

**PLANNED UNIT DEVELOPMENT
STAFF REPORT****Date: February 3, 2011****DEVELOPMENT NAME**

Apostolic Overcoming Holy Church of God

LOCATION2257 St. Stephens Road
(Southwest corner of St. Stephens Road and Allison Street,
extending to the Northwest corner of St. Stephens Road and
Vetter Street)**CITY COUNCIL
DISTRICT**

District 1

PRESENT ZONINGB-1, Buffer Business District, and B-3, Community
Business District**AREA OF PROPERTY**

2.76± Acres (on-site), 0.37 ± Acres (off-site)

CONTEMPLATED USEPlanned Unit Development Approval to amend a
previously approved Planned Unit Development to allow
multiple buildings on a single building site.**TIME SCHEDULE
FOR DEVELOPMENT**

Immediate

ENGINEERING**COMMENTS**

Need to show sidewalks on site plan along all property lines abutting public streets or apply for a sidewalk waiver. Must comply with all storm water and flood control ordinances. Any increase in impervious area in excess of 4,000 square feet will require detention. Any work performed in the right of way will require a right of way permit. Drainage from any new dumpster pads cannot discharge to storm sewer; must have connection to sanitary sewer.

TRAFFIC ENGINEERING**COMMENTS**

Driveway number, size, location, and design to be approved by Traffic Engineering and ALDOT and conform to AASHTO standards. To avoid conflicts, eliminate the additional parking space at the northeast driveway on Alison Street and the two spaces at the southern entrance driveway on St. Stephens Road. Eliminate the mix of angle parking and 90 degree parking. The 90 degree parking spaces back into the parked cars in some areas. Sign and mark all one way aisles.

URBAN FORESTRY**COMMENTS**

Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

FIRE DEPARTMENT**COMMENTS**

All projects must comply with the requirements of the 2003 International Fire Code, including Appendices B through D, as adopted by the City of Mobile, and the 2003 International Existing Building Code, as appropriate.

REMARKS

The applicant is requesting Planned Unit Development Approval to amend a previously approved Planned Unit Development to allow multiple buildings on a single building site.

The applicant proposes to construct a 12,700 square-foot multi-purpose building and parking expansion on the main church site with expanded off-site parking across St. Stephens Road. The main church site is an existing legal lot of record, and the off-site parking area is a metes-and-bounds parcel which has received approval as a one-lot subdivision, the plat for which was signed by Planning on December 23, 2010.

Planned Unit Development review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating excess traffic along minor residential streets in residential districts outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD. PUD approval is *site plan specific*, thus if any new construction is anticipated that will change an approved site plan, an application to amend an existing, approved PUD must be made prior to any construction activities.

The proposed multi-purpose building would be used for church activities, meetings, sports, after-school programs and community activities. The building would be attached to the church sanctuary building and would, therefore, not technically be a separate building on the site. But since the main church site also included a separate church-run domiciliary care facility building which was the subject of an Administrative Planned Unit Development Approval in 2001, that PUD was amended to allow the multi-purpose building since PUD's are site plan specific. The amended PUD was approved by the Commission on May 6, 2010, in conjunction with a one-lot subdivision to create a legal lot of record from the metes-and-bounds off-site parking area. A variance was approved May 3, 2010, allowing grass parking on the main church site and to allow the off-site parking across St. Stephens Road. However, that variance was approved despite Traffic Engineering's concerns with the site plan, but the concerns were incorporated into the conditions of approval, more specifically:

- 1) *revision of the parking layout to meet the requirements of Traffic Engineering, including the closing of curb-cuts, and to ensure that parking circulation minimizes traffic conflicts as discussed in the report;*
- 2) *consult with Traffic Engineering and ALDOT to determine if the delineation of crosswalks is possible across St. Stephens Road, and revision of the site plan to depict crosswalks if determined feasible; and*
- 3) *submission of a new application to the Board of Zoning Adjustment to address parking ratio deficits, and tree / landscaping deficits if applicable, that will occur due to required site redesign.*

Those conditions then necessitated a revised site plan which corrected the Traffic Engineering concerns, but which diminished the provided parking below the required number. A new variance request was required for the reduced parking ratio, and to amend the previously approved parking surface and off-site parking variances, and was approved by the Board of Zoning Adjustment on December 6, 2010. That approval necessitated the amending of the PUD approved on May 6, 2010, because of site plan specificity, hence this application.

Although the latest variance was approved despite staff recommendations for denial due to site over-development and the inability to meet both Traffic Engineering concerns and parking ratio requirements, the site plan submitted for this PUD still has flaws. For instance, the off-site parking area was noted by Urban Forestry in the variance report to have insufficient space to meet the perimeter tree planting requirements along its North and East sides in that insufficient space is provided for the tree root balls. The variance approval condition was “*full compliance with all other municipal codes and ordinances*”. This would pertain to the proper planting of trees and the site plan submitted for this PUD still maintains the same small area allocated for the tree plantings which was found to be insufficient in the variance site plan. And the site plan inaccurately depicts the corner radius dedication required in the one-lot subdivision approval for the off-site parking area. An accurate depiction of the dedication would require revised landscaping calculations for the site, particularly along the street frontages, as none were provided for street frontages for either the off-site parking area or the main church site; only total site calculations were provided. Therefore, the site plan should be revised to accurately depict the required corner radius dedication of the subdivision and provide landscaping calculations for both the total site areas and the street frontage areas of both properties. If compliant landscaping ratios cannot be achieved and sufficient tree planting areas cannot be provided to satisfy Urban Forestry, the applicant will need to submit a variance request to cover such.

Another point of concern is the location of the fenced mechanical yard within the required 20' side street yard setback along Vetter Street. The site plan erroneously labels this as a 25' setback, but the approval of Phillips Temple A.O.H. Subdivision in 2001 required a 25' setback only along St. Stephens Road and Allison Street; therefore, a 20' setback is required along Vetter Street. The site plan should be revised to reflect this. As no structure (including HVAC equipment and generators) or fence above 3' high can be built within the required setback, the mechanical yard should be relocated to a compliant location, or a setback variance sought, or a note should be placed on the site plan indicating that nothing higher than 3' above grade will be constructed within this area.

The site plan indicates a public sidewalk only along Allison Street. All new development requires public sidewalks to be installed along all street frontages; therefore, the site plan should be revised to illustrate proposed public sidewalks where lacking or a sidewalk waiver should be submitted.

As the site plan has deficiencies and discrepancies, this application should be heldover to the meeting of February 3rd to allow the applicant to make the proper revisions and/or variance applications or sidewalk waiver application. Such revisions and/or additional applications should be submitted by January 17th.

RECOMMENDATION

Based upon the preceding, this application is recommended for Holdover to the meeting of February 3rd, with revisions due by January 17th, to allow the applicant to address the following items:

- 1) revision of the site plan to accurately depict the required corner radius dedication of the one-lot subdivision;
- 2) revision of the site plan to provided landscaping ratios for both the total site areas and the street frontage areas of both properties, or the submission of variance requests if compliant ratios cannot be achieved and sufficient tree planting areas cannot be provided to satisfy Urban Forestry;
- 3) revision of the site plan to label the Vetter Street setback as 20' instead of 25';
- 4) revision of the site plan to relocate the fenced mechanical area to a compliant location, or the submission of a setback variance, or the placement of a note on the site plan stating that nothing higher than 3' above grade will be constructed within this area; and
- 5) revision of the site plan to provide public sidewalks along all street frontages where lacking, or the submission of a sidewalk waiver application.

Revised for the February 3rd meeting.

This application was heldover from the January 6th meeting to allow staff to review the revisions submitted by the applicant that day. The applicant submitted a revised site plan addressing items which were the basis for the recommendation of holdover from the January 6th meeting to the February 3rd meeting. The revisions were submitted to staff on January 6, shortly before the meeting time, and did not allow time for a thorough staff review.

The revised site plan and/or narrative clear most of the issues causing concerns with the initial review. There are three remaining items, tree planting areas, parking lot trees, and public sidewalks, which require further attention. The off-site parking area does not appear to have adequate area along the Dickens Avenue frontage to allow tree root-ball planting entirely within the property boundary. Urban Forestry has determined that a Right-of-Way Permit from the Engineering Department is required prior to the planting of new trees along the Dickens Avenue right-of-way. The off-site parking area is short one parking area tree, but there is an available island in the parking area which would allow one under-story tree to be planted; therefore, the site plan should be revised to add one more under-story parking tree. The tree planting plan for

the main site indicates trees proposed to be planted within the right-of-way. Urban Forestry has determined that full compliance with the landscaping and tree planting requirements of the Zoning Ordinance should be coordinated with Urban Forestry, and that a Mobile Tree Commission Permit is required before removing existing trees from the city right-of-way along Vetter Street for construction of the proposed driveway. The site plan should be revised to indicate tree planting compliance for both sites to be coordinated with Urban Forestry.

The site plan still does not indicate public sidewalks to be constructed where currently lacking. The revised narrative indicates that a Sidewalk Waiver will be submitted due to existing trees which were planted to meet landscape requirements of previous approvals for the site.

It should be noted that portions of the Traffic Engineering comments, specifically “To avoid conflicts, eliminate the additional parking space at the northeast driveway on Alison Street and the two spaces at the southern entrance driveway on St. Stephens Road. Eliminate the mix of angle parking and 90 degree parking. The 90 degree parking spaces back into the parked cars in some areas” have been eliminated by Traffic Engineering in revised comments due to the fact that this would require a new variance to be approved to reduce the parking ratio below the already substandard 214 parking spaces approved by the Board of Zoning Adjustment in December, 2010. The parking layout for this application was based upon that approved parking layout.

The note on the site plan referencing lighting compliance should be retained.

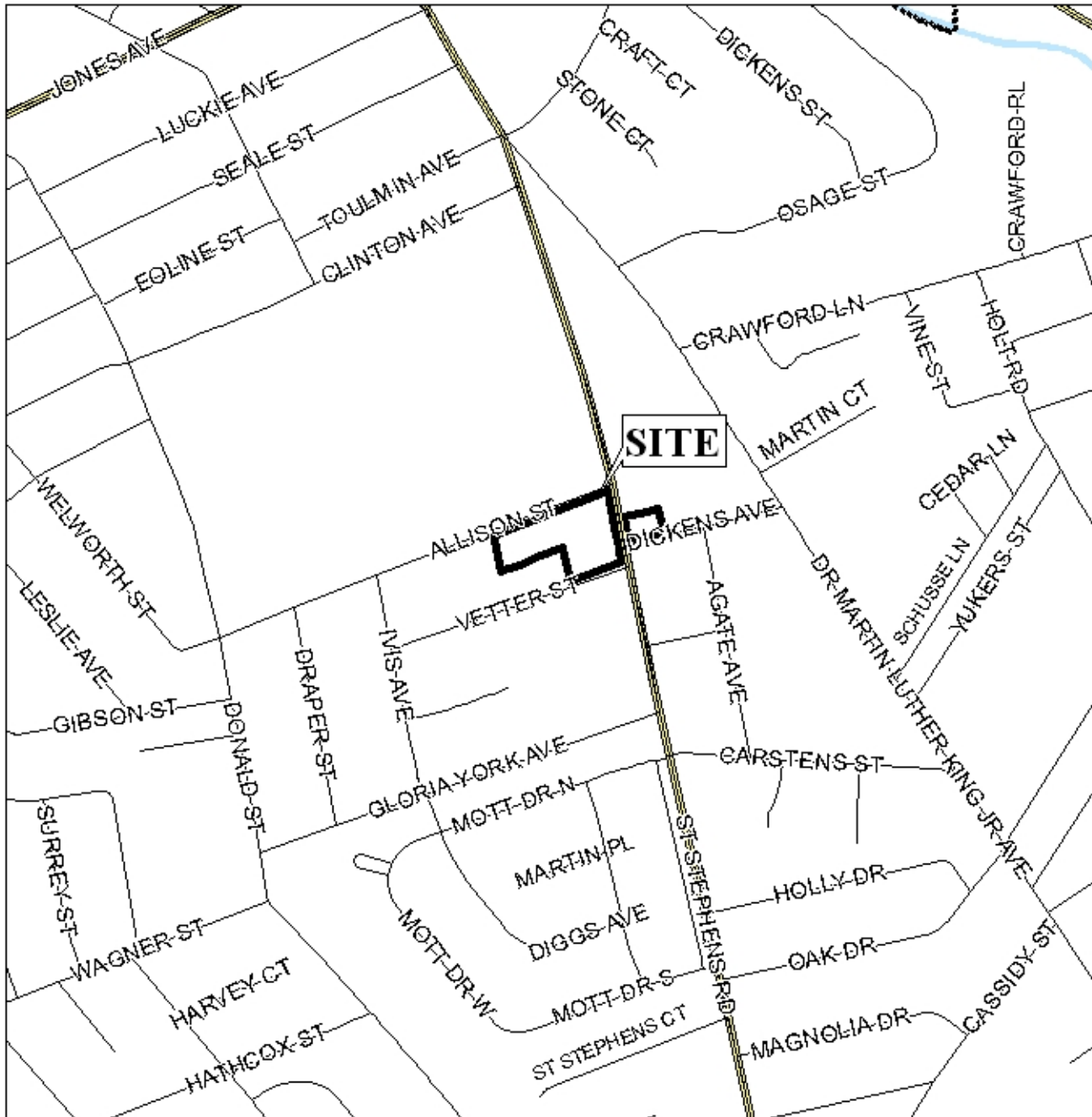
RECOMMENDATION

Based upon the preceding, this application is recommended for approval, subject to the following conditions:

- 1) revision of the landscaping and tree planting plan to show full compliance with the Zoning Ordinance, to be coordinated with Urban Forestry;*
- 2) obtaining of a Mobile Tree Commission Permit before removing existing trees from the city right-of-way along Vetter Street for construction of the proposed driveway;*
- 3) obtaining of a Right-of-Way permit for the planting of trees encroaching into the right-of-way along Dickens Avenue if such trees cannot be planted entirely within the property boundaries, to be coordinated with Urban Forestry;*
- 4) revision of the site plan to provide public sidewalks along all street frontages where lacking, or the submission of a sidewalk waiver application;*
- 5) placement of a note on the site plan stating that any equipment in the mechanical yard within the 20’ setback along Vetter Street is to be no higher than 3’ above grade;*
- 6) placement of a note on the site plan stating that the driveway number, size, location, and design are to be approved by Traffic Engineering and ALDOT and conform to AASHTO standards, and all one-way aisles are to be signed and marked;*
- 7) retention of the note on the site plan stating “lighting shall be provided and maintained during the operation of the parking area, and shall be so arranged that the source of light does not shine directly into adjacent residential properties or traffic”;*

- 8) *subject to the Engineering comments: (Need to show sidewalks on site plan along all property lines abutting public streets or apply for a sidewalk waiver. Must comply with all storm water and flood control ordinances. Any increase in impervious area in excess of 4,000 square feet will require detention. Any work performed in the right of way will require a right of way permit. Drainage from any new dumpster pads cannot discharge to storm sewer; must have connection to sanitary sewer); and*
- 9) *full compliance with all other municipal codes and ordinances.*

LOCATOR MAP



APPLICATION NUMBER 3 DATE February 3, 2011

APPLICANT Apostolic Overcoming Holy Church of God

REQUEST Planned Unit Development

N
NTS

PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The 3rd Precinct Mobile Police Department is to the north of the site, an auto repair business to the east, church buildings to the south, and single family residential units to the west.

APPLICATION NUMBER 3 DATE February 3, 2011

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LEGEND

R-1	R-2	R-3	R-A	R-B	H-B	T-B	B-1	LB-2	B-2	B-3	B-4	B-5	I-1	I-2
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 NTS

PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



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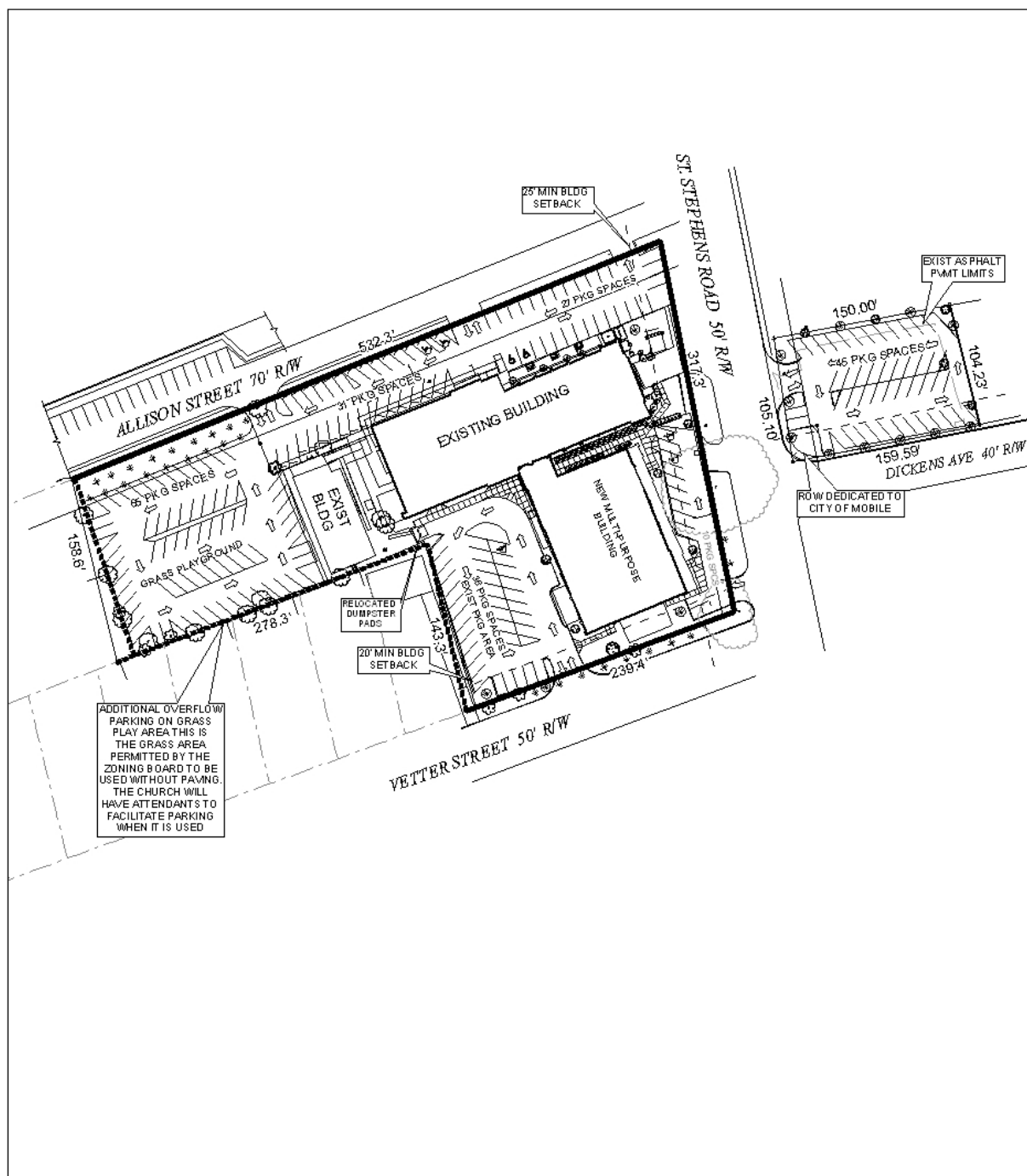
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SITE PLAN



The site plan illustrates existing and proposed parking and buildings.

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