

**PLANNED UNIT DEVELOPMENT
& SUBDIVISION STAFF REPORT****Date: April 3, 2014****DEVELOPMENT NAME**

First Baptist Church of Tillman's Corner Subdivision

SUBDIVISION NAME

First Baptist Church of Tillman's Corner Subdivision

LOCATION

5660, 5670, 5680, 5700 Three Notch Road and 5641, 5651, 5661, and 5673 Jackson Road
(Northeast corner of Three Notch Road and Calhoun Road extending to the South side of Jackson Road)

**CITY COUNCIL
DISTRICT**

District 4

AREA OF PROPERTY

1-Lot/7.9 ± Acres

CONTEMPLATED USE

Planned Unit Development to allow multiple buildings on a single building site and Subdivision approval to combine multiple lots to create one legal lot of record.

**TIME SCHEDULE
FOR DEVELOPMENT**

Not specified.

**ENGINEERING
COMMENTS****Subdivision**

The following comments should be addressed prior to acceptance and signature by the City Engineer:

- A. Add a note to the SUBDIVISION PLAT stating that a Land Disturbance permit will be required for any land disturbing activity in accordance with the of the Storm Water Management and Flood Control Ordinance (Mobile City Code, Chapter 17 , Ordinance #65-007 & #65-045), latest edition.
- B. Add a note to the SUBDIVISION PLAT stating that as shown on the 1984 aerial photo (FLIGHT 20 - # 88) each lot will receive historical credit of impervious area towards storm water detention requirement per the Storm Water Management and Flood Control Ordinance (Mobile City Code, Chapter 17, Ordinance #65-007 & #65-045), latest edition. Coordinate with City Engineering Department staff to establish the exact amount prior to the submittal of the Land Disturbance Permit application.
- C. Add a note to the Plat stating that the approval of all applicable federal, state, and local agencies (including all storm water runoff, wetland and floodplain requirements) will be required prior to the issuance of a Land Disturbance permit.
- D. Dedication of the corner radius (25' minimum or as approved by the City Engineer and Traffic Engineer) at the corner of Three Notch Road and Calhoun Road.

- E. Provide a written legal description for the proposed subdivision and matching bearing and distance labels.
- F. Provide the Surveyor's, Owner's (notarized), Planning Commission, and Traffic Engineering signatures.
- G. Add a note that sidewalk is required to be constructed along the frontage of each lot, or parcel, at time of development, unless a sidewalk waiver is approved.
- H. Remove the County Engineer's signature block from the Plat. The County Engineer no longer signs plats within the municipal limits of the City of Mobile.

Planned Unit Development

- 1. Any work performed in the existing ROW (right-of-way) such as driveways, sidewalks, utility connections, grading, drainage, irrigation, or landscaping will require a ROW permit from the City of Mobile Engineering Department (208-6070) and must comply with the City of Mobile Right-of-Way Construction and Administration Ordinance (Mobile City Code, Chapter 57, Article VIII).
- 2. Any and all proposed land disturbing activity within the property will need to be submitted for review and be in conformance with the Storm Water Management and Flood Control Ordinance (Mobile City Code, Chapter 17, Ordinance #65-007 & #65-045); the City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control..
- 3. Add a note to the PUD Plan stating that the proposed development must comply with all Engineering Department Policy Letters:
 - i. 5-13-2009 Policy Letter(Car wash drains and dumpster pads to drain to Sanitary Sewer System)
 - ii. 8-4-2004 Policy Letter (Video inspection of new Storm Sewer System Piping)
 - iii. 3-18-2004 Policy Letter (Additional subdivision street requirements)

Revised for the May 1st meeting:

- 1. *Any work performed in the existing ROW (right-of-way) such as driveways, sidewalks, utility connections, grading, drainage, irrigation, or landscaping will require a ROW permit from the City of Mobile Engineering Department (208-6070) and must comply with the City of Mobile Right-of-Way Construction and Administration Ordinance (Mobile City Code, Chapter 57, Article VIII).*
- 2. *Any and all proposed land disturbing activity within the property will need to be submitted for review and be in conformance with the Storm Water Management and Flood Control Ordinance (Mobile City Code, Chapter 17, Ordinance #65-007 & #65-045); the City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control..*
- 3. *Add a note to the PUD Plan stating that the proposed development must comply with the following Engineering Department Policy Letters:*
 - i. 5-13-2009 Policy Letter(Car wash drains and dumpster pads to drain to Sanitary Sewer System)*

TRAFFIC ENGINEERING**COMMENTS**

Driveway number, size, location and design to be approved by Traffic Engineering and conform to AASHTO standards. The southern most driveway on

Calhoun Road must be reduced in width, by maintaining the proposed northern radii and moving the southern radii to match the adjacent aisle width. All new on-site parking, including ADA handicap spaces, shall meet the minimum standards as defined in Section 64-6 of the City's Zoning Ordinance.

URBAN FORESTRY

COMMENTS

Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

Preservation status is to be given to the 72", 60" and 50" Live Oak Trees located on the West side of Lot 1 along Calhoun Road. Any work on or under these trees is to be permitted and coordinated with Urban Forestry; removal to be permitted only in the case of disease or impending danger.

FIRE DEPARTMENT

COMMENTS

All projects within the City of Mobile Fire Jurisdiction must comply with the requirements of the 2009 International Fire Code, as adopted by the City of Mobile.

MAWSS COMMENTS

MAWSS has only sewer services available, but a Capacity Assurance application for sewer service has not been applied for. MAWSS cannot guarantee additional service until a Capacity application is approved by Volkert Engineering Inc.

REMARKS

The applicant is requesting Planned Unit Development approval to allow multiple buildings on a single building site and Subdivision approval to create one legal lot of record.

Planned Unit Development review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating excess traffic along minor residential streets in residential districts outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD. PUD approval is **site plan specific**, thus any changes to the site plan / Subdivision plat will require approval by the Planning Commission.

Subdivision review examines the site with regard to promoting orderly development, protecting general health, safety and welfare, and ensuring that development is correlated with adjacent developments and public utilities and services, and to ensure that the subdivision meets the minimum standards set forth in the Subdivision Regulations for lot size, road frontage, lot configuration, etc.

The purpose of the subdivision request for this site is to combine multiple lots to create 1 legal lot of record. It should be pointed out that the site lies along the outer boundaries of an area that

was recently annexed into the City of Mobile in 2008 and due to the site's location along the border of the annexed area, nearly half of the proposed lot is not within the city limits.

The portion of the site which is not within the city limits was most recently before the Planning Commission at its July 17, 2009 meeting in relation to a subdivision request which was heldover from the Commission's June 19, 2009 meeting. The proposed subdivision consisted of 4 lots, however, there were concerns in relation to the proposed Lot 4 because half was inside and half was outside of the city limits. According to the Letter of Decision, the Commission decided to holdover the request until the July meeting to allow the applicant to address the following in relation to the proposed lot which crossed jurisdictional lines:

- 1) revision of the plat to eliminate the crossing of the city limits, or provision of a written statement that the owner will petition for inclusion or exclusion from the city;*

At the July 17th meeting, the Commission decided to tentatively approve the request with the following condition specifically addressed the proposed Lot 4:

- 1) rezoning of Lot 4, if brought into the city, to eliminate the split-zoning condition, prior to any applications for building or development permits for the site;*

As this site appears to have a similar issue with a lot crossing jurisdictional lines, the previous two conditions from the 2009 meetings may be appropriate for this request.

The preliminary plat illustrates the proposed 7.9 ± Acre subdivision which is located at the Northeast corner of Three Notch Road and Calhoun Road extending to the South side of Jackson Road. The portion of the site which is inside of city limits is located in Council District 4 and the applicant states that the subdivision is served by public water and sewer.

In accordance to the Subdivision Regulations, the proposed lot exceeds the minimum size requirements. The lot size in square feet and acres is illustrated on the preliminary plat as well as the 25' minimum building setback line, which is depicted along all street frontages. If approved, both should be retained on the Final Plat.

The site has frontage along three streets: Three Notch Road, Calhoun Road, and Jackson Road; thus a waiver of Section V.D.8. of the Subdivision Regulations regarding double frontage lots would be appropriate and should be required for approval.

According to the Major Street Plan component of the Comprehensive Plan, Three Notch Road is depicted as a major street and should have a right-of-way width of 100', yet, the preliminary plat only depicts a right-of-way of 80'. It should be pointed out that aerial photography indicates that Three Notch Road was recently improved with curb and gutter and widened to 5 lanes along the frontage of this site around 2002 and, as such, any immediate future widening of the street is unlikely in the near future. Typically, dedication would be required to provide 50' from centerline, but as this site is already developed, Staff recommends a 35' minimum building setback in lieu of dedication. If approved, the Final Plat and site plan should be revised to depict a 35' minimum building setback line along Three Notch Road with the provision of a corner

radius in compliance with Section V.D.6. of the Subdivision Regulations at the intersection with Calhoun Road.

Calhoun and Jackson Roads are both minor streets which do not have curb and gutter and should therefore have a right-of-way of 60'. The preliminary plat, however, depicts an existing right-of-way of 50', and as a result, dedication sufficient to provide 30' from the centerline of both streets should be required. If approved, the Final Plat and site plan should be revised to reflect dedication.

Access management is a concern due to the site's fronting 3 streets with multiple curb-cuts and existing substandard vehicle maneuverability areas. The site has 13 existing curb-cuts and as the applicant proposes to improve access, 10 curb-cuts will still remain. The southern lot line, which runs along Three Notch Road, has 4 curb-cuts and the western lot line along Calhoun Road has 3 curb-cuts, one of which appears to be over 75' \pm wide. The curb-cuts access existing and proposed parking area. It should be pointed out that some of the existing travel aisles measure only 9' \pm and, as such, Staff recommends a one-way traffic circulation if the existing parking areas are remain non-compliant. Jackson Road has 6 existing, substandard curb-cuts. The applicant proposes to eliminate 3 and retain and improve 2 of the curb-cuts. As one of the gravel drives appears to remain unimproved, the site plan should be revised to illustrate a paved drive.

As a means of access management, if approved, a note should be place on the Final Plat stating that the lot is limited to the existing curb-cuts along Three Notch and Calhoun Roads, and the site should be limited to three curb-cuts to Jackson Road, if approved, with any changes to be approved by the appropriate Engineering Department and conform to AASHTO standards.

It should be noted that there is a detention pond in the northeast corner of the proposed lot. If approved, the Final Plat and site plan should be revised to include a note stating that all common and detention areas shall be maintained by the property owners.

The geographic area defined by the city of Mobile and its planning jurisdiction, including this site, may contain Federally-listed threatened or endangered species as well as protected non-game species. Development of the site must be undertaken in compliance with all local, state and Federal regulations regarding endangered, threatened or otherwise protected species.

The PUD request for this location is to allow multiple buildings on a single building site. Part of the proposal includes the removal of 3 existing buildings and the construction of a new 2-story education/administration building. It should be pointed out that in accordance with Section 64-4.E.2.b. of the Zoning Ordinance, the total gross floor area of the proposed 2-story building appears to exceed 50% of the gross floor area of the existing buildings, thus triggering full site compliance of the tree and landscaping requirements of the Zoning Ordinance.

As proposed now, half of the site lies outside the city limits as previously mentioned and, as such, should be brought into the city limits, prior to any requests for permits or, petition for the entire church site to be de-annexed from the city, thus negating all Zoning Ordinance related issues.

The site plan illustrates several existing issues within the site. A portion of the eastern property line north of Three Notch Road appears to share access with the adjacent lot to the east, which is

not included in this request. The site plan should be revised to illustrate curbing and landscaping along the eastern property line to prevent vehicular access between the two lots. If the applicant desires to continue a shared access, the lot to the east should be included in the PUD request.

All proposed parking areas appear to be compliant in terms of travel aisle width and parking stall dimensions; however, the site plan does not depict any proposed lighting. Any new lighting on the site will have to comply with the requirements of Sections 64-4.A.2., 64-6.A.3.c., and 64-6.A.8. of the Zoning Ordinance. A note reflecting this requirement should appear on the site plan, and photometric plans will be required at the time of permitting.

It should be pointed out that the site plan does not depict any sidewalks along any street frontages. The site plan should be revised to illustrate sidewalks along all street frontages or the applicant should submit a Sidewalk Waiver request.

Also, the site plan does not depict a residential buffer nor frontage trees and landscaping. A 10' residential buffer should be required along the northwestern lot line where the property abuts an adjacent residentially developed lot and along the northeastern lot line where the property abuts a vacant lot which may be developed residentially. The site plan should be revised to illustrate the buffer in compliance with Section 64-4.D.1. of the Zoning Ordinance and illustrate and quantify full compliance with the tree and landscaping requirements of Section 64-4.E. of the Zoning Ordinance.

The site plan depicts a dumpster and it should be noted that all dumpsters shall be in compliance with Section 64-4.D.9. of the Zoning Ordinance to include being enclosed on at least three sides and connected to sanitary sewer.

RECOMMENDATION

Subdivision: The application is recommended for Holdover until the May 1st meeting, with revisions due by April 17th, to address the following:

- 1) Provision of a written statement that the owner will petition for inclusion or exclusion from the city;
- 2) Rezoning of the proposed lot if brought into the city prior to any applications for building or development permits for the site;
- 3) Revision of the plat to reflect sufficient dedication to provide 30' as measured from the centerline of both Calhoun and Jackson Roads;
- 4) Revision of the plat to depict a corner radius in compliance with Section V.D.6. of the Subdivision at the intersection of Calhoun and Three Notch Roads;
- 5) Revision of the lot size information and the 25' minimum building setback line on the plat to reflect dedication and corner radius;
- 6) Revision of the plat to depict a 35' minimum building setback line along Three Notch Road;
- 7) Revision of the plat to illustrate a 100' right-of-way for Three Notch Road; and
- 8) Revision of the plat to include a note stating that all common and detention areas shall be maintained by the property owners.

Planned Unit Development: The application is recommended for Holdover until the May 1st meeting, with revisions due by April 17th, to address the following:

- 1) Provision of a written statement that the owner will petition for inclusion or exclusion from the city;
- 2) Revision of the site plan to depict a permanent closure of the vehicular access to the adjacent property to the east with curb and landscaping, or the revision of the PUD request to include the lot to the east and shared access;
- 3) Revision of the site plan to comply with Traffic Engineering Comments: *(Driveway number, size, location and design to be approved by Traffic Engineering and conform to AASHTO standards. The southern most driveway on Calhoun Road must be reduced in width, by maintaining the proposed northern radii and moving the southern radii to match the adjacent aisle width. All new on-site parking, including ADA handicap spaces, shall meet the minimum standards as defined in Section 64-6 of the City's Zoning Ordinance.)*;
- 4) Coordination with Staff to improve onsite traffic flow and revision of the site plan to illustrate one-way traffic flow with compliant 12' travel aisles/vehicular maneuverability area where travel aisles are less than 24';
- 5) Revision of the site plan to illustrate proposed lighting along with a note stating any new lighting on the site will have to comply with the requirements of Sections 64-4.A.2., 64-6.A.3.c., and 64-6.A.8. of the Zoning Ordinance;
- 6) Depiction of a paved driveway in the northeast corner of the lot if the applicant desires to retain the curb-cut;
- 7) Depiction of sidewalks on the site plan along all street frontages or the submission of a Sidewalk Waiver request;
- 8) Depiction of a 10' residential buffer on the site plan along the northeastern and northwestern lot lines in compliance with Section 64-4.D.1. of the Zoning Ordinance;
- 9) Revision of the site plan to illustrate and quantify full compliance with the tree and landscaping requirements of Section 64-4.E. of the Zoning Ordinance;
- 10) Revision of the site plan to include a note stating that all common and detention areas shall be maintained by the property owners;
- 11) Revision of the site plan to reflect sufficient dedication to provide 30' as measured from the centerline of both Calhoun and Jackson Roads;
- 12) Revision of the site plan to depict a corner radius in compliance with Section V.D.6. of the Subdivision at the intersection of Calhoun and Three Notch Roads;
- 13) Revision of the 25' minimum building setback line on the site plan to reflect dedication and corner radius;
- 14) Revision of the site plan to depict a 35' minimum building setback line along Three Notch Road; and
- 15) Depiction of a dumpster on the site plan in compliance with Section 64-4.D.9. of the Zoning Ordinance.

Revised for the May 1st meeting:

The application was heldover from the April 3rd meeting to allow the applicant to petition the entire property to be included or excluded from the City, and make revisions to the PUD site plan and preliminary plat.

Since that time, the applicant has requested the applications be heldover an additional two weeks until the May 15th meeting so the church membership will have the proper time to decide the best course of action. As such, the original recommendations would still stand.

RECOMMENDATION

Subdivision: The application is recommended for Holdover until the May 15th meeting, with revisions due by May 2nd, to address the following:

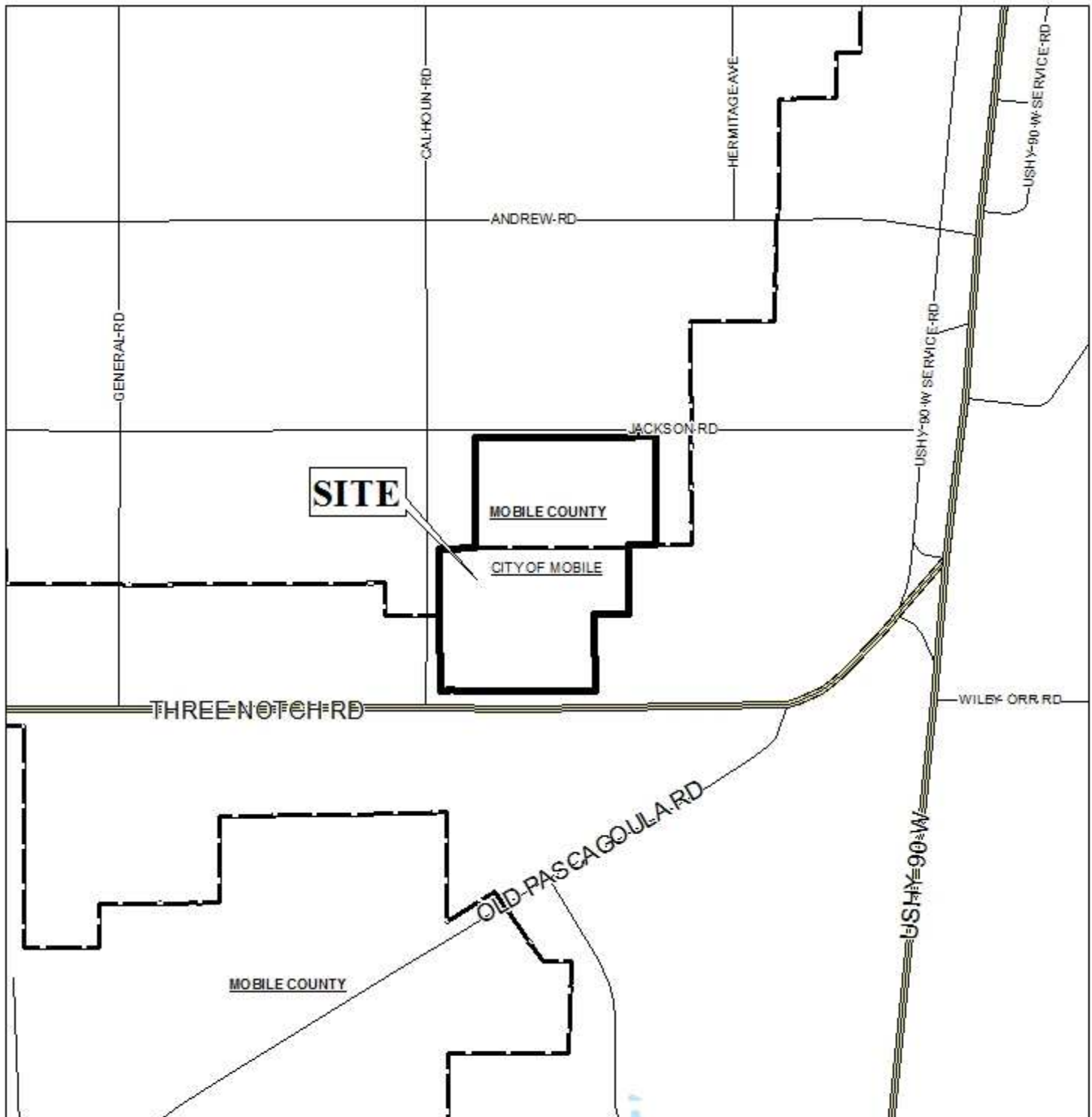
- 1) Provision of a written statement that the owner will petition for inclusion or exclusion from the city;*
- 2) Rezoning of the proposed lot if brought into the city prior to any applications for building or development permits for the site;*
- 3) Revision of the plat to reflect sufficient dedication to provide 30' as measured from the centerline of both Calhoun and Jackson Roads;*
- 4) Revision of the plat to depict a corner radius in compliance with Section V.D.6. of the Subdivision at the intersection of Calhoun and Three Notch Roads;*
- 5) Revision of the lot size information and the 25' minimum building setback line on the plat to reflect dedication and corner radius;*
- 6) Revision of the plat to depict a 35' minimum building setback line along Three Notch Road;*
- 7) Revision of the plat to illustrate a 100' right-of-way for Three Notch Road; and*
- 8) Revision of the plat to include a note stating that all common and detention areas shall be maintained by the property owners.*

Planned Unit Development: *The application is recommended for Holdover until the May 15th meeting, with revisions due by May 2nd, to address the following:*

- 1) Provision of a written statement that the owner will petition for inclusion or exclusion from the city;*
- 2) Revision of the site plan to depict a permanent closure of the vehicular access to the adjacent property to the east with curb and landscaping, or the revision of the PUD request to include the lot to the east and shared access;*
- 3) Revision of the site plan to comply with Traffic Engineering Comments: (Driveway number, size, location and design to be approved by Traffic Engineering and conform to AASHTO standards. The southern most driveway on Calhoun Road must be reduced in width, by maintaining the proposed northern radii and moving the southern radii to match the adjacent aisle width. All new on-site parking, including ADA handicap spaces, shall meet the minimum standards as defined in Section 64-6 of the City's Zoning Ordinance.);*
- 4) Coordination with Staff to improve onsite traffic flow and revision of the site plan to illustrate one-way traffic flow with compliant 12' travel aisles/vehicular maneuverability area where travel aisles are less than 24';*
- 5) Revision of the site plan to illustrate proposed lighting along with a note stating any new lighting on the site will have to comply with the requirements of Sections 64-4.A.2., 64-6.A.3.c., and 64-6.A.8. of the Zoning Ordinance;*
- 6) Depiction of a paved driveway in the northeast corner of the lot if the applicant desires to retain the curb-cut;*
- 7) Depiction of sidewalks on the site plan along all street frontages or the submission of a Sidewalk Waiver request;*
- 8) Depiction of a 10' residential buffer on the site plan along the northeastern and northwestern lot lines in compliance with Section 64-4.D.1. of the Zoning Ordinance;*

- 9) *Revision of the site plan to illustrate and quantify full compliance with the tree and landscaping requirements of Section 64-4.E. of the Zoning Ordinance;*
- 10) *Revision of the site plan to include a note stating that all common and detention areas shall be maintained by the property owners;*
- 11) *Revision of the site plan to reflect sufficient dedication to provide 30' as measured from the centerline of both Calhoun and Jackson Roads;*
- 12) *Revision of the site plan to depict a corner radius in compliance with Section V.D.6. of the Subdivision at the intersection of Calhoun and Three Notch Roads;*
- 13) *Revision of the 25' minimum building setback line on the site plan to reflect dedication and corner radius;*
- 14) *Revision of the site plan to depict a 35' minimum building setback line along Three Notch Road; and*
- 15) *Depiction of a dumpster on the site plan in compliance with Section 64-4.D.9. of the Zoning Ordinance.*

LOCATOR MAP



APPLICATION NUMBER 4 & 5 DATE May 1, 2014

APPLICANT First Baptist Church of Tillman's Corner Subdivision

REQUEST Subdivision, Planned Unit Development



PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The site is surrounded by commercial land use. Single family residences are located to the west and north of the site.

APPLICATION NUMBER 4 & 5 DATE May 1, 2014

APPLICANT First Baptist Church of Tillman's Corner Subdivision

REQUEST Subdivision, Planned Unit Development

LEGEND

R-1	R-2	R-3	R-A	R-B	H-B	T-B	B-1	LB-2	B-2	B-3	B-4	B-5	I-1	I-2
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PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The site is surrounded by commercial land use. Single family residences are located to the west and north of the site.

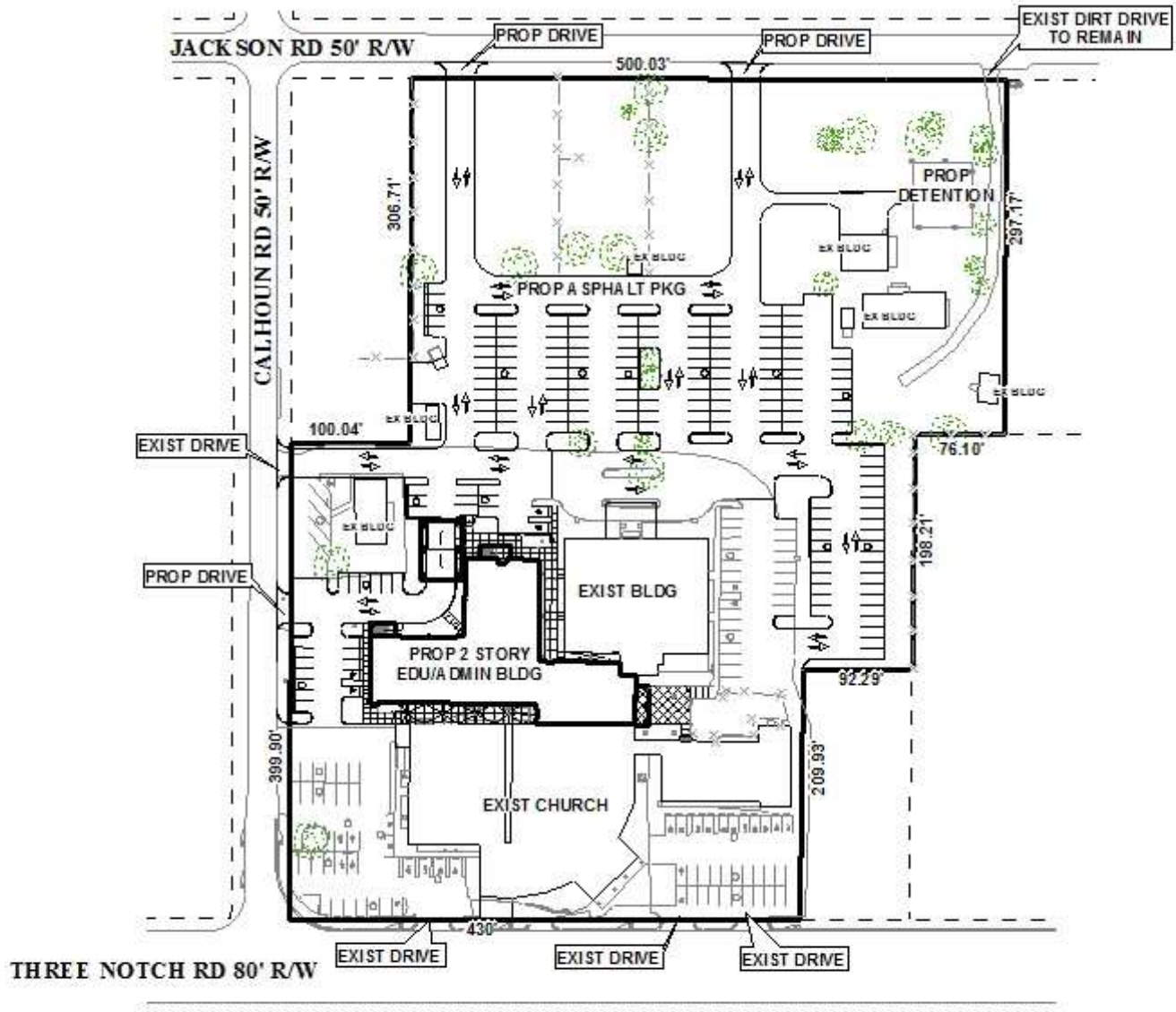
APPLICATION NUMBER 4 & 5 DATE May 1, 2014

APPLICANT First Baptist Church of Tillman's Corner Subdivision

REQUEST Subdivision, Planned Unit Development



SITE PLAN

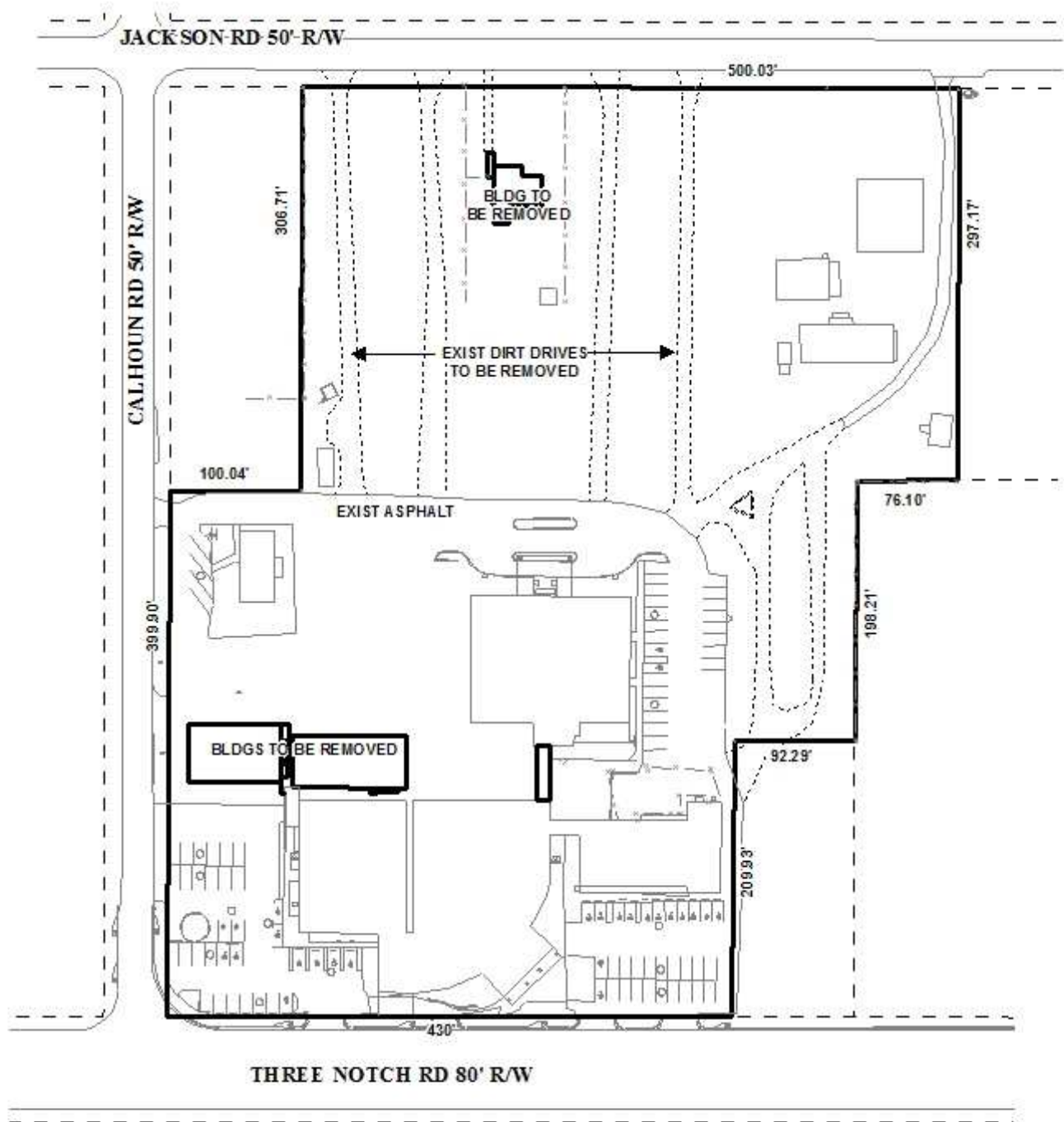


The site plan illustrates the existing improvements, proposed building, proposed parking areas, and proposed asphalt drives.

APPLICATION NUMBER 4 & 5 DATE May 1, 2014
 APPLICANT First Baptist Church of Tillman's Corner Subdivision
 REQUEST Subdivision, Planned Unit Development



DEMOLITION PLAN



APPLICATION NUMBER 4 & 5 DATE May 1, 2014
APPLICANT First Baptist Church of Tillman's Corner Subdivision
REQUEST Subdivision, Planned Unit Development

