

**PLANNED UNIT DEVELOPMENT****Date: May 7, 2009****DEVELOPMENT NAME**

SME Airport, LLC

**LOCATION**

4013 Airport Boulevard  
(South side of Airport Boulevard, 230'± West of Azalea Road, extending to the West side of Azalea Road, 230'± South of Airport Boulevard)

**CITY COUNCIL  
DISTRICT**

City Council District 5

**PRESENT****ZONING DISTRICT**

B-2, Neighborhood Business District

**CONTEMPLATED USE**

Planned Unit Development Approval to allow a truck unloading and trash pickup service yard in an existing parking lot in a B-2, Neighborhood Business District.

**TIME SCHEDULE  
FOR DEVELOPMENT**

None Given

**ENGINEERING  
COMMENTS**

Must comply with all storm water and flood control ordinances. Any work performed in the right of way will require a right of way permit. Drainage from trash loading/unloading areas cannot discharge to storm sewer; must have connection to sanitary sewer.

**TRAFFIC ENGINEERING  
COMMENTS**

Driveway number, size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards. Wall height and planting should be chosen that do not block line of sight for traffic stopped next to the service area. Changes in the sight plan have created two ninety degree turns that most vehicles will be unable to negotiate. The changes also create a situation where trucks entering the service yard do so by driving in the opposing lane in the middle of the two ninety degree turns. Changes need to be made to the plan that will correct these problems.

**URBAN FORESTRY  
COMMENTS**

Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

**FIRE DEPARTMENT****COMMENTS**

All projects must comply with the requirements of the 2003 International Fire Code, including Appendices B through D, as adopted by the City of Mobile, and the 2003 International Existing Building Code, as appropriate.

**REMARKS**

The applicant is seeking Planned Unit Development Approval to allow a truck unloading and trash pickup service yard in an existing parking lot in a B-2, Neighborhood Business District. The site is located in Council District 5.

Planned Unit Development review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating excess traffic along minor residential streets in residential districts outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD.

It is very important to note that Planned Unit Development review is site plan specific; therefore any future changes (parking, structure expansion, etc.,) must be resubmitted for Planned Unit Development review, and be approved through the planning process.

The site plan illustrates the 53,048 square foot retail building with the proposed service yard (completely screened from view) on the Northeast corner of the structure. The applicant states that the use of the service yard will include truck unloading and trash pick up. While several parking spaces will be removed for the addition, the applicant will retain 306 parking spaces (only 177 are required).

As illustrated on the site plan, trucks will enter the service area from the South and exit to the North. This raises two concerns. First, staff is concerned about visibility, as it may be difficult for trucks to anticipate traffic circulation outside of the service area; its location will yield a rather awkward traffic flow (see site plan). Second, the proposed site plan does not appear to be pedestrian friendly, as no sidewalk is illustrated around the service area to access other retail establishments. This will require customers to either walk into traffic aisles, or they will have to drive, which defeats the purpose of an outdoor strip mall.

It should be noted that, currently, there is minimal landscaping and trees on site. Several curbed "islands" exist throughout the parking area that could be landscaped. Particular in parking lots of this size (over 300 spaces), landscaping aids in breaking up the expanse of paving and provides valuable shade for pedestrians and/or vehicles. At a minimum, staff recommends that the applicant landscape all "islands" in the parking lot with grass and understory trees.

The geographic area defined by the city of Mobile and its planning jurisdiction, including this site, may contain Federally-listed threatened or endangered species as well as protected non-game species. Development of the site must be undertaken in compliance with all local, state and Federal regulations regarding endangered, threatened or otherwise protected species.

Based on the preceding, it is recommended that this application be held over until the June 4<sup>th</sup> meeting to allow the applicant to address the following:

- 1) provision of a sidewalk around the service area;
- 2) provision of landscaping and understory trees throughout the parking lot, specifically on the existing curbed islands;
- 3) compliance with City Engineering comments: *(Must comply with all storm water and flood control ordinances. Any work performed in the right of way will require a right of way permit. Drainage from trash loading/unloading areas cannot discharge to storm sewer; must have connection to sanitary sewer);*
- 4) compliance with Traffic Engineering comments: *(Driveway number, size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards. Wall height and planting should be chosen that do not block line of sight for traffic stopped next to the service area);* and
- 5) full compliance with all municipal codes and ordinances.

**Revised for the May 21<sup>st</sup> meeting:**

Due to a lack of quorum, the Commission was unable to vote on this application at the May 7<sup>th</sup> meeting.

Nevertheless, the applicant has submitted a revised site plan addressing the concerns expressed by staff. However, while sidewalk, visibility and trees have been addressed, there is still an issue with regard to circulation. As stated by Traffic Engineering, “changes in the sight plan have created two ninety degree turns that most vehicles will be unable to negotiate. The changes also create a situation where trucks entering the service yard do so by driving in the opposing lane in the middle of the two ninety degree turns. Changes need to be made to the plan that will correct these problems.”

It should be further noted that the applicant has an alternative design that will, in fact, comply with the Zoning Ordinance and not require a Planned Unit Development application.

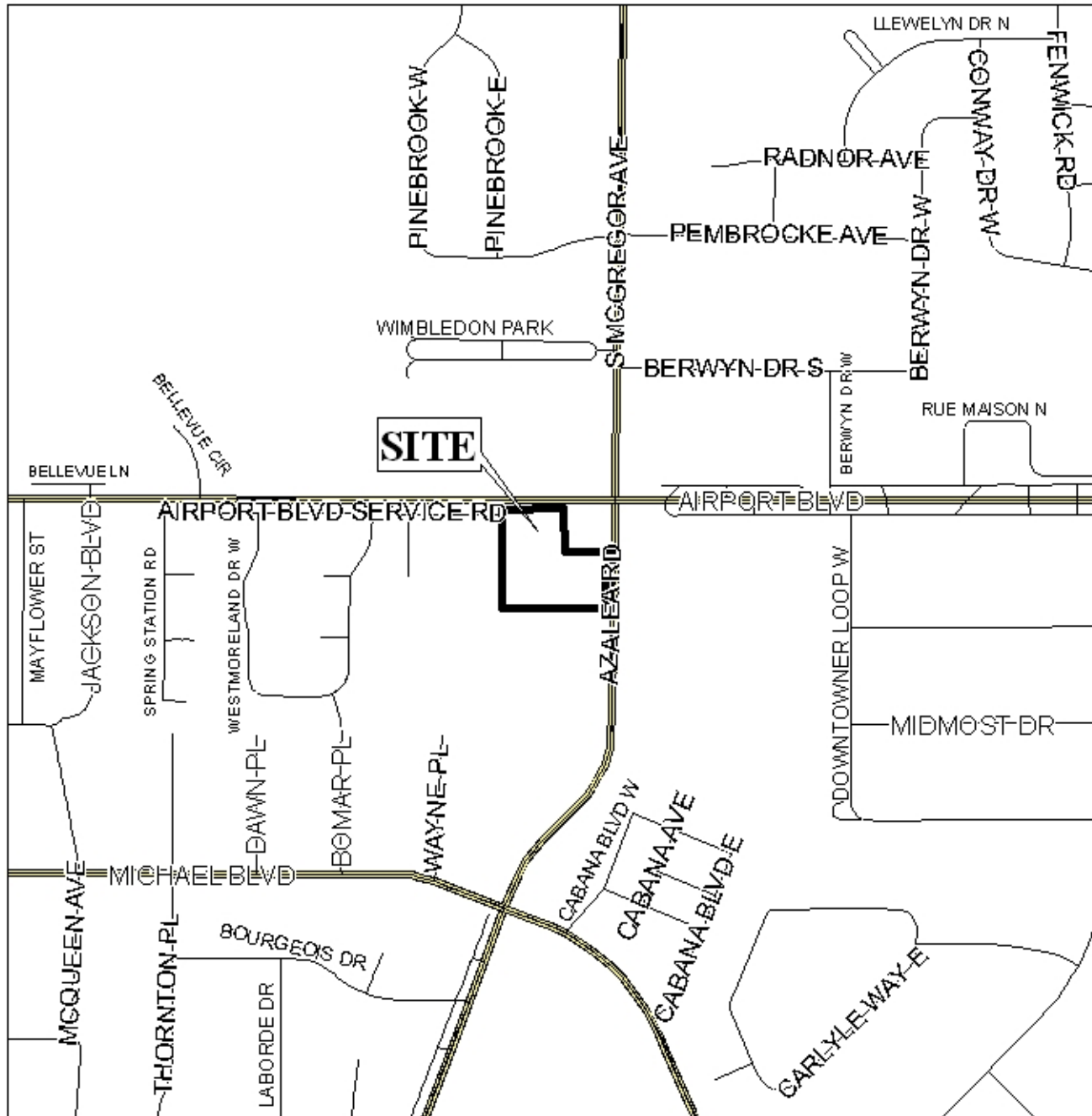
Based on the preceding, it is recommended that this application be held over until the June 18<sup>th</sup> meeting to allow the applicant to address the following:

- 1) compliance with City Engineering comments: *(Must comply with all storm water and flood control ordinances. Any work performed in the right of way will require a right of way permit. Drainage from trash loading/unloading areas cannot discharge to storm sewer; must have connection to sanitary sewer);*
- 2) compliance with Traffic Engineering comments: *(Driveway number, size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards. Wall height and planting should be chosen that do not block line of sight for traffic stopped next to the service area. Changes in the sight plan have created two ninety degree turns that most vehicles will be unable to negotiate. The changes*

*also create a situation where trucks entering the service yard do so by driving in the opposing lane in the middle of the two ninety degree turns. Changes need to be made to the plan that will correct these problems); and*

- 3) full compliance with all municipal codes and ordinances.

# LOCATOR MAP



APPLICATION NUMBER 4 DATE May 21, 2009  
 APPLICANT SME Airport, LLC  
 REQUEST Planned Unit Development



# PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The site is surrounded by commercial land use.

APPLICATION NUMBER 4 DATE May 21, 2009

APPLICANT SME Airport, LLC

REQUEST Planned Unit Development

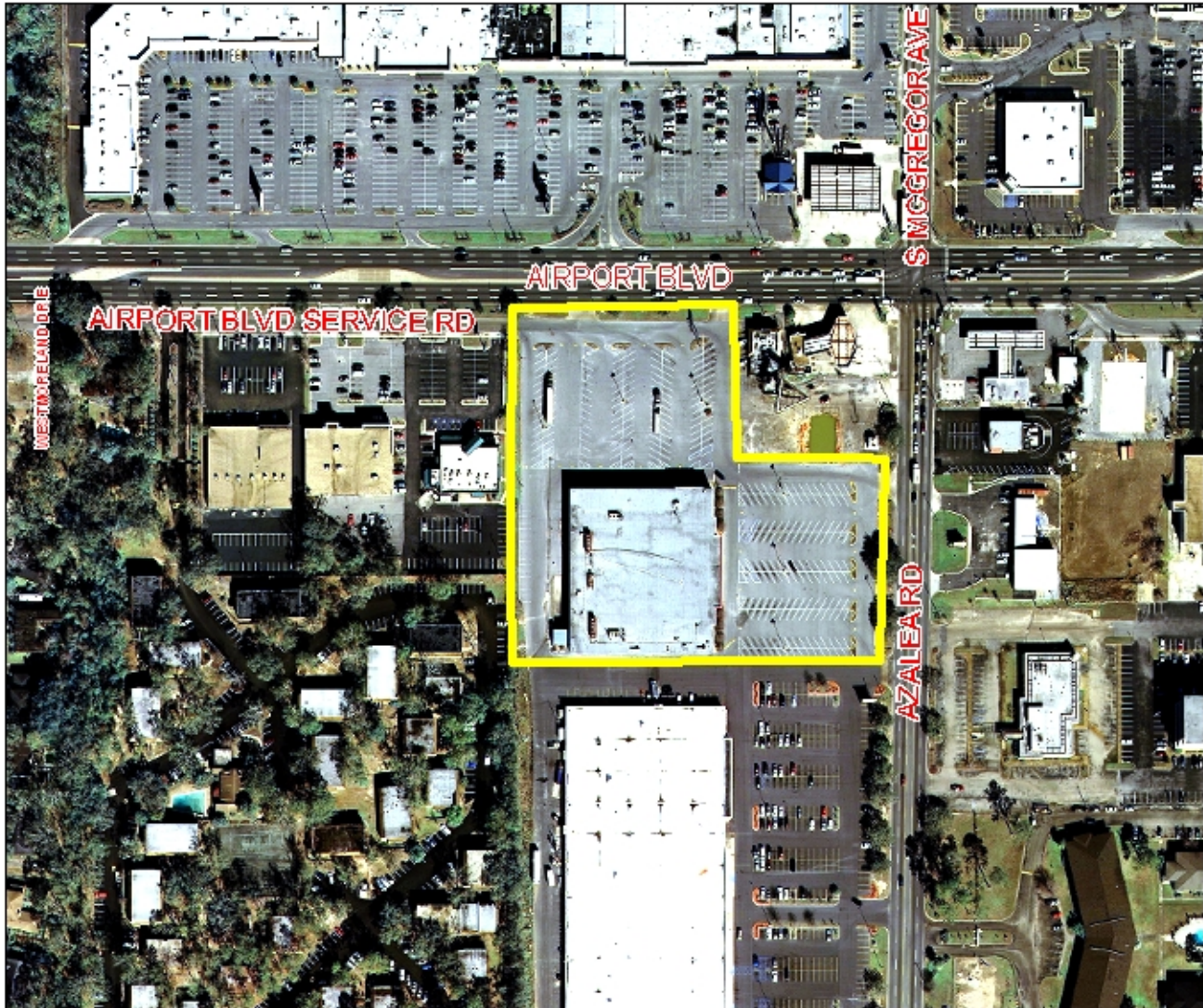
LEGEND 

R-1	R-2	R-3	R-4	R-B	H-B	T-B	B-1	LB-2	B-2	B-3	B-4	B-5	I-1	I-2
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## PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The site is surrounded by commercial land use.

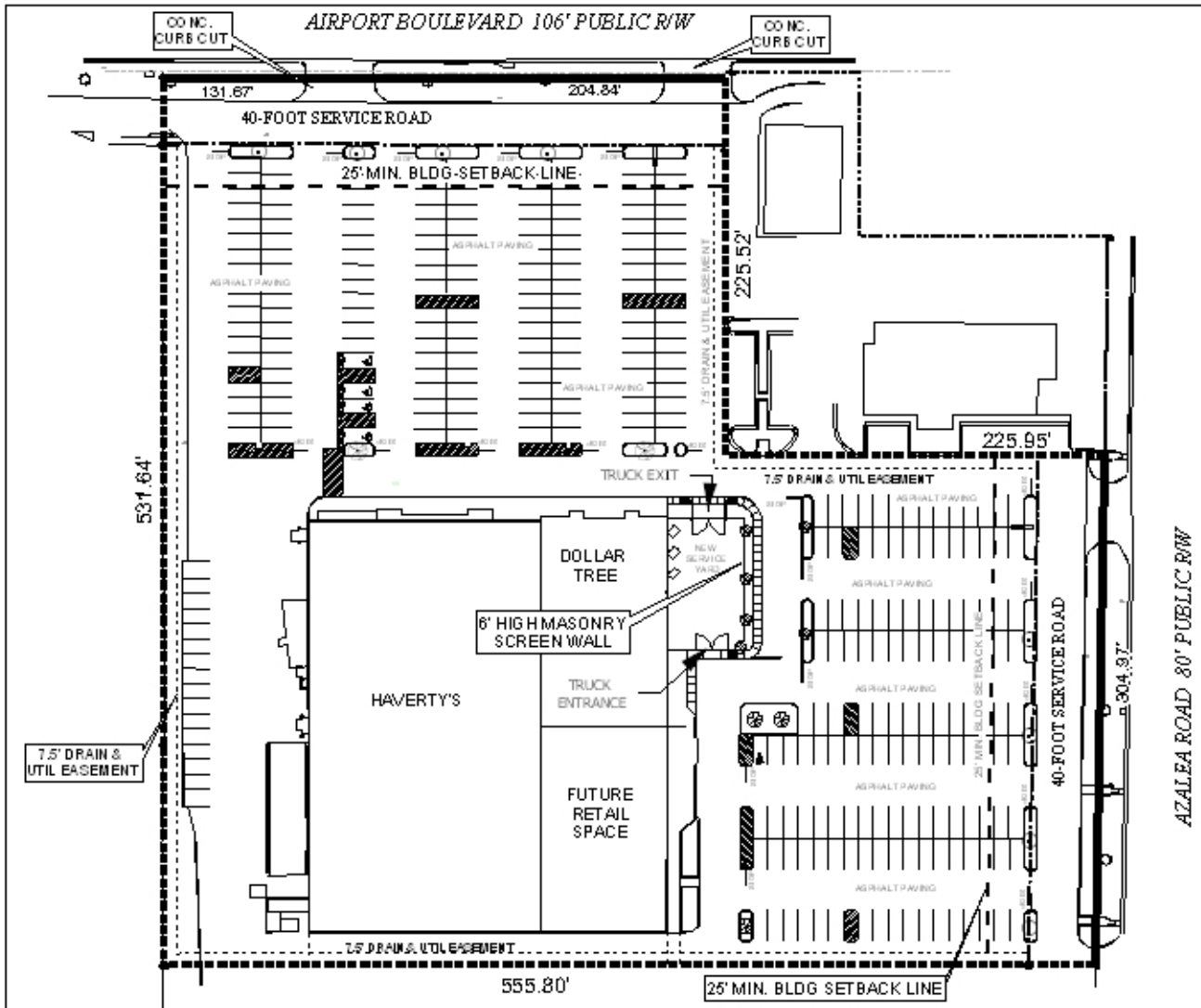
APPLICATION NUMBER 4 DATE May 21, 2009

APPLICANT SME Airport, LLC

REQUEST Planned Unit Development

N  
NTS

# SITE PLAN



The site plan illustrates the existing buildings and proposed improvements.

APPLICATION NUMBER 4 DATE May 21, 2009  
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 REQUEST Planned Unit Development

N  
 NTS