

**PLANNED UNIT DEVELOPMENT
STAFF REPORT**

Date: April 16, 2020

DEVELOPMENT NAME Casey Pipes

LOCATION 3250 Airport Boulevard and 325 East I-65 Service Road South
(Northeast corner of Airport Boulevard and East I-65 Service Road South)

CITY COUNCIL DISTRICT District5

PRESENT ZONING B-3, Community Business District

AREA OF PROPERTY 2 Lots / 58.1 ± Acres (*excludes Sam’s Club site*)

CONTEMPLATED USE Planned Unit Development to amend a previously approved Planned Unit Development to allow shared access and parking between multiple building sites and allow a unique sign package for the development.

TIME SCHEDULE FOR DEVELOPMENT Immediate

ENGINEERING COMMENTS

1. According to the FEMA flood map information, this property is located within a Special Flood Hazard Area. You will need to show and label the flood hazard area(s) on your PUD Site Plan. Also, you will need to list the Minimum Finished Floor Elevation (MFFE).
2. ADD THE FOLLOWING NOTES TO THE PUD SITE PLAN:
 - a. Any work performed in the existing Public ROW (right-of-way or drainage easement) such as driveways, sidewalks, utility connections, grading, drainage, irrigation, or landscaping will require a ROW permit from the City of Mobile Engineering Department (208-6070) and must comply with the City of Mobile Right-of-Way Construction and Administration Ordinance (Mobile City Code, Chapter 57, Article VIII).
 - b. A Land Disturbance Permit application shall be submitted for any proposed land disturbing activity with the property. A complete set of construction plans including, but not limited to, drainage, utilities, grading, storm water detention systems and paving will need to be included with the Land Disturbance permit. This Permit must be submitted, approved, and issued prior to beginning any of the construction work.
 - c. Any and all proposed land disturbing activity within the property will need to be submitted for review and be in conformance with Mobile City Code, Chapter 17, Storm Water Management and Flood Control); the City of Mobile, Alabama Flood

Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control.

- d. Any existing or proposed detention facility shall be maintained as it was constructed and approved. The Land Disturbance Permit application for any proposed construction includes a requirement of a Maintenance and Inspection Plan (signed and notarized by the Owner) for the detention facility. This Plan shall run with the land and be recorded in the County Probate Office prior to the Engineering Department issuing their approval for a Final Certificate of Occupancy.
- e. The approval of all applicable federal, state, and local agencies (including all storm water runoff, wetland and floodplain requirements) will be required prior to the issuance of a Land Disturbance permit. The Owner/Developer is responsible for acquiring all of the necessary permits and approvals.
- f. The proposed development must comply with all Engineering Department design requirements and Policy Letters.

TRAFFIC ENGINEERING

COMMENTS

Applicant has performed a Traffic Impact Study (TIS) and has arrived at several recommendations that are acceptable to Traffic Engineering as follows:

- 1) Upon resurfacing of the parking lot, remove the speed bumps along the Backage Road on the northeast side of the property. Speed lumps adjacent to the building can remain.
- 2) Install SPEED LIMIT 15 MPH signs along the Backage Road at four locations defined in the TIS.
- 3) Close the redundant/unnecessary access points along the southern side of the property by removing asphalt pavement and installing curb and landscaping. Sites to be closed are defined in Figures 13 and 14 in the TIS.
- 4) Restripe yield lines along E I-65 Service Road at 2 locations.
- 5) Install yield lines on the eastbound and westbound approaches to Study Intersection 3 (as defined in Figure 14 of the TIS).

Size, location, and design of any new cut, or changes to existing cuts, to be approved by Traffic Engineering and conform to AASHTO standards. All on-site parking, including ADA handicap spaces, shall meet the minimum standards as defined in Section 64-6 of the City’s Zoning Ordinance.

URBAN FORESTRY

COMMENTS

Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 2015-116 and City Code Chapters 57 and 64). Private removal of trees in the right-of-way will require approval of the Mobile Tree Commission. Removal of heritage trees from a commercial site will require a tree removal permit.

FIRE DEPARTMENT

COMMENTS

All projects within the City Limits of Mobile shall comply with the requirements of the City of Mobile Fire Code Ordinance. (2012 International Fire Code)

REMARKS

The applicant is requesting Planned Unit Development approval to amend a previously approved Planned Unit Development to allow shared access and parking between multiple building sites and allow a unique sign package for the development.

The site has been before the Planning Commission numerous times for Planned Unit Development review, with the first appearance in 1986. The last appearance before the Commission was in 2003, coinciding with the proposed development of the Sam's Club main building and gas station. Numerous Administrative Planned Unit Developments have also occurred throughout this time, all addressing issues associated with the numerous out parcels associated with the overall mall development.

The site has been given a District Center land use designation, per the Future Land Use Plan and Map, adopted on May 18, 2017 by the Planning Commission. The Future Land Use Plan and Map complements and provides additional detail to the Development Framework Maps in the Map for Mobile, adopted by the Planning Commission at its November 5, 2015 meeting.

This designation applies across the city to larger areas of existing mixed-use character or where such character is encouraged. These areas will include moderate to high-density residential (minimum densities of 6 dwelling units per acre) in dynamic, horizontal or vertical mixed use environments, to provide a balance of housing and employment.

District Centers generally serve several surrounding neighborhoods and may even have a city-wide or region-wide reach. As such, they are often anchored by a major commercial or institutional employer such as a shopping mall or a medical center.

Depending on location and assigned zoning, residential areas in District Centers may incorporate a mix of housing types, ranging from mid-rise multifamily buildings containing apartments and lofts, to townhouses and detached single-family homes. Major civic cultural institutions and public spaces provide regional and neighborhood destinations.

District Centers should be designed to induce pedestrian activity, with high quality streetscapes connecting the different components of a center as well as the center to its surrounding area. DC districts may be served by transit and include development of an intensity and design that supports transit use.

It should be noted that the Future Land Use Plan and Map components of the Map for Mobile Plan are meant to serve as a general guide, not a detailed lot and district plan. In many cases the designation on the new Future Land Use Map may match the existing use of land, but in others the designated land use may differ from what is on the ground today. As such, the Future Land Use Plan and Map allows the Planning Commission and City Council to consider individual cases based on additional information such as the classification request, the surrounding development, the timing of the request, and the appropriateness and compatibility of the proposed use and, where applicable, the zoning classification.

Planned Unit Development review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating

excess traffic along minor residential streets in residential districts outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD. PUD approval is site plan specific, thus if any new construction is anticipated that will change an approved site plan, an application to amend an existing, approved PUD must be made prior to any construction activities. PUD approvals expire within one (1) year if no permits for the development are obtained.

In pursuit of these purposes, the objectives to be met by a PUD are as follows: a) to encourage innovative and diversified design in building form and site development; b) flexibility, to permit greater flexibility in the location and arrangement of buildings and uses than is generally possible under district regulations; c) to encourage the most efficient and sustainable use of land, especially tracts in the inner part of the city that remain undeveloped or that are appropriate for re-development; d) to preserve and protect as urban amenities the natural features and characteristics of land; e) to encourage the provision of common open space through efficient site design; and, f) to encourage optimum use of available public utilities, streets and community facilities.

As previously stated, the last appearance of the site before the Planning Commission was in 2003, to accommodate the future Sam's Club development. The applicant now proposes to demolish portions of existing buildings, to allow for an increase in the number of parking spaces and landscape area, as well as make modifications to other buildings within the development. A sign package is also proposed for the development. The applicant has, however, excluded the Sam's Club development (*included in the 2003 PUD*), as well as two other outparcels (*Raising Caines, Administrative PUD 2006, which uses parking and access in the Springdale Mall for compliance, and the ALLTEL Building, Administrative PUD 2004, allowing shared access*), from the PUD application at hand.

The applicant provided the following narrative regarding their proposal:

Description of Contemplated Use and Character of Improvements of Property:

The site is improved as a retail shopping center with multiple tenants, and this is not changing. It was originally developed as an interior shopping mall, but over time it has been repurposed as a multi-tenant shopping center without interior hallways for store access. This means that there are larger and fewer tenants on the shopping center now than when it was originally constructed and contemplated. The shopping center then had only large retail tenants with outside customer access. As the market for retail has continued to evolve, we are now transitioning the site to smaller tenant spaces, but still with outside customer access.

The first of the modifications requested in this application is to demolish approximately 55,919 SF of the existing building and to replace it with improved truck circulation areas, parking areas, and landscaped areas. This area of work is shown on the attached

site plan and is located generally to the East of the Sam's Club store. The second modification is to increase the number of available parking spaces from 3,469 to 3,513. These spaces are primarily found in this same area of the demolished building structure. There will also be various improvements made to the parking field adjacent to the east side of the existing mall property both for ADA compliance issues and for new landscaped areas.

Another modification is to remove the former bank's drive thru area and rework the parking field in the immediate vicinity of the former bank location in the Northwestern corner of the subject property. The existing building will remain, but there will be changes made to the exterior facade and a separation in the building to accommodate multiple tenants. The six-lane drive thru service area of the bank which is currently located on the West side of the building (facing the service road and I-65) will be removed. A single-lane drive thru service area will be added to the North side of the existing building as shown on the site plan.

The final change proposed by this application is to put an exterior sign plan in place over the entire site in order to afford current and future tenants adequate signage to attract national retailers. There previously were fewer and larger retail tenants, but we will now have more tenants in smaller spaces, so a new signage plan needs to be adopted. The signage plan is set forth in more detail on the attachment hereto, but it generally calls for different exterior signs on the outside of the buildings and one new free-standing sign on Airport Blvd. This is needed to account for the addition of new tenants which are more numerous than before based on changing retail space needs. This change is also designed to solve some tenant requests to the City for variances from the sign code in order to make the site comply with leasing guidelines of tenants and to facilitate customers being able to locate the entrances to the stores they wish to visit.

Plan Consistency Analysis:

The proposed use of the property is not changing, so there is no change in the consistency of this plan from the Comprehensive Plan. This area is still targeted as large retail shopping center, and that use will continue. The changes in the retail market nationally are driving the changes required at this site to make smaller retail facilities available with quick and easy access to customers. More tenants at this location means more signs. The proposed PUD modification, which reduces total building square footage in favor of increased travel circulation, parking and landscaped areas, is consistent with (or at least not inconsistent with) the proposed zoning for this property and the current Comprehensive Plan. This area is the major, concentrated retail hub of the City, and these changes requested here are to keep this a viable shopping center that is attractive to retailers and to the public.

The proposed demolition on the east side will primarily remove portions of the mall originally built to house a multiplex movie theater, and a small one story portion of a building originally constructed as part of the Montgomery Wards addition. As stated in the narrative, these cleared areas will be reused for additional parking and landscape areas. On the west side of the site, an

existing multi-lane bank drive-thru will be removed, and a new single-lane drive-thru will be added to the north side of the same building.

The revisions, as proposed, are not anticipated to negatively impact site circulation, and may improve the appearance of the property due to the provision of addition landscape areas. However, there are additional landscape issues that need to be addressed by this Planned Unit Development. The current owners of Springdale were approved to remove six (6) live oak trees along the west side of the property, adjacent to the Sam's Club gas station, subject to coordination for their replacement on the site. The live oak trees were required as part of the 2003 PUD, which resulted in the development of the Sam's Club gas station, and were in fact tied to the gas station itself. The PUD site plan submitted with this application shows the live oak trees still in place, and doesn't address this issue.

The amount of demolition proposed with this application is not enough in itself to trigger full compliance with the tree and landscaping requirements of the Zoning Ordinance. However, the Planning Commission has historically required the provision of overstory frontage trees for PUD sites undergoing substantial changes. The site has approximately 3,400 linear feet of street frontage along Airport Boulevard / I-65, and another 330 linear feet along Springdale Boulevard. This translates into 113 overstory frontage trees along the Airport / I-65 frontage, and 11 overstory trees along Springdale Boulevard. The Commission has also historically worked with applicants to ensure that trees are appropriately placed, and their location approved by the Urban Forester.

Regarding the proposed sign package for the development, the staff recommended a sign package submittal as part of the PUD application. The mall has been the subject of numerous sign variances over the years, and staff was of the opinion that a comprehensive sign package for the mall would better address the unique sign requirements of a development of this size.

The applicant proposes to divide the site into two areas: the primary mall area, and the out parcel area. The applicant provided the following proposed sign criteria:

Section 1: Purpose and Intent

- 1.1 The purpose and intent of the Comprehensive Signage Requirements for Springdale Mall (CSR) is to establish the requirement for locations, numbers, types and sizes of on-premise signs that are appropriate to the size and character of the development and the zoning district while maintaining the protective elements of Section 64-11 (1) "Title and Statement of Purpose" of the Mobile Zoning Ordinance. In addition, the CSR intends to promote adequate means of communication through signage that provide for the attractive visual appearance with the city, order and meaning to business identification, and ease of public and business delivery systems to locate their destinations.*
- 1.2 The location of this development needs appropriate signage not only for the safe routing of internal and neighborhood traffic; but also for the traveling public using I-65, I-65 Service Roads, Airport Boulevard, and Springdale Boulevard. Appropriate*

signage within the development will also encourage the desired internal routing of vehicular traffic. Establishments within this complex may be situated so that they are not visible from exterior roadways. It will be necessary to allow signage on any side of buildings facing perimeter roads: I-65, I-65 Service Roads, Airport Boulevard, and Springdale Boulevard.

Section 2: Freestanding Signs

2.1 Given the nature of the existing development, multiple means for evaluating freestanding signage are required. As such, the establishments operating within the respective Primary Mall Area or Outparcel Area outlined on the attached Exhibit A shall be subject to the following requirements:

2.1.1 Primary Mall Area:

2.1.1.1 Two freestanding multi-tenant pylon signs shall be permitted along the Airport Boulevard right of way property line, as indicated on the attached Exhibit A. Each sign shall be limited to a maximum display surface area of three hundred fifty (350) square feet per side and a maximum height of fifty (50) feet.

2.1.1.2 One freestanding high-rise multi-tenant pylon sign shall be permitted along the I-65 right of way property line, as indicated on the attached Exhibit A. This high-rise sign shall be limited to six hundred (600) square feet display area per side and a maximum height restriction of one hundred (100) feet.

2.1.1.3 Menu boards shall not count toward the freestanding signage limitations. One (1) drive-thru menu/order board shall be permitted per drive-thru lane, with a maximum of forty-eight (48) square feet each.

2.1.1.4 Internal wayfinding/directional signage guiding pedestrian and vehicular traffic circulation shall not count toward the freestanding signage limitations. Each sign shall be limited to a maximum display surface area of thirty-six (36) square feet per side and a maximum height of six (6) feet.

2.1.2 Outparcel Area:

2.1.2.1 Buildings housing a single establishment located in this area shall each be permitted one (1) freestanding sign with one hundred (130) square feet display area per side and a maximum height restriction of thirty-five (35) feet.

2.1.2.2 Buildings housing multiple establishments located in this area shall be permitted one freestanding multi-tenant pylon sign. Each sign shall be limited to a maximum display surface area of three hundred fifty (350) square feet per side and a maximum height of thirty-five (35) feet.

2.1.2.3 *Menu boards shall not count toward the freestanding signage limitations. One (1) drive-thru menu/order board shall be permitted per drive-thru lane, with a maximum of forty-eight (48) square feet each.*

2.1.2.4 *Internal wayfinding/directional signage guiding pedestrian and vehicular traffic circulation shall not count toward the freestanding signage limitations. Each sign shall be limited to a maximum display surface area of ten (10) square feet per side and a maximum height of three (3) feet.*

Section 3: Building Signs

3.1 *Given the nature of the existing development, multiple means for evaluating building-mounted signage are required. As such, the establishments operating within the respective Primary Mall Area or Outparcel Area outlined on the attached Exhibit A shall be subject to the following requirements:*

3.1.1 Primary Mall Area:

3.1.1.1 *Single-Tenant Building: Shall be defined as a building designed to house only one establishment and is located in the Primary Mall Area as outlined on Exhibit A. Wall signs, canopy signs, projecting (blade) signs and window signs are not limited with respect to quantity and may be used in any combination and the sum of the square footage of all sign types may not exceed 30% (thirty percent) of the exterior building wall area. Wall signs may be installed on any exterior walls regardless of orientation to the public right of way.*

3.1.1.2 *Multi-Tenant Buildings: Shall be defined as a building designed to accommodate multiple establishments and is located in the Primary Mall Area as outlined on Exhibit A. Wall signs, canopy signs, projecting (blade) signs and window signs are not limited with respect to quantity and may be used in any combination provided that the sum of the square footage of all sign types may not exceed 30% (thirty percent) of the establishment's primary entrance wall area. The primary entrance wall area being defined as the width of the premises between demising walls multiplied by the building height from finished floor to the top of parapet wall. All establishments are permitted a second wall sign of equal or lesser size than the primary entrance wall sign on another exterior wall of the same building, regardless of orientation to the public right of way. The second wall sign shall not count toward the base allowance of signage on the primary entrance wall.*

3.1.2 Outparcel Area:

3.1.2.1 *Single-Tenant Building: Shall be defined as a building designed to house only one establishment and is located in the Primary Mall Area as outlined on*

Exhibit A. Wall signs, canopy signs, projecting (blade) signs and window signs are not limited with respect to quantity and may be used in any combination and the sum of the square footage of all sign types may not exceed 30% (thirty percent) of the exterior building wall area. Wall signs may be installed on any exterior walls regardless of orientation to the public right of way.

3.1.2.2 Multi-Tenant Building: Shall be defined as a building designed to accommodate multiple establishments and is located in the Primary Mall Area as outlined on Exhibit A. Wall signs, canopy signs, projecting (blade) signs and window signs are not limited with respect to quantity and may be used in any combination provided that the sum of the square footage of all sign types may not exceed 30% (thirty percent) of the establishment's primary entrance wall area. The primary entrance wall area being defined as the width of the premises between demising walls multiplied by the building height from finished floor to the top of parapet wall. All establishments are permitted a second wall sign of equal or lesser size than the primary entrance wall sign on another exterior wall of the same building, regardless of orientation to the public right of way. The second wall sign shall not count toward the base allowance of signage on the primary entrance wall.

Section 4: Compliance

4.1 The requirements as set forth in the City of Mobile Code of Ordinances Chapter 64-Zoning; Section 64-11 "Sign regulation provisions" shall govern where not specifically addressed within this Comprehensive Signage Requirements.

4.2 Minor deviations to requirements of this CSR may be made by City of Mobile Urban Development staff with the written authorization of the Property Owner.

4.3 All other signs allowed by or determined to not require permit by Chapter 64-Zoning, Section 64-11 shall be permitted.

The proposed sign package will allow, generally, more signs than the standard sign regulations will permit. Following are the areas of primary difference between the existing and proposed regulations:

For a single-tenant properties:

❖ Building Mounted Signs:

- Proposed. the proposal will allow an unlimited number of building mounted signs on all walls, subject to the combined total area not exceeding 30% of the wall area.
- Existing. Limited to two walls signs, not exceeding 30% of usable wall area, with a 350 square foot limit for each sign.

❖ **Freestanding Signs:**

- Proposed. Limited to one freestanding sign, either monument or pole, 130 square feet maximum per face, 35 foot height limit.
- Existing. Limited to one freestanding sign, either monument or pole, 200 square feet maximum per face, 35 foot height limit. A single tenant site may instead have a high-rise freestanding up to 100 feet in height, if located within 1000 feet of an interstate.

For Multi-Tenant Properties:

❖ **Building Mounted Signs:**

- Proposed. the proposal will allow an unlimited number of building mounted signs on the primary entrance wall of the tenant space, subject to the combined total area not exceeding 30% of the wall area, and one wall sign on a secondary wall of the tenant space.
- ❖ Existing. Limited to building mounted sign (unless a corner unit facing a street, then a second building mounted sign is allowed on that facade), not exceeding 30% of usable wall area, with a 350 square foot limit for each

❖ **Freestanding Signs:**

- Proposed.
 - The Primary Mall Area is limited to two 50-foot tall freestanding multi-tenant panel signs on Airport Boulevard, not to exceed 350 square feet per face.
 - The Primary Mall Area is also allowed one 100-foot tall multi-tenant panel sign on the I-65 frontage, not to exceed 600 square feet per face.
 - A site in the Outparcel Area, with a multi-tenant building, would be allowed one 35-foot tall freestanding multi-tenant panel sign, not to exceed 350 square feet per face.
- Existing.
 - A multi-tenant site is allowed no more than three (3) multi-tenant pylon signs, not to exceed 350 square feet per face, and a height limit of 50 feet. The number of signs is based upon the length of road frontage for the property: one sign for 600 feet of frontage, two for 600 to 1200 feet, and three for more than 1200 feet.
 - An outparcel site would be allowed one freestanding sign, in accordance to the single-tenant sign requirements.

To summarize, the primary differences between the existing and proposed sign standards are 1) that the *number* of building mounted signs is not limited under the proposed sign standards, and 2) that a 100-foot tall multi-tenant panel freestanding sign is allowed along I-65. The existing sign standards limit the number of signs on a building, and do not allow for a 100-foot tall *multi-tenant panel* freestanding sign (a *single tenant site* is allowed a 100-foot tall sign by right within 1000 feet of the interstate).

The site has three existing multi-tenant panel freestanding signs for the Primary Mall Area, thus it appears that the proposed regulations will not increase the number of these types of signs, however, signs will be relocated. It is unclear if Sam's Club would participate in the multi-tenant panel freestanding signs for the Primary Mall Area along I-65, as they currently do, or if they are excluded from the proposed sign standards.

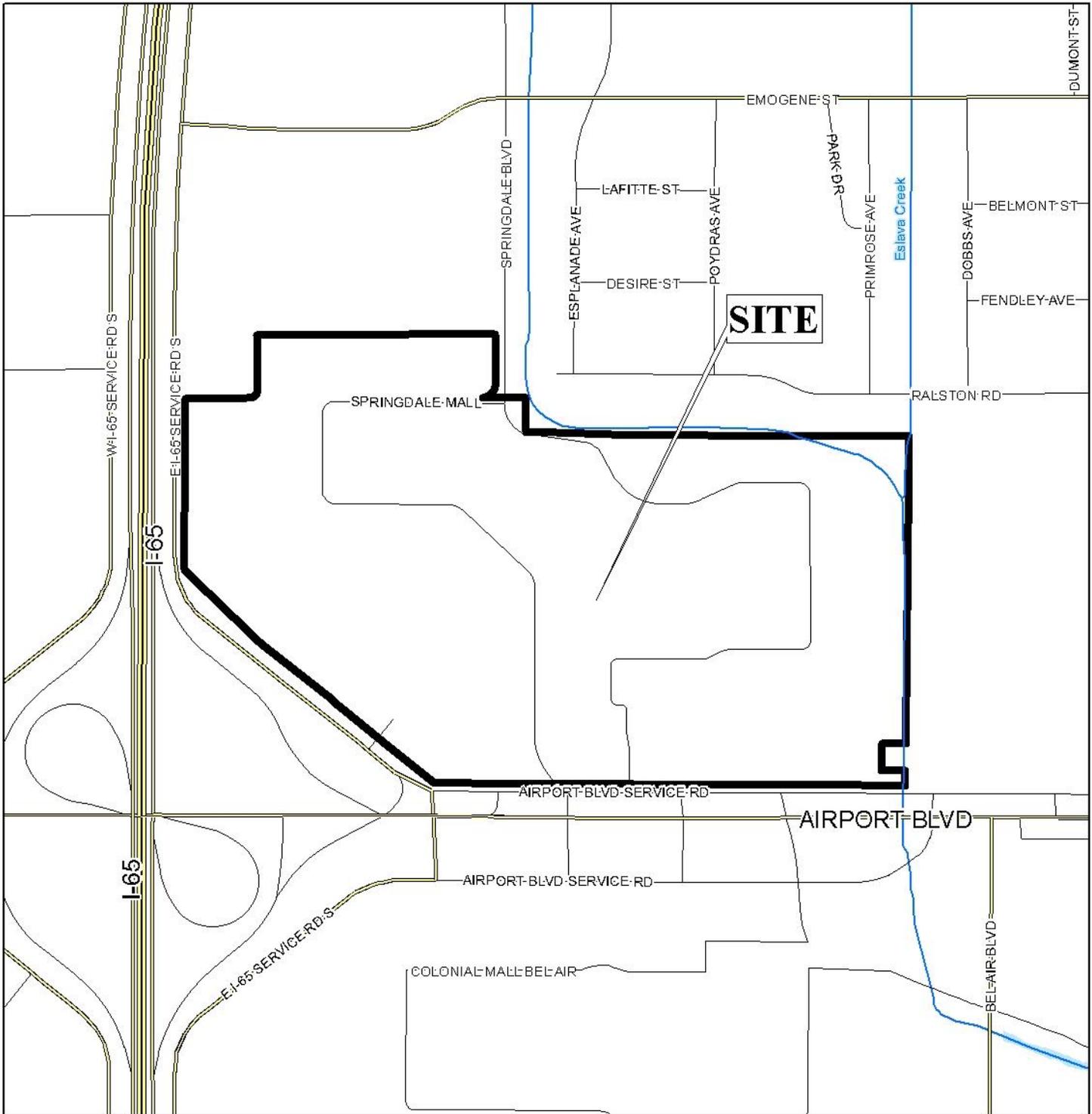
It should be noted that there is no indication as part of the proposed sign standards if digital signage is proposed. If digital signage is proposed along the I-65 corridor, the applicant should contact ALDOT to determine if additional regulations may apply.

RECOMMENDATION

Planned Unit Development: Staff recommends request be heldover until the May 21st meeting, with revisions provided by May 1st, to address the following:

- 1) Revision of the application to include all relevant properties as part of the Planned Unit Development, with consent from each property owner;
- 2) Revision of the site plan to reflect Traffic Engineering comments regarding closure of access (*Close the redundant/unnecessary access points along the southern side of the property by removing asphalt pavement and installing curb and landscaping. Sites to be closed are defined in Figures 13 and 14 in the TIS*);
- 3) Revision of the site plan to reflect frontage tree compliance along all street frontages, in particular to address the live oak trees removed from the I-65 frontage;
- 4) Clarification of proposed locations for new freestanding signs on the site plan, and notation which existing signs will be removed or replaced, including sign illustrations; and
- 5) Clarification as to which properties will initially have their own single or multi-tenant freestanding signs.

LOCATOR MAP



APPLICATION NUMBER 4 DATE April 16, 2020

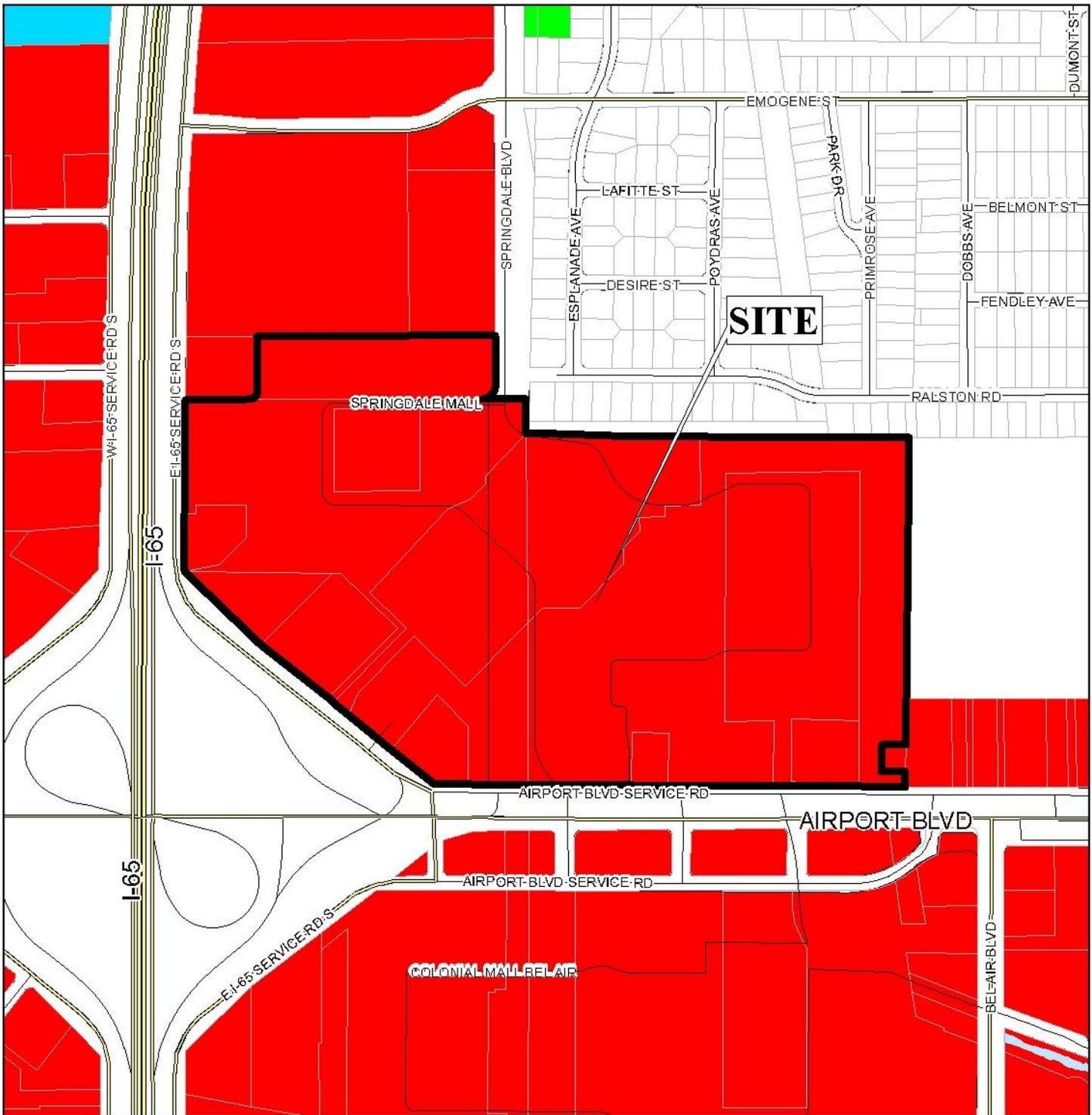
APPLICANT Casey Pipes

REQUEST Planned Unit Development



NTS

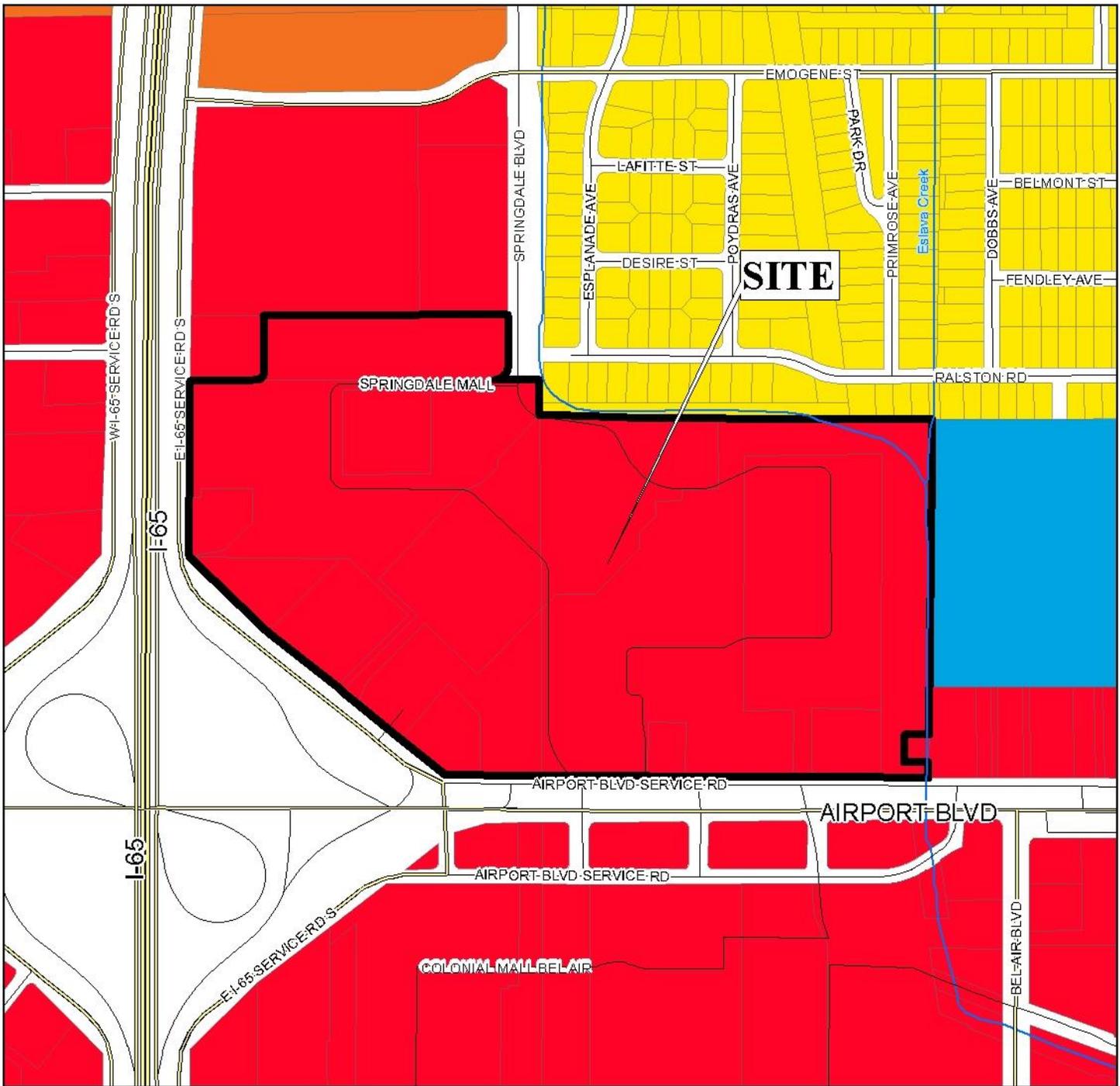
LOCATOR ZONING MAP



APPLICATION NUMBER 4 DATE April 16, 2020
APPLICANT Casey Pipes
REQUEST Planned Unit Development



FLUM LOCATOR MAP



APPLICATION NUMBER 4 DATE April 16, 2020

APPLICANT Casey Pipes

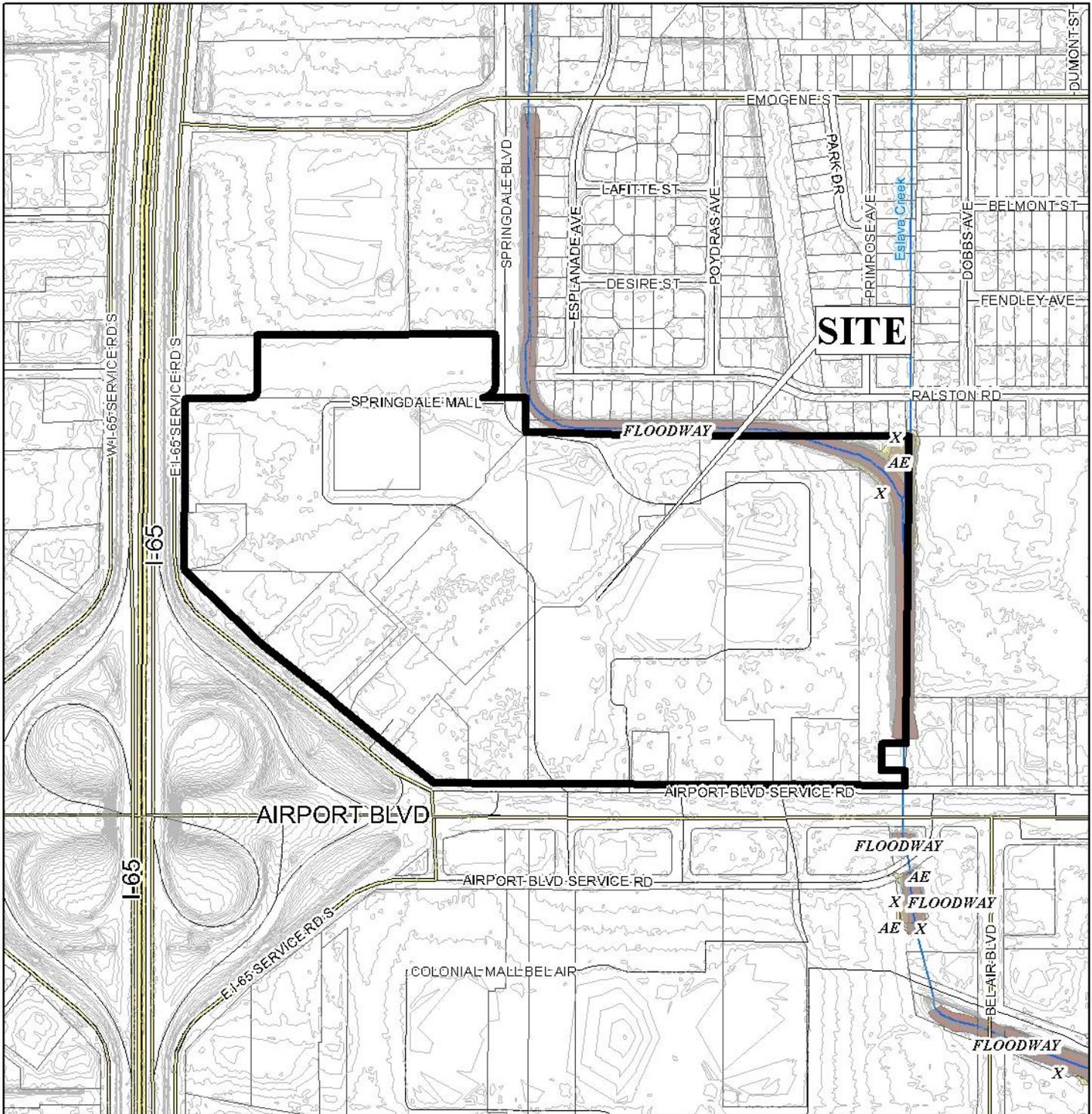
REQUEST Planned Unit Development

- | | | | |
|---------------------------|-----------------------------------|---------------------|--------------------|
| Low Density Residential | Neighborhood Center - Traditional | Downtown Waterfront | Parks & Open Space |
| Mixed Density Residential | Neighborhood Center - Suburban | Light Industry | Water Dependent |
| Downtown | Traditional Corridor | Heavy Industry | |
| District Center | Mixed Commercial Corridor | Institutional | |



NTS

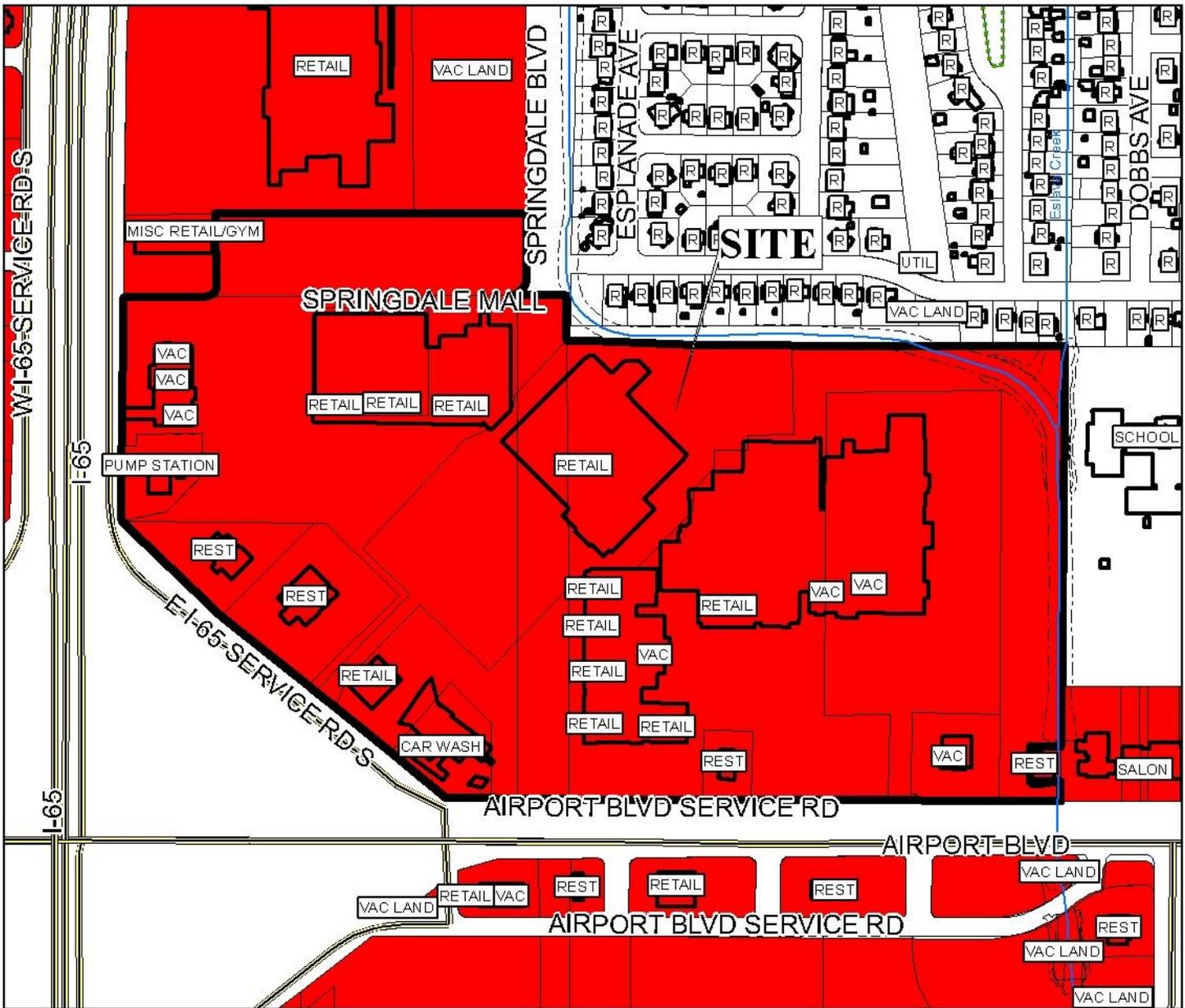
ENVIRONMENTAL LOCATOR MAP



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 APPLICANT Casey Pipes
 REQUEST Planned Unit Development



PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The site is surrounded by commercial units to the south and west, and residential units to the northeast.

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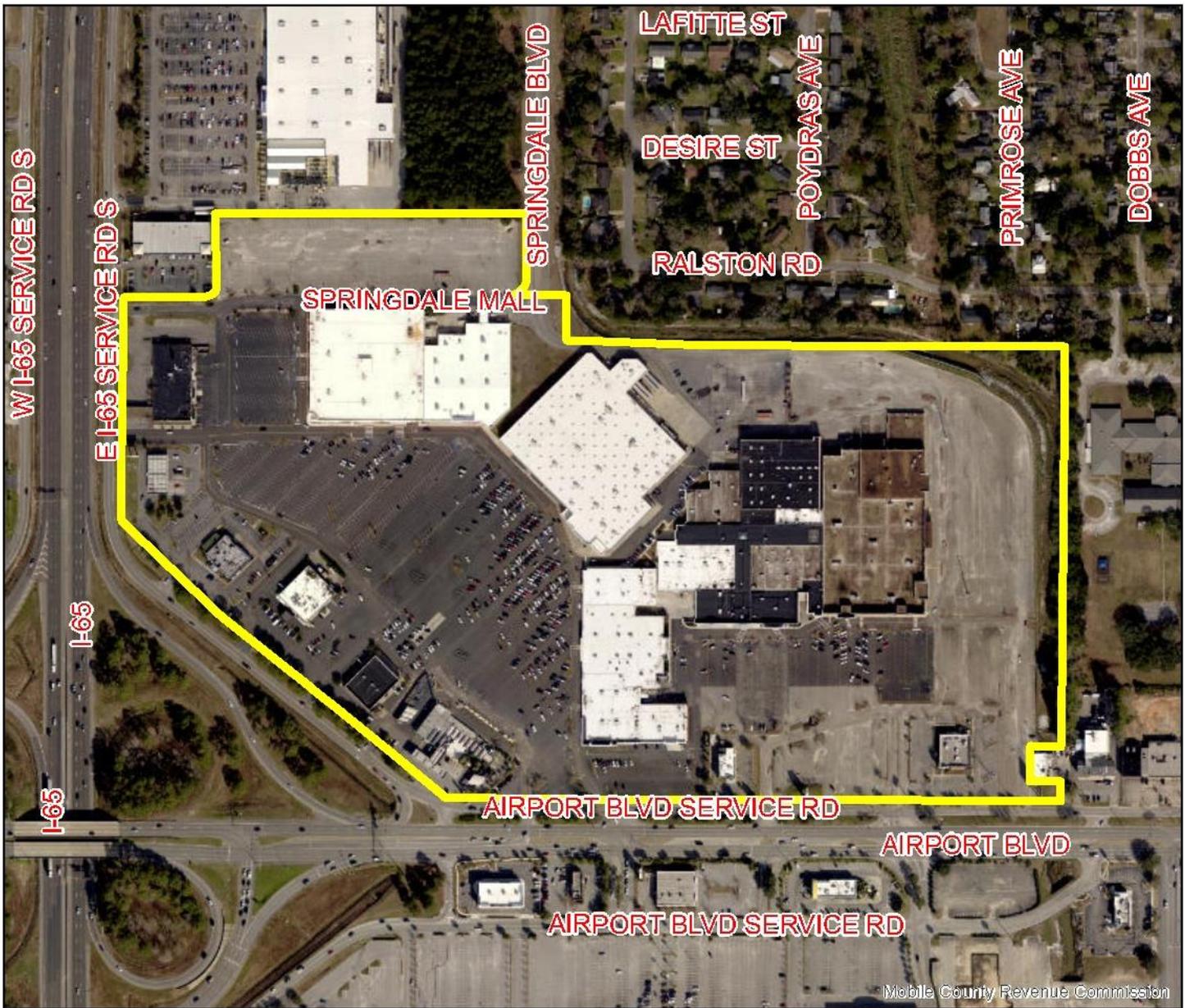
REQUEST Planned Unit Development

 R-A	 R-3	 T-B	 B-2	 B-5	 MUN	 SD-WH	 T5.1
 R-1	 R-B	 B-1	 B-3	 I-1	 OPEN	 T3	 T5.2
 R-2	 H-B	 LB-2	 B-4	 I-2	 SD	 T4	 T6



NTS

PLANNING COMMISSION VICINITY MAP - EXISTING AERIAL



Mobile County Revenue Commission

The site is surrounded by commercial units to the south and west, and residential units to the northeast.

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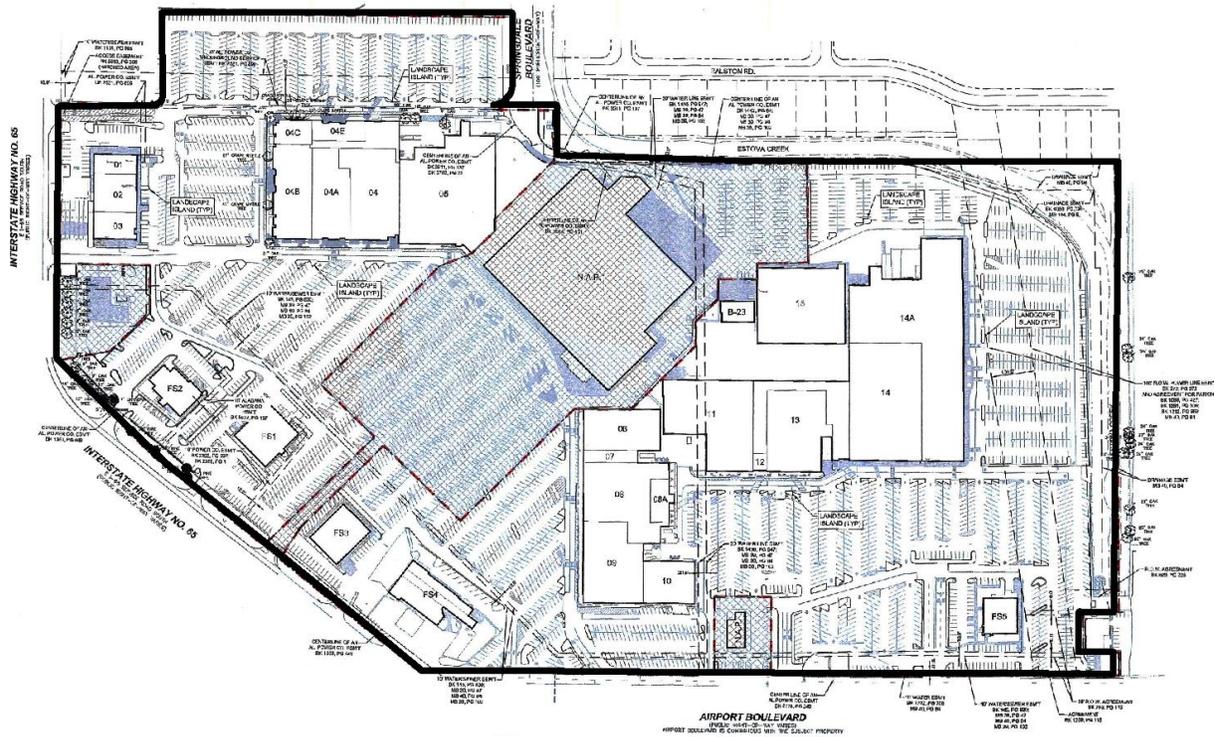
APPLICANT Casey Pipes

REQUEST Planned Unit Development



NTS

EXISTING SITE PLAN



The site plan illustrates the existing buildings, parking, and easements.

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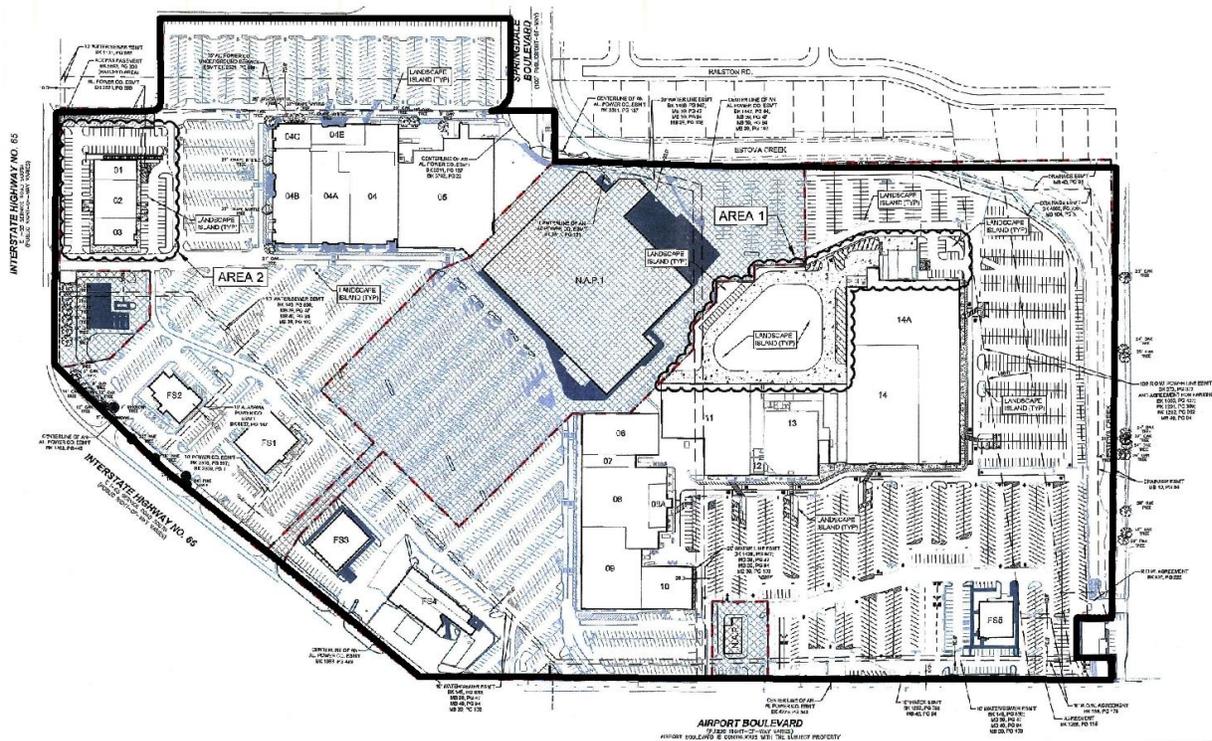
APPLICANT Casey Pipes

REQUEST Planned Unit Development



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PROPOSED SITE PLAN



The site plan illustrates the existing buildings, parking, and easements.

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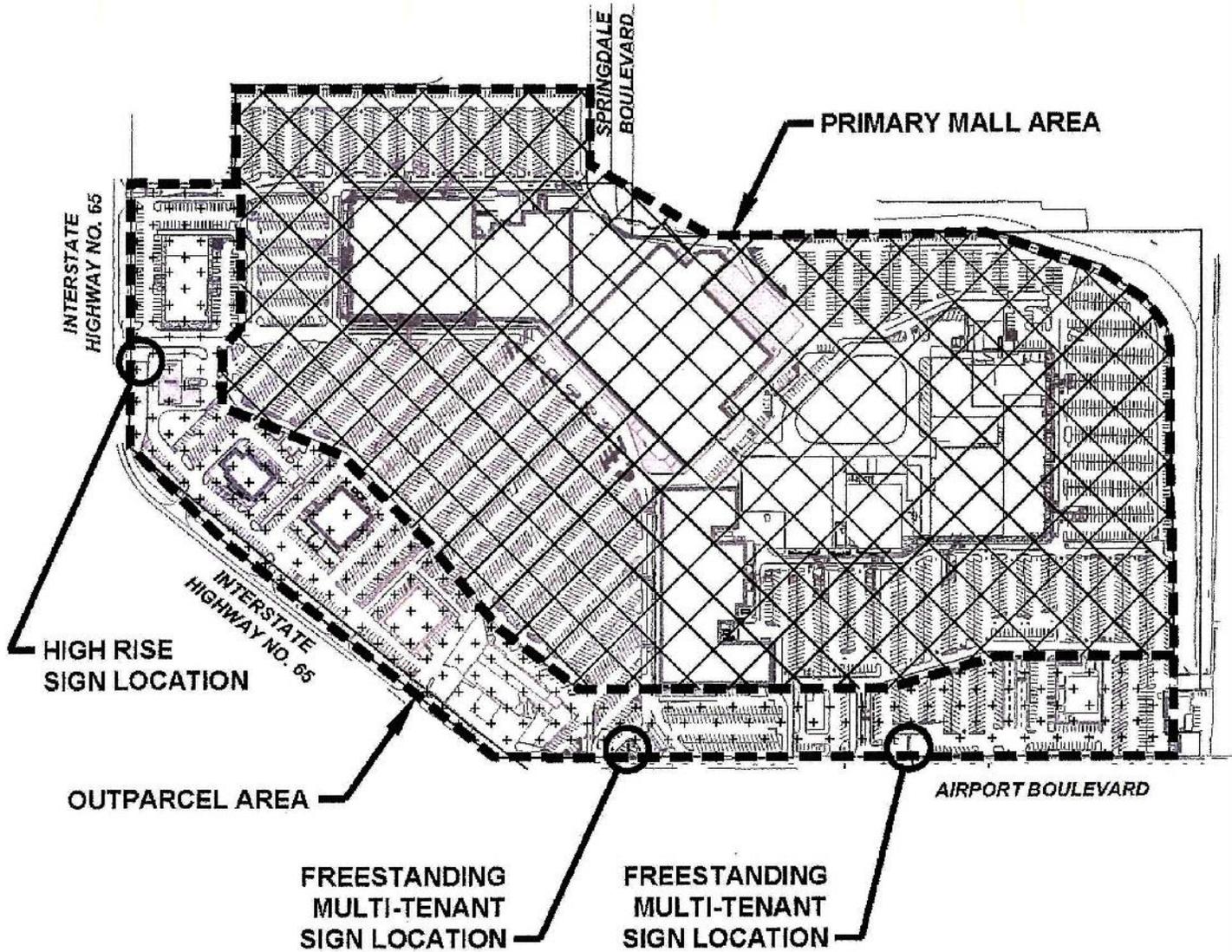
APPLICANT Casey Pipes

REQUEST Planned Unit Development



NTS

DETAIL SITE PLAN



APPLICATION NUMBER 4 DATE April 16, 2020

APPLICANT Casey Pipes

REQUEST Planned Unit Development

