

RANGELINE CROSSING SUBDIVISION

Engineering Comments: The following comments should be addressed prior to acceptance and signature by the City Engineer: A. Provide additional line weight to the exterior boundary lines and the metes and bounds labels of the proposed subdivision. B. Revise the west end of the north line of the proposed 60 ft. ROW between Common Area No. 1 and No. 2. Maintain a constant 60 foot width by removing the proposed 100.82' long segment and curve C22 (or as approved by the City Engineer and Traffic Engineer). C. Provide the Surveyor's Certificate and Signature. D. Provide the Surveyor's, Owner's (notarized), Planning Commission, and Traffic Engineering signatures. E. Add a note that sidewalk is required to be constructed along the frontage of each lot, or parcel, at time of development, and, along each common area with the construction of the subdivision roadway, unless a sidewalk waiver is approved.

Traffic Engineering Comments: Each lot is limited to two curb-cuts per street frontage (including future right-of-way) with size, location and design to be approved by ALDOT (where applicable) and Traffic Engineering and conform to AASHTO standards.

Fire Department Comments: All projects within the City of Mobile Fire Jurisdiction must comply with the requirements of the 2009 International Fire Code, as adopted by the City of Mobile.

MAWSS Comments: No comments.

Urban Forestry Comments: Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

The plat illustrates the proposed 2 lot, 82.7± acre subdivision, which is located at the Northeast corner of Rangeline Service Road North and Halls Mill Road, extending to the Northwest corner of Rangeline Service Road North and Demetropolis Road, in Council District 4. The applicant states the subdivision is served by public water and sanitary sewer services.

The purpose of this application is to resubdivide four existing legal lots of record and a Future Development parcel into two legal lots of record and create a public right-of-way.

The entire site was the subject of Halls Mill Crossing Subdivision, approved by the Commission in November, 2008, when the site was then in the County prior to annexation. That subdivision created four legal lots encompassing approximately seven acres, with the remainder of the site as a Future Development tract. In October, 2013, the Commission approved a four-lot subdivision for Rangeline Crossing Subdivision, similar to this one, but the plat for that version was not signed due to modifications to the internal street which were different from what was approved by the Commission. The applicant now proposes to obtain approval of the street modifications. The proposed subdivision would eliminate the four recorded lots of Halls Mill Crossing Subdivision and include all of the Future Development area in the two new lots encompassing the entire site.

The site fronts Halls Mill Road, Rangeline Service Road North, and Demetropolis Road. Halls Mill Road is a collector street with a required 70' right-of-way along a portion of the frontage. Dedication from the recording of Halls Mill Crossing Subdivision provided 35' from the centerline along the frontages of two of the recorded lots, but none along the Future Development frontage where the 50' right-of-way remained. Therefore, dedication would be required along Halls Mill Road to provide 35' from the centerline within that area and is so indicated on the preliminary plat. Rangeline Road is a component of the Major Street with a planned 300' right-of-way width. The plat indicates a current compliant 410' right-of-way along Rangeline Road; therefore, no dedication would be required. Demetropolis Road is a minor street without curb and gutter requiring a 60' right-of-way. The plat indicates dedication to provide a compliant 30' from the centerline of Demetropolis Road.

Each lot would have over 200' of street frontage along all street frontages, including the internal street. As per the Traffic Engineering comments, a note should be required on the Final Plat stating that each lot is limited to two curb cuts per street frontage, including the future right-of-way, with the size, location, and design of all curb cuts to be approved by Traffic Engineering and ALDOT (where applicable) and conform to AASHTO standards.

A proposed internal street with a minimum 60' right-of-way is indicated on the plat with a cul-de-sac terminus exceeding the minimum radius of 60' as required by Section V.B.14. of the Subdivision Regulations. As the street is over 600' in length, a waiver of Section V.B.6. of the Subdivision Regulations would be required. The proposed street would need to be constructed to City standards prior to signing of the Final Plat.

The plat illustrates the 25' minimum building setback line along Demetropolis Road as measured from the required frontage dedication. However, there is a 15' water and sewer easement slightly further within the property along that street frontage. Therefore, the plat should be revised to illustrate a minimum building setback line along Demetropolis Road as necessary to also include this easement and measured from the required dedication. As shown in other areas of the site, the 25' minimum building setback line should also be illustrated along all street frontages as measured from any required right-of-way dedication. The plat should be revised to also illustrate the 25' minimum building setback line along the future right-of-way connection between the cul-de-sac and the Demetropolis Road/Rangeline Road intersection.

As on the preliminary plat, the lot sizes should be labeled on the Final Plat in both square feet and acres, after any required right-of-way dedication, or a table should be furnished on the Final Plat providing the same information.

Three common areas are indicated on the plat. As on the preliminary plat, a note should be required on the Final Plat stating that the maintenance of the common areas is the responsibility of the property owners.

The geographic area defined by the City of Mobile and its planning jurisdiction, including this site, may contain Federally-listed threatened or endangered species as well as protected non-game species. Development of the site must be undertaken in compliance with all local, state and Federal regulations regarding endangered, threatened or otherwise protected species.

Based upon the preceding, and with a waiver of Section V.B.6. of the Subdivision Regulations, this application is recommended for Tentative Approval, subject to the following conditions:

- 1) dedication of sufficient right-of-way to provide 35' from the centerline of Halls Mill Road along Lot 1 where no previous dedication was recorded;
- 2) dedication of sufficient right-of-way to provide 30' from the centerline of Demetropolis Road;
- 3) placement of a note on the Final Plat stating that each lot is limited to two curb cuts per street frontage, including the future right-of-way, with the size, location, and design of all curb cuts to be approved by Traffic Engineering and ALDOT (where applicable) and conform to AASHTO standards;
- 4) construction of the internal street to City standards prior to signing the Final Plat;
- 5) revision of the plat to illustrate a minimum building setback line along Demetropolis Road as necessary to also include the 15' water and sewer easement and measured from any required dedication;
- 6) illustration of the 25' minimum building setback line along all street frontages as measured from any required right-of-way dedication;
- 7) revision of the plat to illustrate a 25' minimum building setback line along the future right-of-way connection between the cul-de-sac and the Demetropolis Road/Rangeline Road intersection;
- 8) labeling of each lot with its square feet and acres, after any required right-of-way dedication, or the furnishing of a table on the Final Plat providing the same information;
- 9) placement of a note on the Final Plat stating that the maintenance of the common areas is the responsibility of the property owners;
- 10) placement of a note on the Final Plat stating that development of the site must be undertaken in compliance with all local, state and Federal regulations regarding endangered, threatened or otherwise protected species;
- 11) subject to the Engineering comments: *(The following comments should be addressed prior to acceptance and signature by the City Engineer: A. Provide additional line weight to the exterior boundary lines and the metes and bounds labels of the proposed subdivision. B. Revise the west end of the north line of the proposed 60 ft. ROW between Common Area No. 1 and No. 2. Maintain a constant 60 foot width by removing the proposed 100.82' long segment and curve C22 (or as approved by the City Engineer and Traffic Engineer). C. Provide the Surveyor's Certificate and Signature. D. Provide the Surveyor's, Owner's (notarized), Planning Commission, and Traffic Engineering signatures. E. Add a note that sidewalk is required to be constructed along the frontage of each lot, or parcel, at time of development, and, along each common area with the construction of the subdivision roadway, unless a sidewalk waiver is approved);*
- 12) subject to the Traffic Engineering comments: *(Each lot is limited to two curb-cuts per street frontage (including future right-of-way) with size, location and design to be approved by ALDOT (where applicable) and Traffic Engineering and conform to AASHTO standards.);*
- 13) subject to the Urban Forestry comments: *[Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64); and*
- 14) subject to the Fire Department comments: *(All projects within the City of Mobile Fire Jurisdiction must comply with the requirements of the 2009 International Fire Code, as adopted by the City of Mobile.)*

Revised for the March 20th meeting:

This application was heldover from the March 6th meeting due to a lack of quorum to vote on the application. There have been no revisions to the application; however, one aspect of the site not originally commented on is now included and an additional condition placed upon the approval.

The site contains a very small NWI wetlands area. The presence of wetlands indicates that the area may be environmentally sensitive; therefore, the approval of all applicable federal, state and local agencies would be required prior to the issuance of any permits or land disturbance activities.

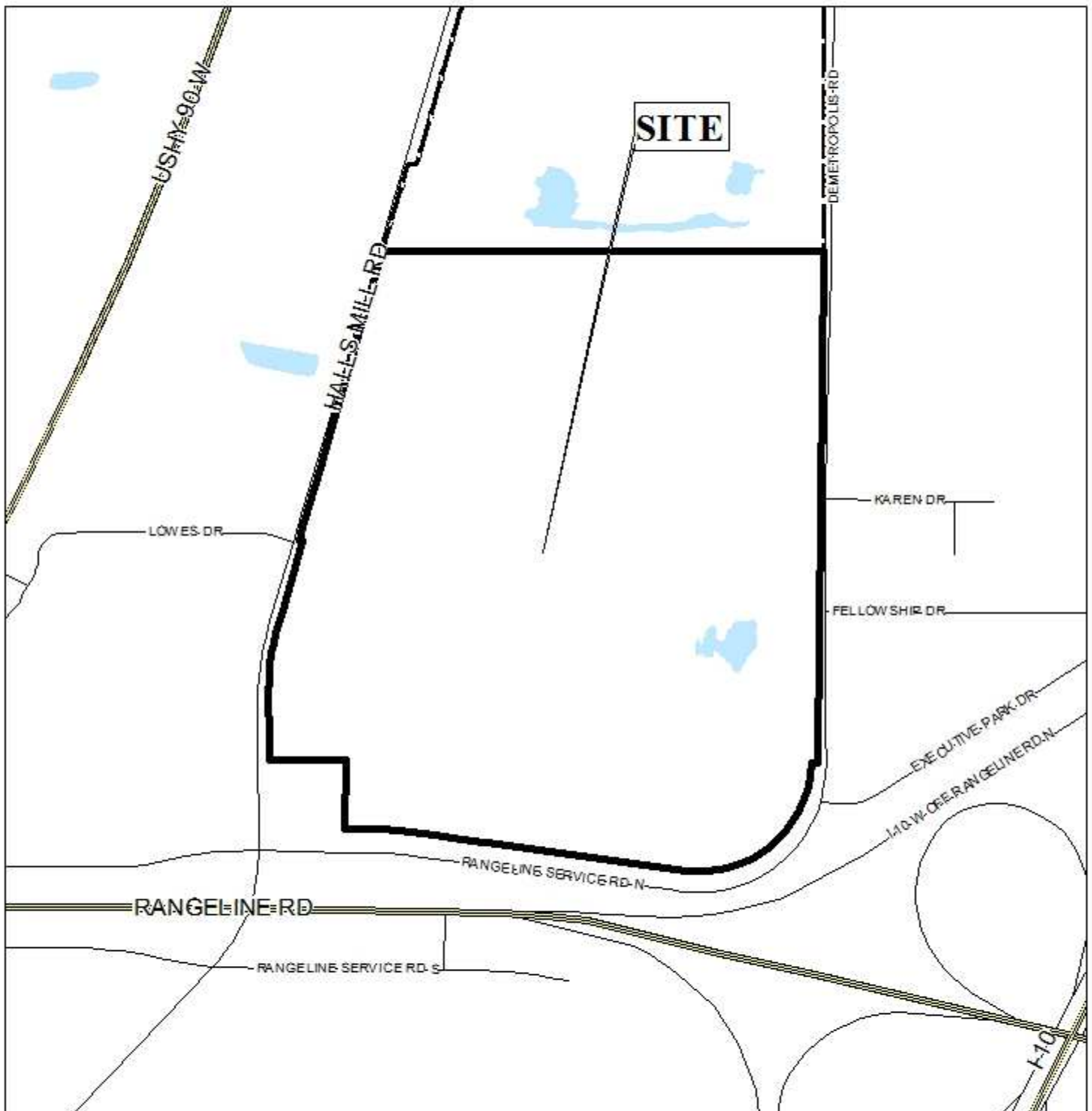
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- 5) revision of the plat to illustrate a minimum building setback line along Demetropolis Road as necessary to also include the 15' water and sewer easement and measured from any required dedication;*
- 6) illustration of the 25' minimum building setback line along all street frontages as measured from any required right-of-way dedication;*
- 7) revision of the plat to illustrate a 25' minimum building setback line along the future right-of-way connection between the cul-de-sac and the Demetropolis Road/Rangeline Road intersection;*
- 8) labeling of each lot with its square feet and acres, after any required right-of-way dedication, or the furnishing of a table on the Final Plat providing the same information;*
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LOCATOR MAP



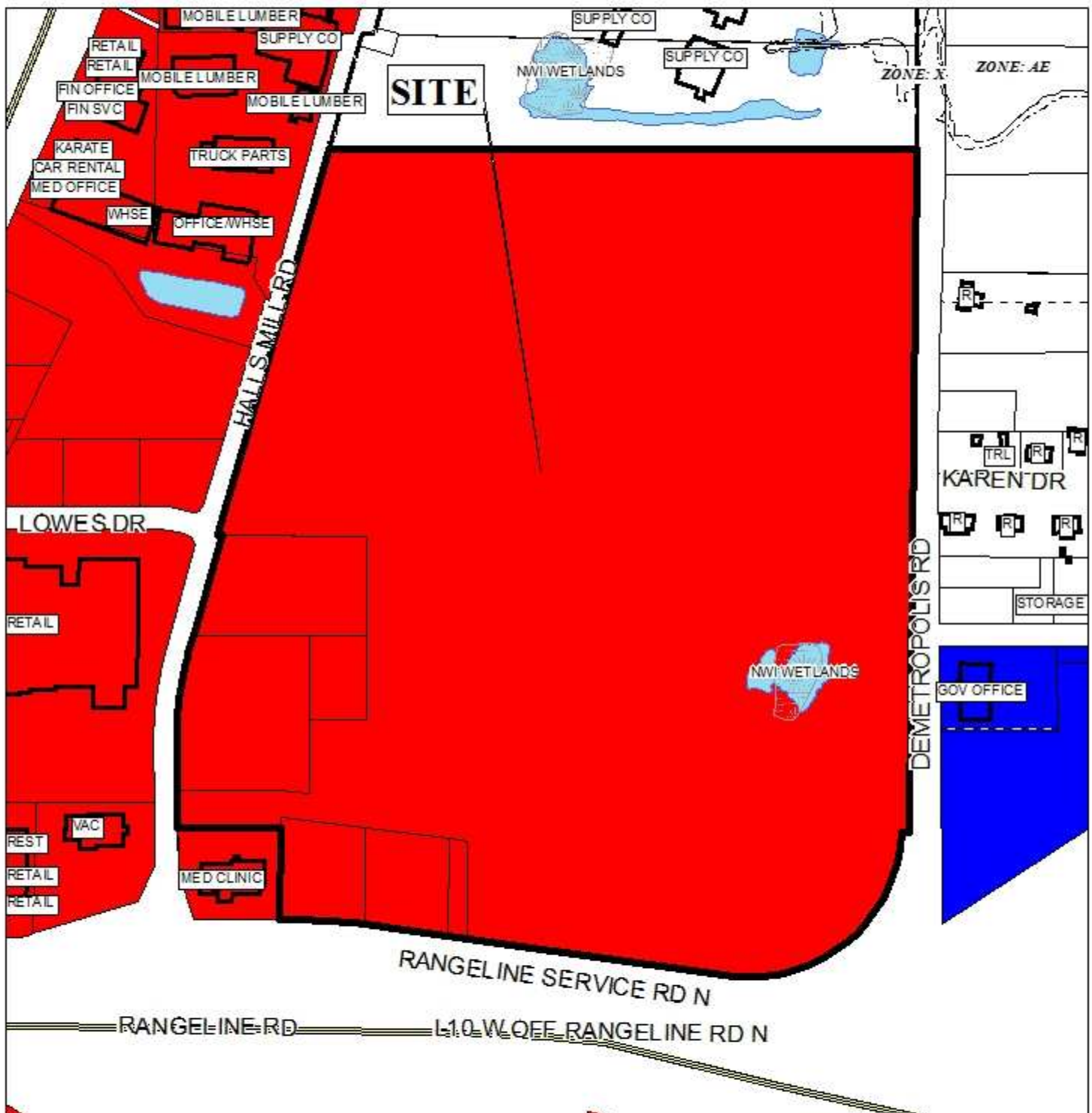
APPLICATION NUMBER 4 DATE March 20, 2014

APPLICANT Rangeline Crossing Subdivision

REQUEST Subdivision



RANGELINE CROSSING SUBDIVISION



APPLICATION NUMBER 4 DATE March 20, 2014

LEGEND

R-1	R-2	R-3	R-A	R-B	H-B	T-B	B-1	LB-2	B-2	B-3	B-4	B-5	I-1	I-2
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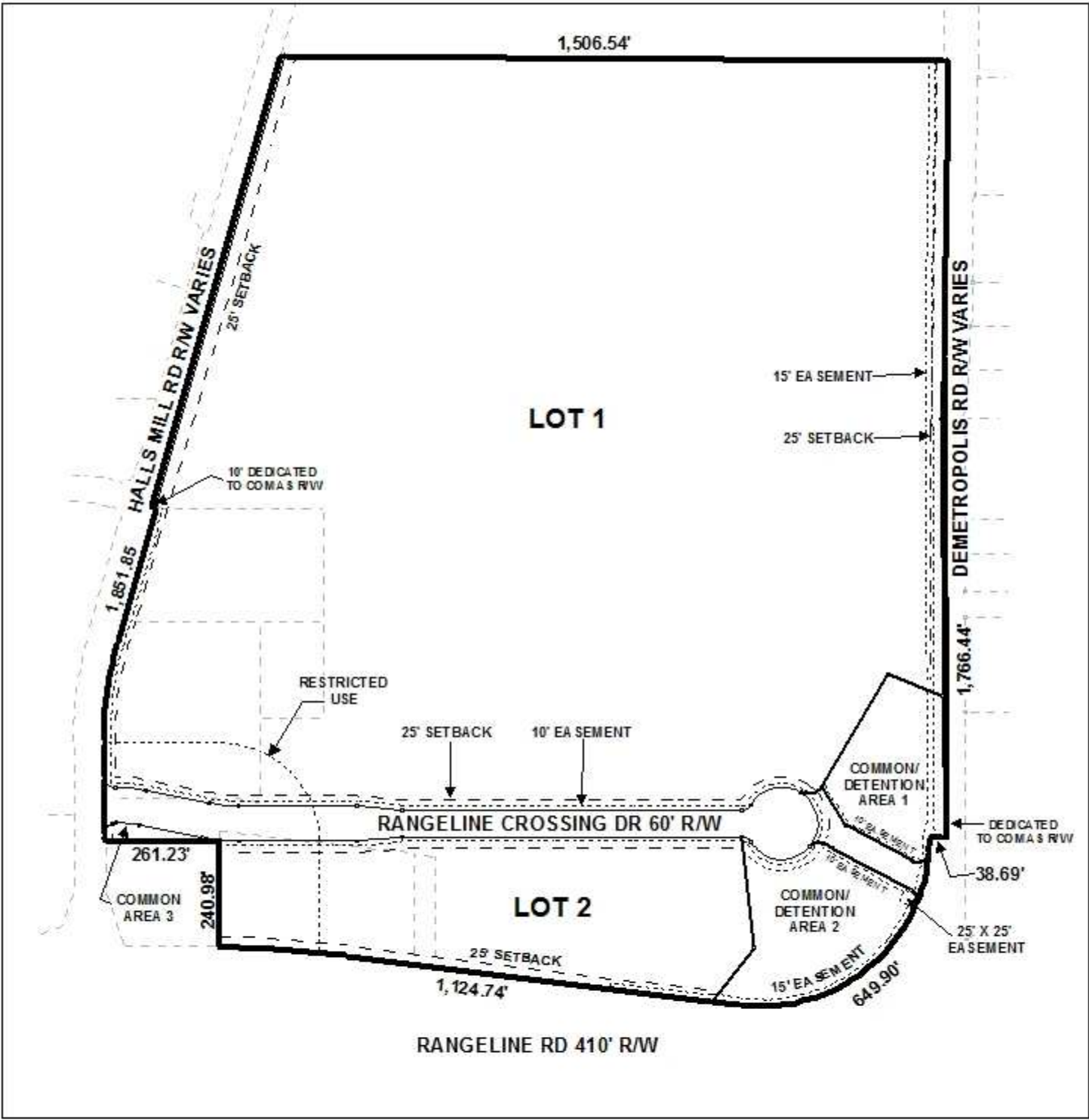
RANGELINE CROSSING SUBDIVISION



APPLICATION NUMBER 4 DATE March 20, 2014



DETAIL SITE PLAN



APPLICATION NUMBER 4 DATE March 20, 2014

APPLICANT Rangeline Crossing Subdivision

REQUEST _____ Subdivision _____

