

NGUYEN PLACE SUBDIVISION

Engineering Comments: FINAL PLAT COMMENTS (should be addressed prior to submitting the FINAL PLAT for review and/or signature by the City Engineer):

- A. Provide all of the required information on the SUBDIVISION PLAT (i.e. signature blocks, signatures, certification statements, written legal description, required notes, legend, scale, bearings and distances) that is required by the current Alabama State Board of Licensure for Professional Engineers and Land Surveyors.
- B. Add street names to the vicinity map, especially the ones shown on the plat.
- C. Review and revise the written legal description to match the reference to the page number shown in the description of the Point of Beginning.
- D. Show and label each and every Right-Of-Way and easement.
- E. Provide and label the monument set or found at each subdivision corner.
- F. Add a signature block for the Owner, Notary Public, Planning Commission, Traffic Engineer, and City Engineer.
- G. Provide the Surveyor's Certificate and Signature.
- H. Provide the Surveyor's, Owner's (notarized), Planning Commission, and Traffic Engineering signatures.
- I. Add a note to the SUBDIVISION PLAT stating that a Land Disturbance permit will be required for any land disturbing activity in accordance with Mobile City Code, Chapter 17, Storm Water Management and Flood Control); the City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control.
- J. Add a note to the SUBDIVISION PLAT stating that as shown on the 1984 aerial photo (FLIGHT 20 - #79) LOTS 1 and 2 will have to share the historical credit of existing (1984) historical credit of impervious area towards stormwater detention requirement per Mobile City Code, Chapter 17, Storm Water Management and Flood Control). Coordinate with City Engineering Department staff to establish the exact amount that each Lot will receive prior to the submittal of the Final Plat from review and signatures.
- K. Add a note to the Plat stating that the approval of all applicable federal, state, and local agencies (including all storm water runoff, wetland and floodplain requirements) will be required prior to the issuance of a Land Disturbance permit.
- L. Add a note that sidewalk is required to be constructed, and/or repaired, along the frontage of each lot, or parcel, at time of new development or construction, unless a sidewalk waiver is approved.
- M. Provide a copy of the FINAL SUBDIVISION PLAT to the Engineering Dept. for review. No signatures are required on this drawing.
- N. After addressing all of the FINAL SUBDIVISION PLAT review comments by the Engineering Dept. provide the ORIGINAL (with all other signatures) and one (1) copy (signatures not required) of the revised Final Plat to the Engineering Department.

Traffic Engineering Comments: Each lot is limited to one curb cut, or one shared curb-cut with appropriated easement, with size, location and design to be approved by Traffic Engineering and conform to AASHTO standards. Lot 1 is denied access to Knollwood Drive.

Urban Forestry Comments: Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 2015-116 and City Code Chapters 57 and 64).

Fire Department Comments: All projects within the City Limits of Mobile shall comply with the requirements of the City of Mobile Fire Code Ordinance (2012 International Fire Code).

MAWWS Comments: MAWSS has only water services available. Sanitary sewer service would have to be extended to the property.

Map for Mobile Development Area(s) and Intent: Suburban Neighborhood:

- emphasize connectivity to surrounding neighborhoods and close services and retail;
- accommodation of pedestrian and bicycle traffic in addition to automobiles; and,
- appropriate scaled infill development to complement existing character of neighborhoods.

The plat illustrates the proposed 2-lot, 2.1± acres subdivision located at the North terminus of Nguyen Le Drive. The applicant states the property is served by public water and sewer, in Council District 6.

The purpose of this application is to subdivide one metes-and-bounds parcel into two legal lots of record. It should be noted that the site is developed and was subject to Use Variance approval at the January 7, 1974 meeting of the Board of Zoning Adjustment to allow a private swim club for residents of the abutting Amberly Subdivision. Similar use of the property was granted legal nonconforming status via affidavit notarized on November 18, 1996.

Proposed Lot 1 has frontage on Knollwood Drive, to the East, a major street with curb and gutter requiring a 100' right-of-way width; and Nguyen Le Drive, formerly Swim Club Road, to the South, a minor street with curb and gutter requiring a 50' right-of-way width. Proposed Lot 2 also has frontage on Nguyen Le Drive. The preliminary plat illustrates adequate rights-of-way widths along both streets to which the site fronts; therefore, no additional dedications should be required.

It should be noted that Nguyen Le Drive is a closed-end street that would traditionally require an approved turnaround to facilitate Fire Department operations. However, such turnarounds are only required for closed-end streets 150' or longer, and Nguyen Le Drive is only 140'± long; therefore, no turnaround should be required.

The proposed lots are irregularly shaped and do not meet the minimum 60' width requirements for lots at their building setback lines; rather, each lot is 25' wide. As such, a waiver of V.D.2. would be required for approval, and may be appropriate given the unusual configuration of the site resulting from the two abutting subdivisions. However, with regards to access management, the proposed lots should be limited to one shared curb cut to Nguyen Le Drive; and, per previous approvals of abutting lots with similar frontage Lot 1 should be denied access to Knollwood Drive. A note should be required on the Final Plat, if approved, stating both lots are limited to one shared curb cut to Nguyen Le Drive, with any changes in its size, design, or location to be

approved by Traffic Engineering and conform to AASHTO standards. An additional note should state that Lot 1 is denied access to Knollwood Drive.

Each lot meets the minimum size requirements of Section V.D.2. of the Subdivision Regulations regarding lots served by public water and sanitary sewer, and are appropriately labeled with their sizes in square feet and acres. This information should be retained on the Final Plat, if approved; or, a table providing the same information may be furnished on the Final Plat, if approved.

The 25-foot minimum building setback is also illustrated on the preliminary plat along Nguyen Le Drive, but not along Knollwood Drive. As such, revision of the plat to illustrate the 25' minimum building setback line along all street frontages should be required, if approved.

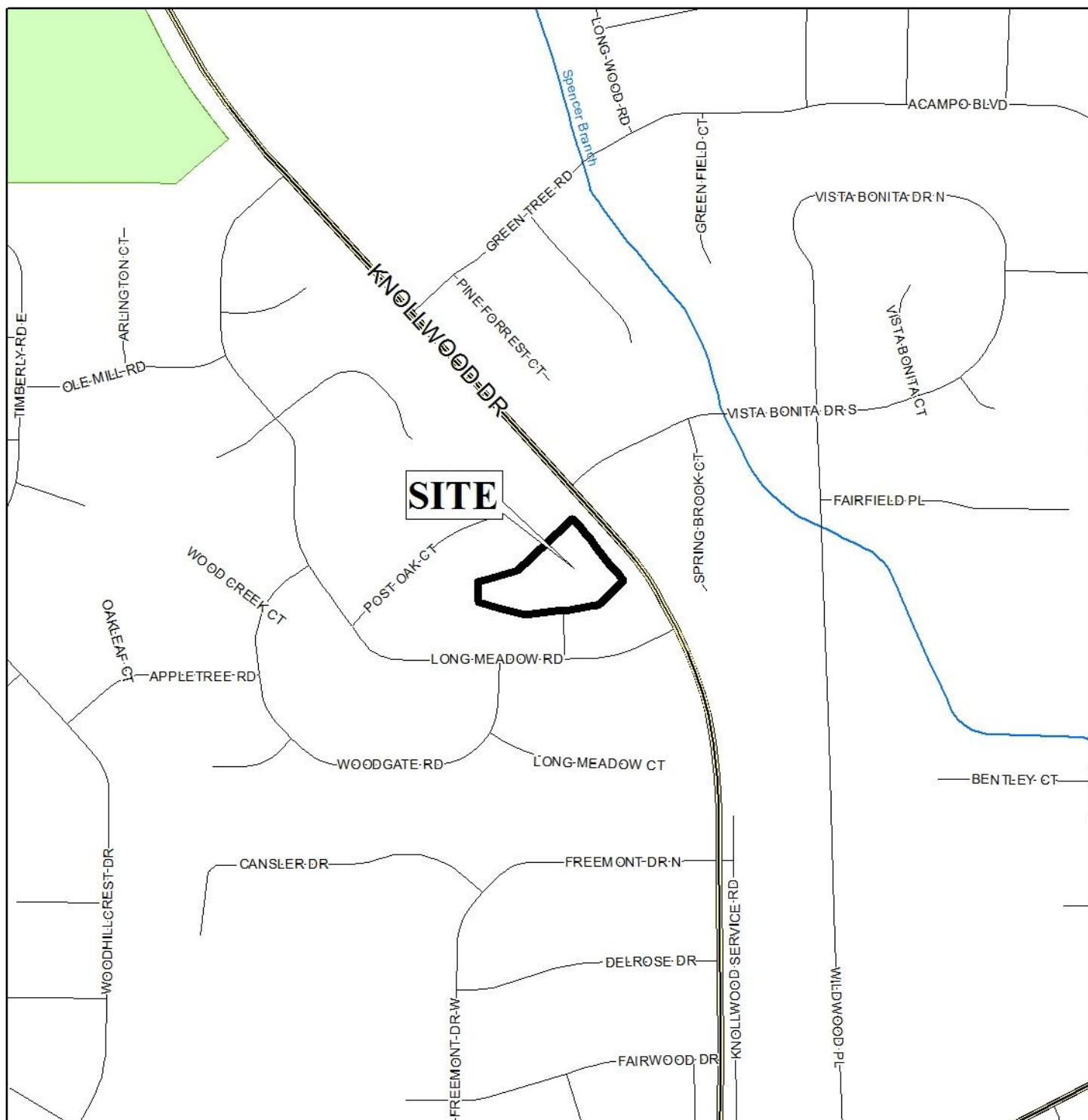
Finally, the site abuts Amberly Subdivision, Unit 1, to the South, and Amberly Subdivision, Unit 1-A, to the North, plats for which were recorded in Mobile County Probate Court on March 6, 1974 and June 5, 1975, respectively. Lots abutting the South property line of the subject site illustrate 7.5' drainage and utility easements; and, 15' drainage and utility easements are illustrated where lots abut the North property line of the subject site. Staff has verified that no vacations of said easements have been granted by the City Clerk's office; therefore, the plat should be revised to illustrate 15' and 7.5' drainage and utility easements along the North and South property lines, respectively, if approved. Also, if approved, a note should be placed on the Final Plat stating no structures will be constructed in any easement.

Based on the preceding, and with a waiver of Section V.D.2. of the Subdivision Regulations, the plat is recommended for Approval, subject to the following conditions:

- 1) placement of a note on the Final Plat stating the lots are limited to one shared curb cut to Nguyen Le Drive, with any changes in its size, design, or location to be approved by Traffic Engineering and conform to AASHTO standards;
- 2) placement of a note on the Final Plat stating Lot 1 is denied access to Knollwood Drive;
- 3) retention of the lot sizes in square feet and acres on the Final Plat, or the furnishing of a table on the Final Plat providing the same information;
- 4) revision of the plat to illustrate the 25' minimum building setback line along all street frontages;
- 5) revision of the plat to illustrate the 15' drainage and utility easement along the North property line of each lot;
- 6) revision of the plat to illustrate the 7.5' drainage and utility easement along the South property line of each lot;
- 7) placement of a note on the Final Plat stating no structures will be constructed in any easement;
- 8) compliance with Engineering comments: (*FINAL PLAT COMMENTS (should be addressed prior to submitting the FINAL PLAT for review and/or signature by the City Engineer): A) Provide all of the required information on the SUBDIVISION PLAT (i.e. signature blocks, signatures, certification statements, written legal description, required notes, legend, scale, bearings and distances) that is required by the current Alabama State Board of Licensure for Professional Engineers and Land Surveyors. B) Add street names to the vicinity map, especially the ones shown on the plat. C) Review and revise the written legal description to match the reference to the page number shown in the*

- description of the Point of Beginning. D) Show and label each and every Right-Of-Way and easement. E) Provide and label the monument set or found at each subdivision corner. F) Add a signature block for the Owner, Notary Public, Planning Commission, Traffic Engineer, and City Engineer. G) Provide the Surveyor's Certificate and Signature. H) Provide the Surveyor's, Owner's (notarized), Planning Commission, and Traffic Engineering signatures. I) Add a note to the SUBDIVISION PLAT stating that a Land Disturbance permit will be required for any land disturbing activity in accordance with Mobile City Code, Chapter 17, Storm Water Management and Flood Control); the City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control. J) Add a note to the SUBDIVISION PLAT stating that as shown on the 1984 aerial photo (FLIGHT 20 - #79) LOTS 1 and 2 will have to share the historical credit of existing (1984) historical credit of impervious area towards stormwater detention requirement per Mobile City Code, Chapter 17, Storm Water Management and Flood Control). Coordinate with City Engineering Department staff to establish the exact amount that each Lot will receive prior to the submittal of the Final Plat from review and signatures. K) Add a note to the Plat stating that the approval of all applicable federal, state, and local agencies (including all storm water runoff, wetland and floodplain requirements) will be required prior to the issuance of a Land Disturbance permit. L) Add a note that sidewalk is required to be constructed, and/or repaired, along the frontage of each lot, or parcel, at time of new development or construction, unless a sidewalk waiver is approved. M) Provide a copy of the FINAL SUBDIVISION PLAT to the Engineering Dept. for review. No signatures are required on this drawing. N) After addressing all of the FINAL SUBDIVISION PLAT review comments by the Engineering Dept. provide the ORIGINAL (with all other signatures) and one (1) copy (signatures not required) of the revised Final Plat to the Engineering Department.);*
- 9) *compliance with Traffic Engineering comments: (Each lot is limited to one curb cut, or one shared curb-cut with appropriated easement, with size, location and design to be approved by Traffic Engineering and conform to AASHTO standards. Lot 1 is denied access to Knollwood Drive.);*
 - 10) *compliance with Urban Forestry comments: (Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 2015-116 and City Code Chapters 57 and 64).);*
 - 11) *compliance with Fire Department comments: (All projects within the City Limits of Mobile shall comply with the requirements of the City of Mobile Fire Code Ordinance (2012 International Fire Code).); and,*
 - 12) *completion of the subdivision process prior to any requests for new construction.*

LOCATOR MAP



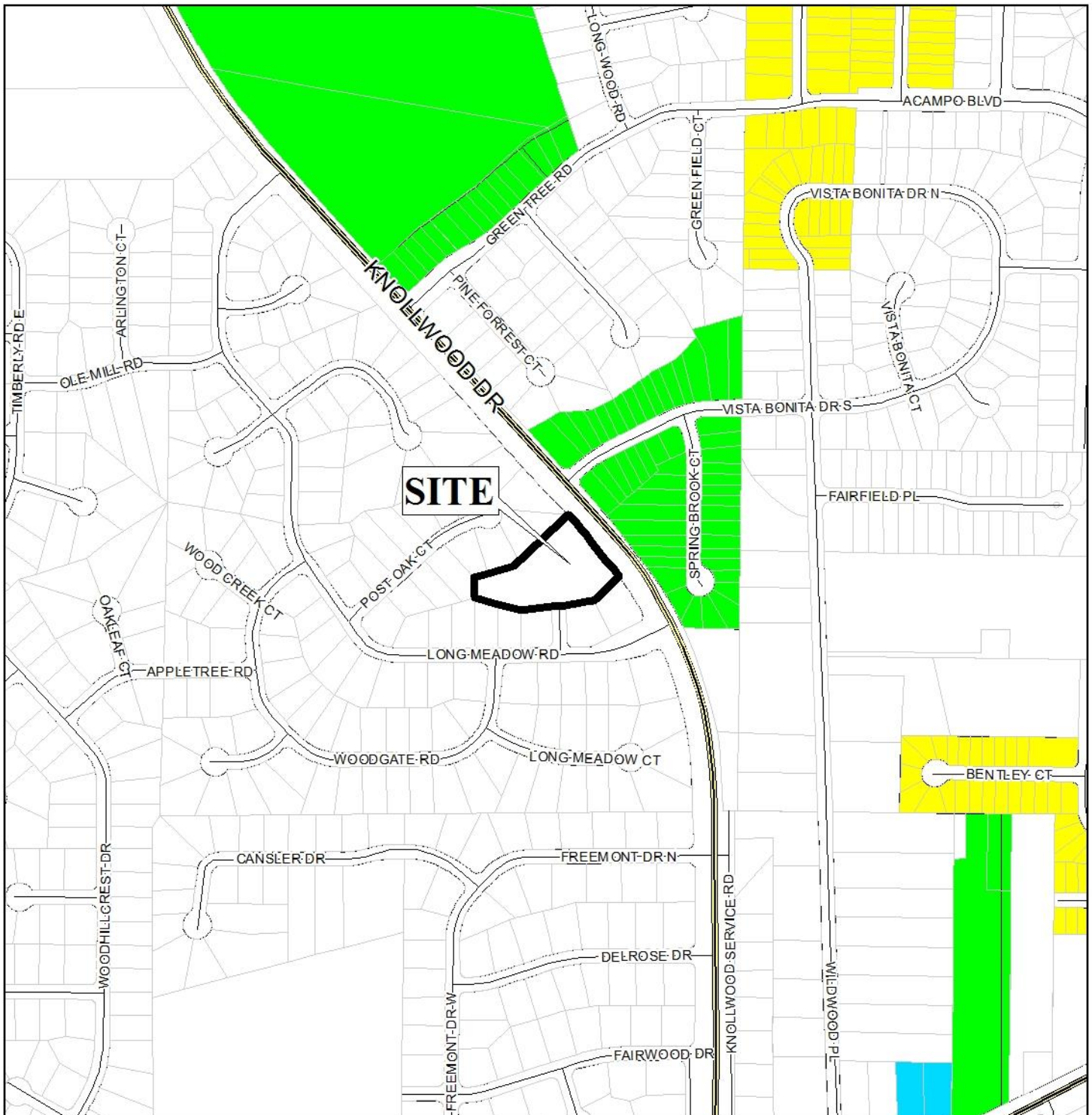
APPLICATION NUMBER 4 DATE April 6, 2017

APPLICANT Nguyen Place Subdivision

REQUEST Subdivision



LOCATOR ZONING MAP



APPLICATION NUMBER 4 DATE April 6, 2017

APPLICANT Nguyen Place Subdivision

REQUEST Subdivision



NGUYEN PLACE SUBDIVISION

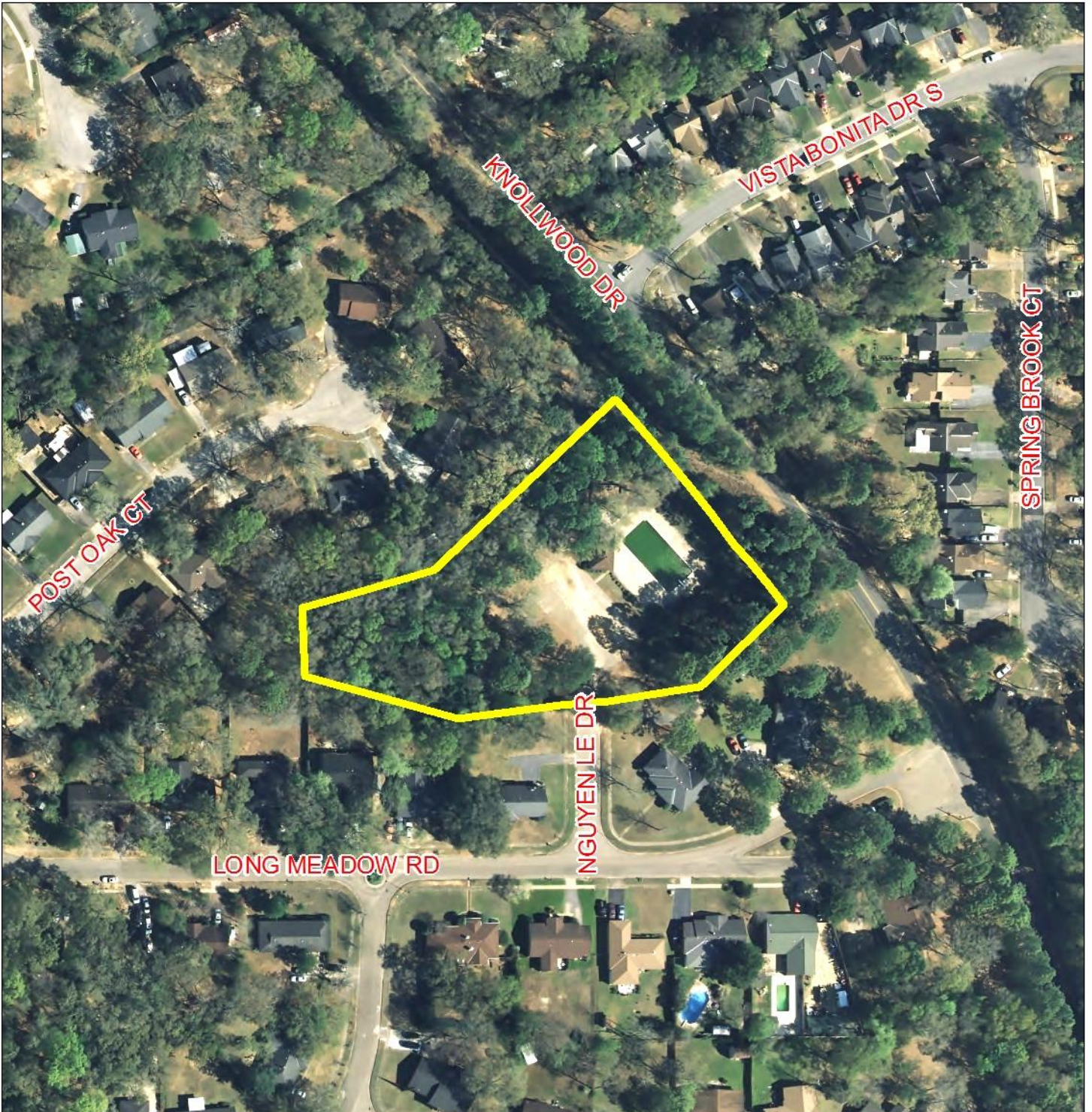


APPLICATION NUMBER 4 DATE April 6, 2017

R-A	R-3	T-B	B-2	B-5	MUN	SD-WH	T5.1
R-1	R-B	B-1	B-3	I-1	OPEN	T3	T5.2
R-2	H-B	LB-2	B-4	I-2	SD	T4	T6



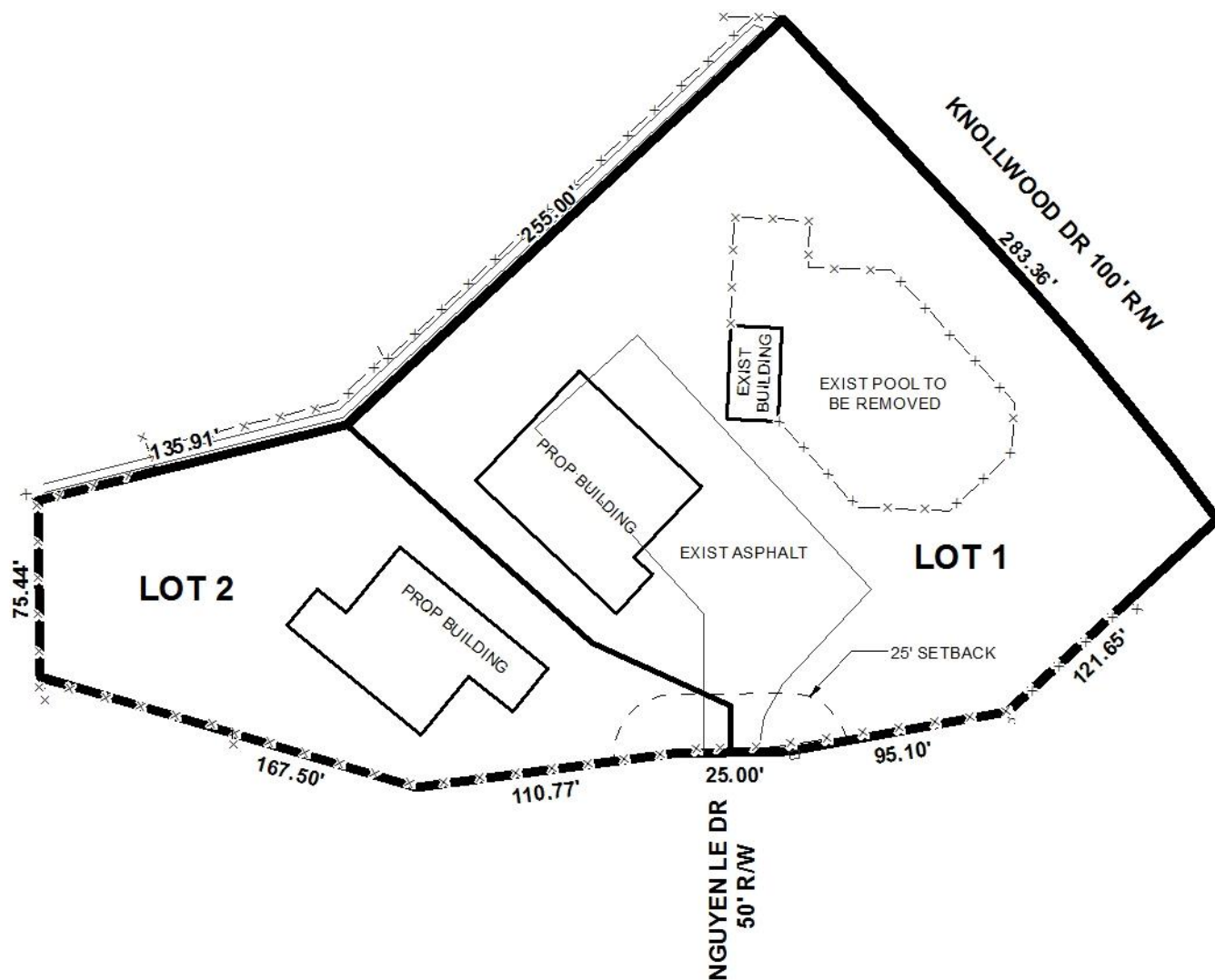
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APPLICATION NUMBER 4 DATE April 6, 2017



DETAIL SITE PLAN



APPLICATION NUMBER 4 DATE April 6, 2017

APPLICANT Nguyen Place Subdivision

REQUEST Subdivision



