tanks on the site and proposing to expand the facilities onto an adjacent lot. The large existing building will be renovated for office use and the proposed building will contain additional petroleum storage tanks. Additional gravel

parking and drives are proposed; gravel parking and drives are allowed by right in I-2 districts.

Although the site adjoins R-1 property to the South, a buffer should not be required if and until the site to the South is developed residentially. Additionally, full compliance with the landscaping and tree planting requirements of the Ordinance would be required; however, in I-2 districts, only frontage trees are required.

The site fronts Cochrane Bridge Causeway, a planned major street, which has an existing right-of-way in compliance with the Major Street Plan. However, access management is a concern, thus the number, location and design of all curb cuts should be approved by Traffic Engineering.

RECOMMENDATION

Rezoning: Based upon the preceding, this application is recommended for approval subject to the following conditions: 1) the approval of all necessary federal, state and local agencies prior to development; 2) provision of a buffer along the South property line, at such time the adjacent property is developed residentially; 3) full compliance with the landscaping and tree planting requirements of the Ordinance (frontage trees only for industrial property); 4) number, location and design of all curb cuts to be approved by Traffic Engineering; and 5) full compliance with all municipal codes and ordinances.

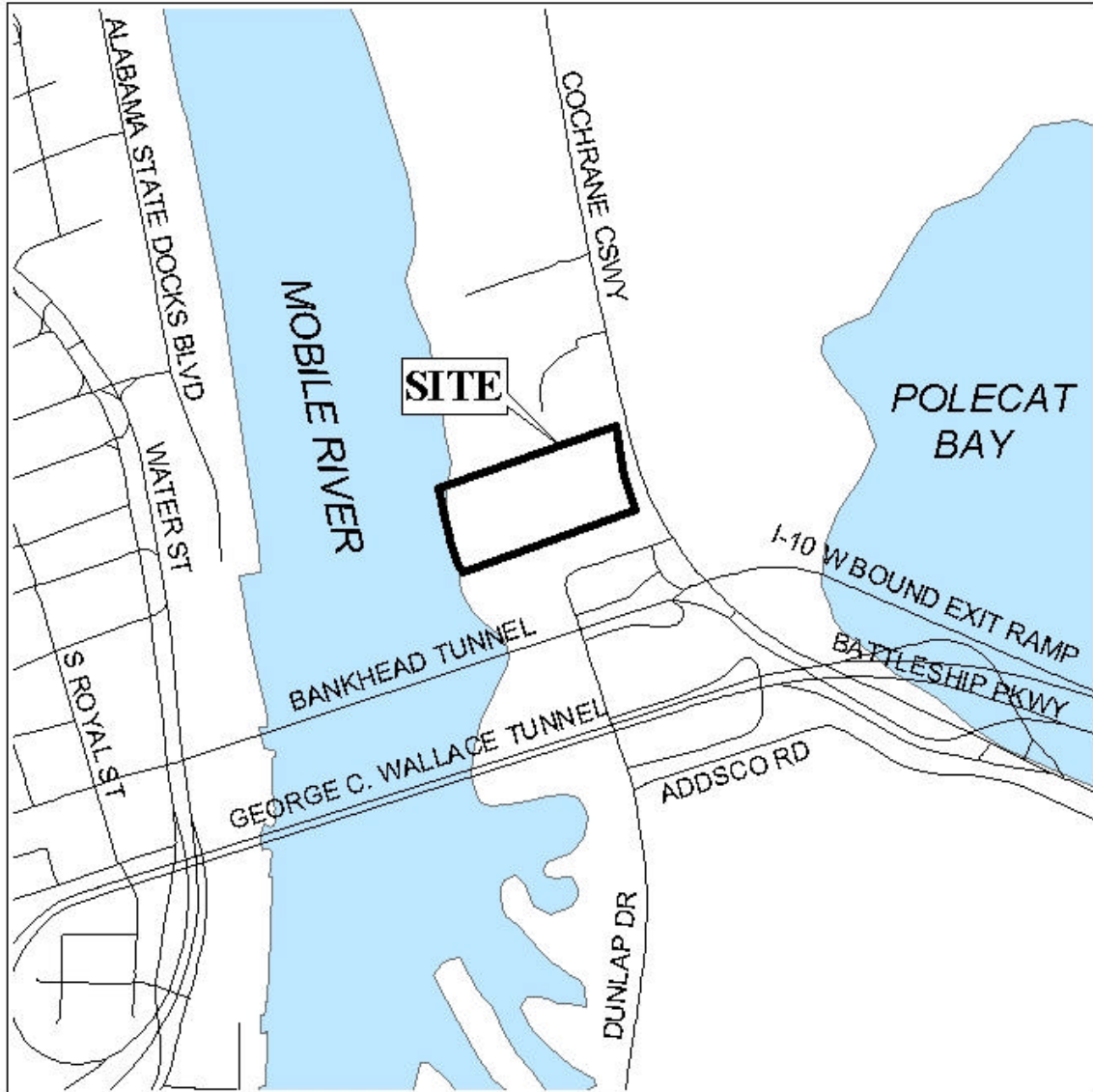
RECOMMENDATION

Planning Approval: 1) the approval of all necessary federal, state and local agencies prior to development; 2) provision of a buffer along the South property line, at such time the adjacent property is developed residentially; 3) full compliance with the landscaping and tree planting requirements of the Ordinance (frontage trees only for industrial property); 4) number, location and design of all curb cuts to be approved by Traffic Engineering; and 5) full compliance with all municipal codes and ordinances.

RECOMMENDATION

Planned Unit Development: 1) the approval of all necessary federal, state and local agencies prior to development; 2) provision of a buffer along the South property line, at such time the adjacent property is developed residentially; 3) full compliance with the landscaping and tree planting requirements of the Ordinance (frontage trees only for industrial property); 4) number, location and design of all curb cuts to be approved by Traffic Engineering; and 5) full compliance with all municipal codes and ordinances.

LOCATOR MAP



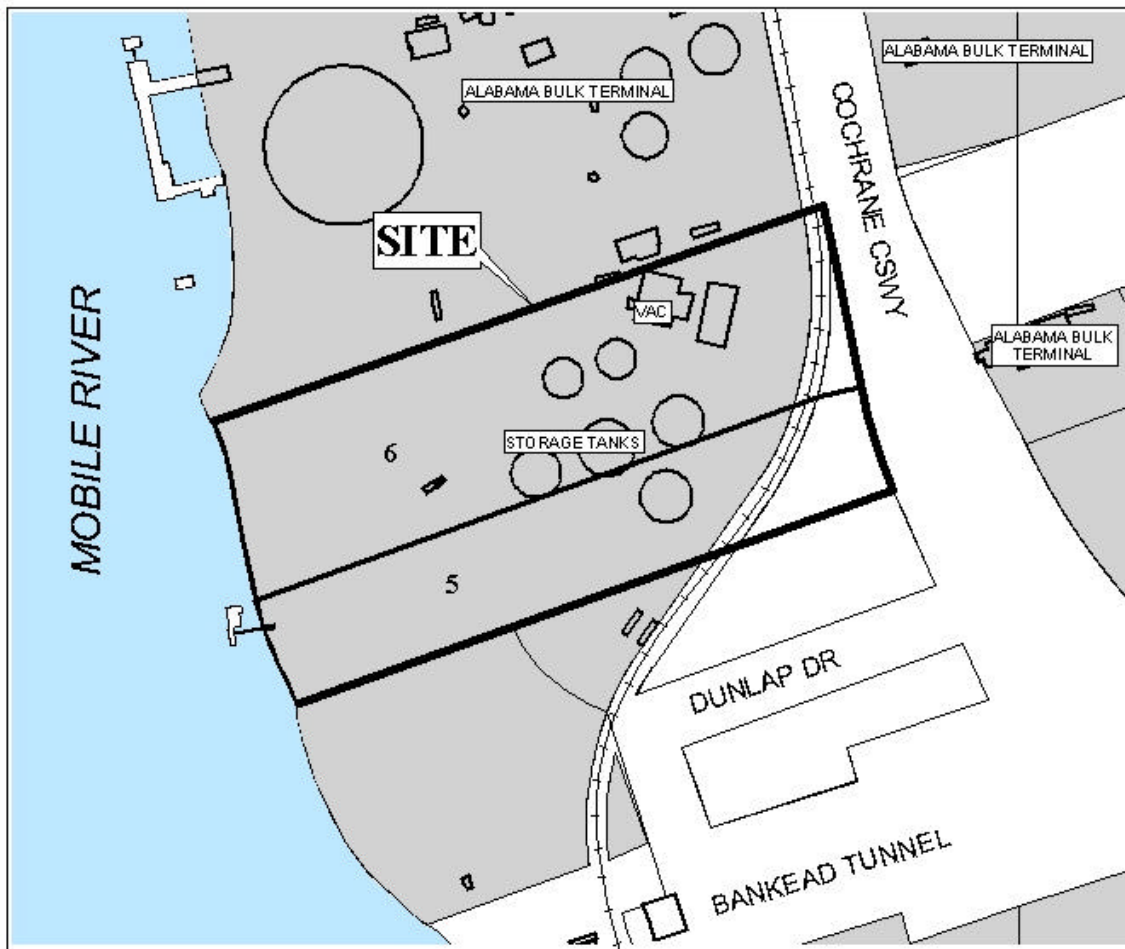
APPLICATION NUMBER 3 & 4 & 5 DATE April 17, 2003

APPLICANT Radcliff/Economy Marine Services Inc.

REQUEST Rezoning, Planning Approval and PUD



PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The site is located in an industrial area north of the Bankhead Tunnel and east of the Mobile River.

APPLICATION NUMBER 3 & 4 & 5 DATE April 17, 2003

APPLICANT Radcliff/Economy Marine Services Inc.

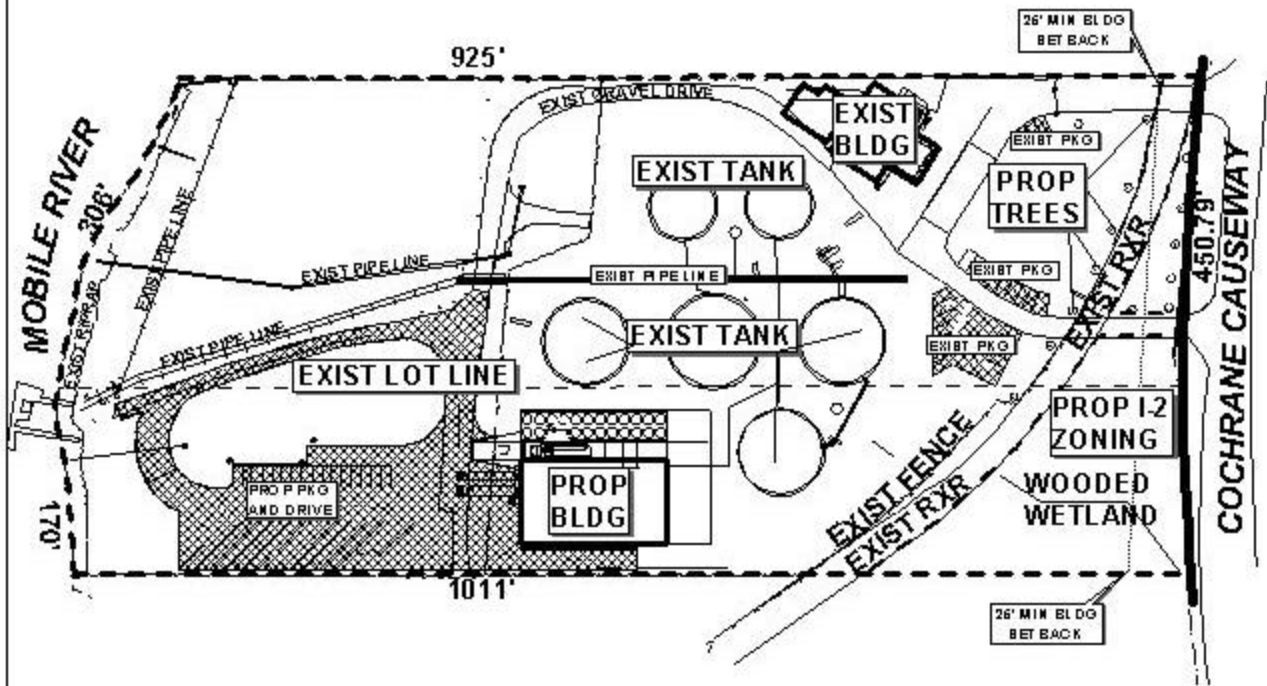
REQUEST Rezoning, Planning Approval and PUD

LEGEND



NTS

SITE PLAN



The site plan illustrates the existing buildings, tanks, drives, pipe lines, and railroad along with proposed trees, building, proposed areas to be rezoned, and proposed setback.

APPLICATION NUMBER 3 & 4 & 5 DATE April 17, 2003

APPLICANT Radcliff/Economy Marine Services, Inc.

USE/REQUEST Rezoning, Planning Approval, Planned Unit Development



NTS