

**PLANNING APPROVAL &
SUBDIVISION STAFF REPORT****Date: May 19, 2011****NAME**

Plantation Memorial Gardens Subdivision

SUBDIVISION NAME

Plantation Memorial Gardens Subdivision

LOCATION5501 Bear Fork Road
(South side of Bear Fork Road at the South terminus of
Jarrett Road)**CITY COUNCIL
DISTRICT**

District 7

PRESENT ZONING

R-1, Single-Family Residence District

AREA OF PROPERTY1 lot / 25.0 acres \pm **CONTEMPLATED USE**Planning Approval to allow a cemetery with 21,250 plots in
an R-1, Single-Family Residential District, and Subdivision
approval to create one lot.**TIME SCHEDULE
FOR DEVELOPMENT**Phase One, one year
Phase Two, the remainder of parcel, within seven years**ENGINEERING
COMMENTS**

Must comply with all storm water and flood control ordinances. Any increase in impervious area in excess of 4,000 square feet will require detention. The proposed detention pond will need to be sized to provide at a minimum, detention from a 100 year storm with a 2 year release rate as outlined in the storm water ordinance. Any work performed in the right of way will require a right of way permit in addition to any required land disturbance permit.

TRAFFIC ENGINEERING

COMMENTS Driveway number, size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards. Driveway shown is 60 feet in width. Narrow the driveway to a standard width of 24 feet

URBAN FORESTRY

COMMENTS Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

FIRE DEPARTMENT**COMMENTS**

All projects within the City of Mobile Fire Jurisdiction must comply with the requirements of the 2009 International Fire Code, as adopted by the City of Mobile.

REMARKS

The applicant is requesting Planning Approval to allow a cemetery with 21,250 plots in an R-1, Single-Family Residential District, and Subdivision approval to create one lot. Cemeteries require Planning Approval when located in R-1 districts.

The review required for Planning Approval examines the applicant's location and site plan with regard to transportation, parking and access, public utilities and facilities, traffic congestion and hazard, and to determine if the proposal is in harmony with the orderly and appropriate development of the district.

It is very important to note that the Planning Approval review is site plan specific; therefore any future changes to the overall site plan must be submitted for Planning Approval review.

The applicant proposes to develop a 25 acre cemetery as part of a multi phase development that ultimately is envisioned to incorporate the remaining 88-acre parent parcel (*depicted as future development*). A note on the preliminary plat states that the 25-acre lot will contain 850 plots per acre, for a total of 21,250 burial plots. The written narrative for the Planning Approval states that the cemetery will have two sections: a section with flush bronze grave-markers, and a section with upright granite monuments. Internal circulation for the site will be via 24-foot wide private asphalt roads within 50-foot wide rights-of-way. A large storm water detention facility is provided.

The site fronts onto Bear Fork Road, a minor street with an adequate 80-foot right-of-way. Two proposed major streets cross the overall site: the Red Creek-Eight Mile Creek Parkway and Middle Ring Road, with proposed rights-of-way of 200 feet and 100 feet, respectively. The preliminary plat and site plan for the proposed 25-acre lot depicts a 50-foot reservation for future right-of-way along the West property line to accommodate the proposed Middle Ring Road, however, this should either be depicted as an outright dedication, or as additional setback with a note stating that is to allow the future development of Middle Ring Road. As the proposed lot is not apparently within the path of the proposed Red Creek-Eight Mile Creek Parkway, no reservation is depicted, however, future phases of the cemetery will be required to accommodate the parkway in some manner.

The site is bounded by large undeveloped parcels to the East, South and West, and by primarily undeveloped land to the North, across Bear Fork Road in Prichard, however, there are one or two single-family residences located across from the site.

While Bear Fork Road is a minor street, access management is a concern. The site plan depicts one curb-cut onto Bear Fork Road, although the proposed lot has nearly 900 feet of frontage onto Bear Fork Road. To ensure consistency with the submitted site plan, the proposed lot should be limited to one curb-cut to Bear Fork Road, with the size, design and location to be approved by Traffic Engineering and Planning, and to comply with AASHTO standards.

No buildings are proposed with the initial 25-acre development. The applicant envisions that future phases will include any buildings that may be desired as the cemetery expands. As any future phases will require new applications for Planning Approval, those issues can be addressed at that time.

It should be pointed out that MAWSS has obtained a 0.1 acre parcel for a pump station, and it appears that the parcel was created in 2006 without the appropriate application for Subdivision approval. The parcel should be included as part of the overall application for the proposed lot at hand.

As a commercial development, full compliance with the tree and landscaping requirements of the Zoning Ordinance will be required. The site plan depicts fairly extensive landscaping along Bear Fork Road, however, no landscape or tree calculations are provided. As the majority of the cemetery will be sod, landscape area should not be an issue. Due to the extensive frontage and perimeter, however, the provision of an adequate number of frontage and perimeter trees is questionable, thus the site plan should be revised to indicate the provision of required trees, or existing trees that will be retained for credit.

Due to the proposed use of the site as a cemetery, buffering should be provided. A minimum of a 10-foot wide landscape buffer should be provided where the site abuts R-1 property, and due to the size of the property, this buffer can remain as a natural vegetative buffer. If the applicant intends to place a fence and gates on the site, the site plan should indicate the location of the fence and gates, as well as the proposed height.

A sidewalk along Bear Fork Road is depicted on the site plan, and will be required when the lot is developed (during construction of the internal circulation drive).

Portions of the parent parcel are encumbered by wetlands and floodplains associated with Eight Mile Creek. The presence of wetlands and floodplains indicate that the area may be environmentally sensitive; therefore, the approval of all applicable federal, state and local agencies would be required for floodplain and wetland issues prior to the issuance of any permits or land disturbance activities.

The geographic area defined by the City of Mobile and its planning jurisdiction, including this site, may contain Federally-listed threatened or endangered species as well as protected non-game species. Development of the site must be undertaken in compliance with all local, state and Federal regulations regarding endangered, threatened or otherwise protected species.

Finally, the 25-foot minimum building setback line is not depicted on the plat, however it is partially depicted on the site plan. The plat and site plan should be revised to depict the 25-foot minimum building setback line along both Bear Fork Road and the proposed Middle Ring Road right-of-way.

RECOMMENDATION

Planning Approval: The request is recommended for Holdover until the June 16 meeting, so that the following revisions can be made by June 3rd:

- 1) Revision of the site plan to depict compliance with the tree and landscaping requirements of the Zoning Ordinance, including calculations for open space and trees;
- 2) Revision of the site plan to depict the 25-foot minimum building setback from both the Bear Fork Road and proposed Middle Ring Road frontages;
- 3) Revision of the label for Middle Ring Road to depict the future right-of-way as being dedicated to the City of Mobile, or as a setback, in addition to the 25-foot minimum building setback;
- 4) Revision of the site plan to depict any fencing or gates proposed, to include the height of the proposed fencing;
- 5) Compliance with Traffic Engineering comments (*Driveway number, size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards. Driveway shown is 60 feet in width. Narrow the driveway to a standard width of 24 feet*);
- 6) Compliance with Engineering comments (*Must comply with all storm water and flood control ordinances. Any increase in impervious area in excess of 4,000 square feet will require detention. The proposed detention pond will need to be sized to provide at a minimum, detention from a 100 year storm with a 2 year release rate as outlined in the storm water ordinance. Any work performed in the right of way will require a right of way permit in addition to any required land disturbance permit*); and
- 7) Depiction and labeling of a 10-foot wide natural vegetative buffer where the site abuts residentially-zoned property.

Subdivision: The request is recommended for Holdover until the June 16 meeting, so that the following revisions can be made by June 3rd:

- 1) Inclusion of the MAWSS parcel, or provision of a letter from MAWSS requesting exclusion from the Subdivision request;
- 2) Compliance with Traffic Engineering comments (*Driveway number, size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards. Driveway shown is 60 feet in width. Narrow the driveway to a standard width of 24 feet*);
- 3) Compliance with Engineering comments (*Must comply with all storm water and flood control ordinances. Any increase in impervious area in excess of 4,000 square feet will require detention. The proposed detention pond will need to be sized to provide at a minimum, detention from a 100 year storm with a 2 year release rate as outlined in the storm water ordinance. Any work performed in the right of way will require a right of way permit in addition to any required land disturbance permit*);
- 4) Revision of the site plan to depict the 25-foot minimum building setback from both the Bear Fork Road and proposed Middle Ring Road frontages; and
- 5) Revision of the label for Middle Ring Road to depict the future right-of-way as being dedicated to the City of Mobile, or as a setback, in addition to the 25-foot minimum building setback.

Revised for the June 16th meeting:

Revisions were submitted to address concerns raised by staff. A letter was submitted by MAWSS stating their desire not to participate in the Subdivision.

The preliminary Subdivision plat does not appear to reflect the required setbacks from Bear Fork Road and the setback in-lieu of dedication for the proposed Middle Ring Road, and the

required 25-foot setback. These setbacks are depicted on the Planning Approval site plan. The preliminary plat should be revised to show all setbacks. The “50’ reservation for future road” label should be removed, and instead be relabeled as a “50’ setback in-lieu of dedication for future road” for both the site plan and plat.

The site plan was revised to relocate the driveway entrance, as well as change the size of the curb-cut to meet Traffic Engineering requirements.

A 3-foot high decorative fence with gate is proposed along the Bear Fork Road frontage. The gate, as proposed, will be almost on the right-of-way edge, however, typically a distance of three (3) queuing spaces, or 51-feet, is required between a gate and the edge of the right-of-way, thus the site plan should be revised to reflect this requirement. In addition to the decorative fence, a 6-foot high chain link fence is proposed around the remainder of the site. Permits will be required for the proposed fencing.

Internal circulation has been changed to provide two stubs to the East of the site, anticipating future expansion of the cemetery. A one-way loop will serve the majority of the site: one way drives should be marked with signage and pavement markings.

Frontage trees are depicted along Bear Fork Road, while natural buffers, 10-feet wide, are depicted along the West, South and East sides. Even though frontage trees and natural buffers are depicted, compliance with the tree and landscaping requirements of the Zoning Ordinance cannot be determined. The applicant must ensure that the site will meet all tree and landscaping requirements, including frontage and perimeter trees, as well as landscape area: a revised site plan depicting compliance should be submitted prior to any request for land disturbance.

Finally, it should be noted that no information was provided regarding the total number of proposed plots after the redesign of the site. Therefore, staff recommends that the first phase be limited to a maximum of 21,250 plots, as originally proposed.

RECOMMENDATION

Planning Approval: As revised, the request is recommended for Approval, subject to the following conditions:

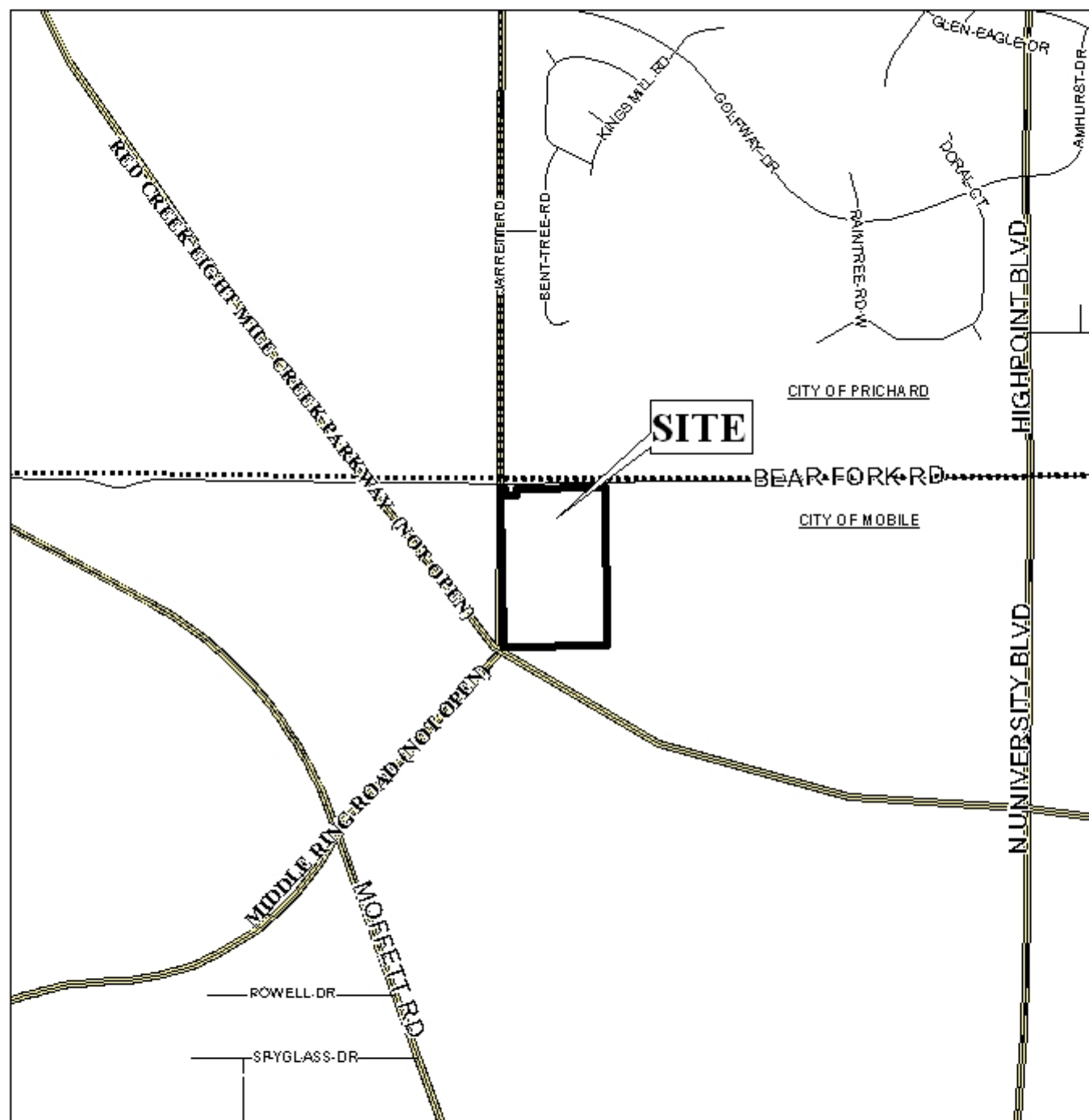
- 1) First phase of development limited to a maximum of 21,250 plots;
- 2) Revision of the site plan to depict compliance with the tree and landscaping requirements of the Zoning Ordinance, including calculations for open space and trees;
- 3) Revision of the site plan to depict a 51-foot setback for the proposed entry gate from the Bear Fork Road right-of-way;
- 4) Revision of the label for Middle Ring Road to depict the future right-of-way as a “50’ setback in-lieu of dedication for future road;”
- 5) Obtaining of permits for any proposed fencing;
- 6) Depiction of one-way pavement markings and signs for those portions of the driveways that will be one-way;
- 7) Compliance with Traffic Engineering comments (*Driveway number, size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards. Driveway shown is 60 feet in width. Narrow the driveway to a standard width of 24 feet;*)

- 8) Compliance with Engineering comments (*Must comply with all storm water and flood control ordinances. Any increase in impervious area in excess of 4,000 square feet will require detention. The proposed detention pond will need to be sized to provide at a minimum, detention from a 100 year storm with a 2 year release rate as outlined in the storm water ordinance. Any work performed in the right of way will require a right of way permit in addition to any required land disturbance permit*); and
- 9) Submission of a revised site plan prior to completion of the Subdivision process or any request for land disturbance.

Subdivision: As revised, the request is recommended for Tentative Approval, subject to the following conditions:

- 1) Placement of a note on the Final Plat stating that the lot is limited to one (1) curb-cut onto Bear Fork Road and denied access to the proposed Middle Ring Road, with the size, design and location of curb-cuts to be approved by Traffic Engineering and conform to AASHTO standards;
- 2) Compliance with Traffic Engineering comments (*Driveway number, size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards. Driveway shown is 60 feet in width. Narrow the driveway to a standard width of 24 feet*);
- 3) Compliance with Engineering comments (*Must comply with all storm water and flood control ordinances. Any increase in impervious area in excess of 4,000 square feet will require detention. The proposed detention pond will need to be sized to provide at a minimum, detention from a 100 year storm with a 2 year release rate as outlined in the storm water ordinance. Any work performed in the right of way will require a right of way permit in addition to any required land disturbance permit*);
- 4) Revision of the site plan to depict the 25-foot minimum building setback from both the Bear Fork Road and proposed Middle Ring Road frontages;
- 5) Revision of the label for Middle Ring Road to depict the future right-of-way as a “50’ setback in-lieu of dedication for future road;”
- 6) Placement of a note on the final plat stating that development of the site must be undertaken in compliance with all local, state and federal regulations regarding endangered, threatened or otherwise protected species;
- 7) Placement of a note on the Final Plat stating that approval of all applicable federal, state and local agencies is required for floodplain and wetland issues prior to the issuance of any permits or land disturbance activities; and
- 8) Submission of a revised site plan prior to completion of the Subdivision process or any request for land disturbance.

LOCATOR MAP



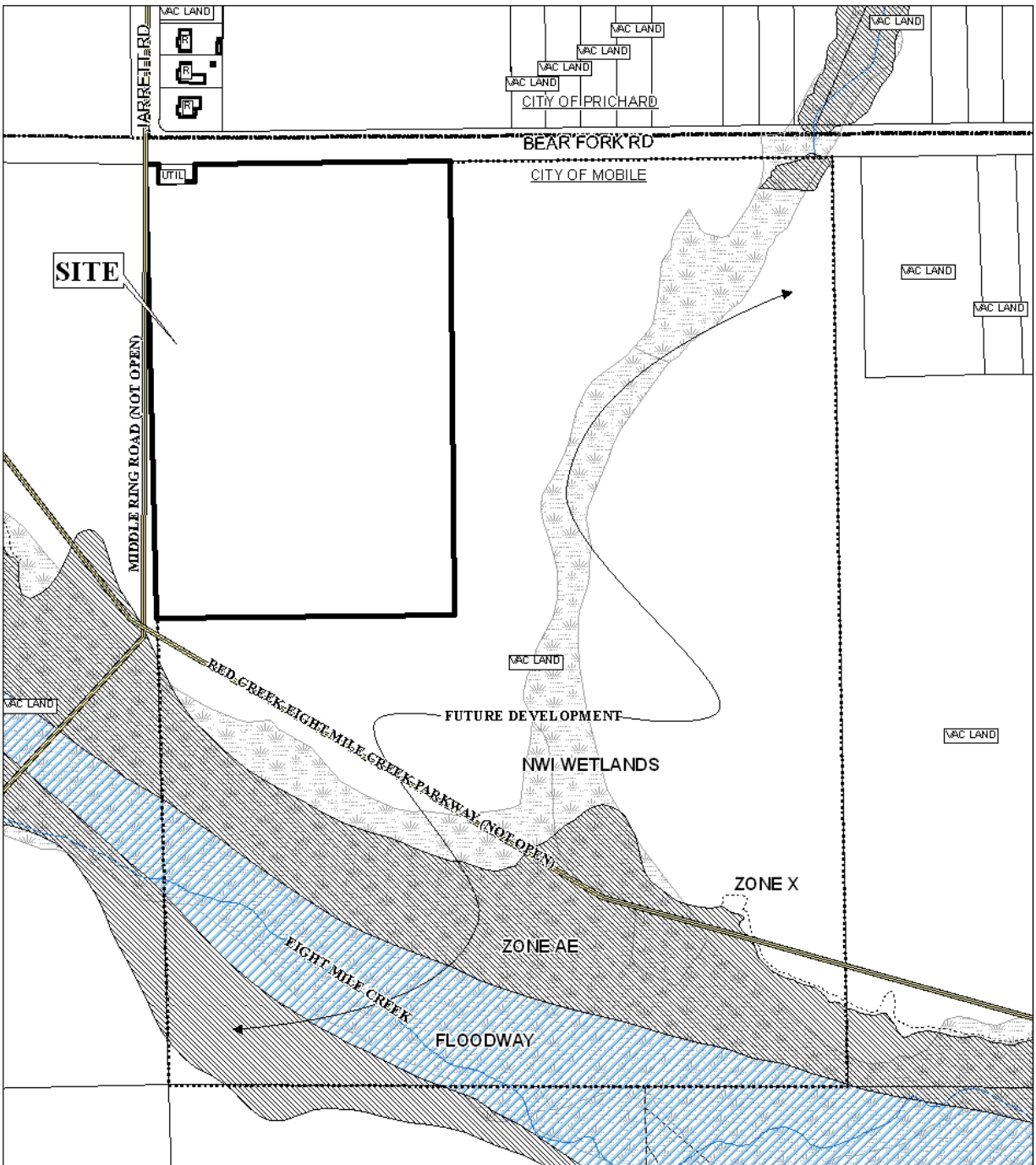
APPLICATION NUMBER 4 & 5 DATE June 16, 2011

APPLICANT Plantation Memorial Gardens Subdivision

REQUEST Subdivision, Planning Approval



PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



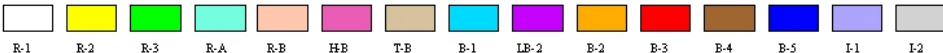
The site is surrounded by undeveloped land.

APPLICATION NUMBER 4 & 5 DATE June 16, 2011

APPLICANT Plantation Memorial Gardens Subdivision

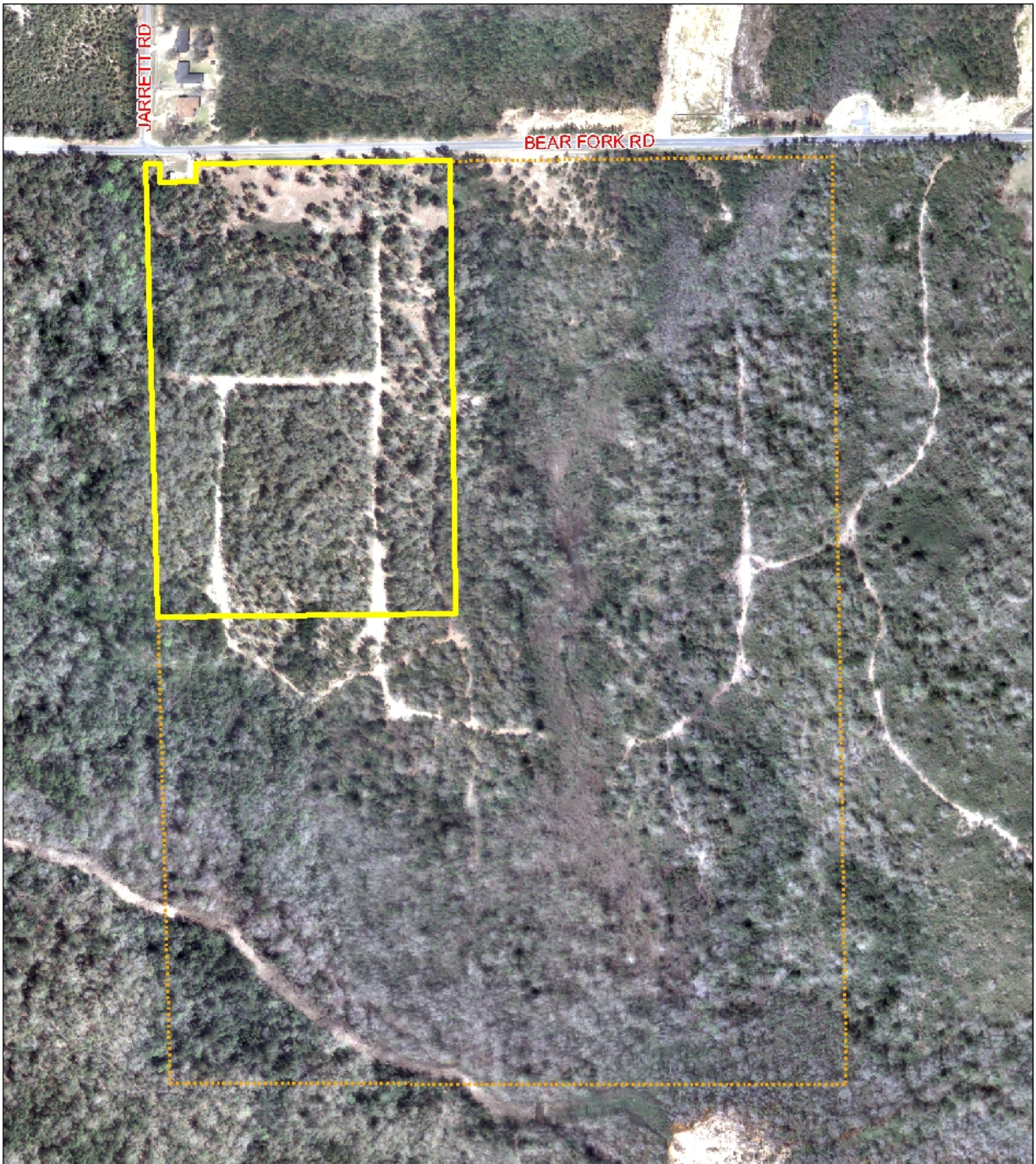
REQUEST Subdivision, Planning Approval

LEGEND



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PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The site is surrounded by undeveloped land.

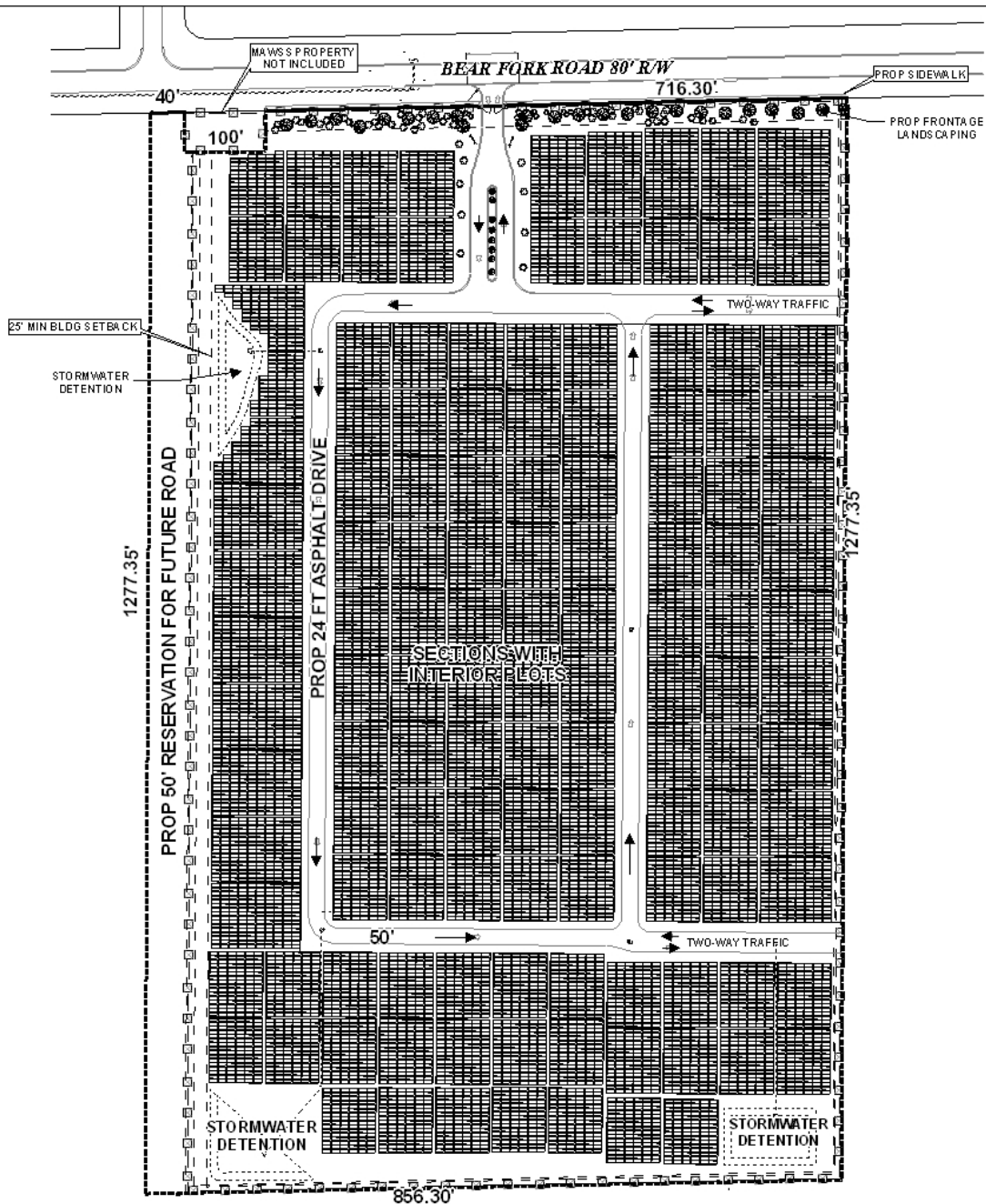
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APPLICANT Plantation Memorial Gardens Subdivision

REQUEST Subdivision, Planning Approval



SITE PLAN



The site plan illustrates the proposed cemetery

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REQUEST Subdivision, Planning Approval

