

**PLANNED UNIT DEVELOPMENT
& SUBDIVISION STAFF REPORT****Date: June 7, 2007****DEVELOPMENT NAME**

Myers at General Bullard Subdivision

SUBDIVISION NAME

Myers at General Bullard Subdivision

LOCATION4686 Airport Boulevard
(Northeast corner of Airport Boulevard and General
Bullard Avenue)**CITY COUNCIL
DISTRICT**

District 5

PRESENT ZONING

B-2, Neighborhood Business District

AREA OF PROPERTY

1 Lot / 1.1± acres

CONTEMPLATED USEPlanned Unit Development approval to allow multiple
buildings on a single commercial building site, and Subdivision approval to create one legal lot
from a metes and bounds parcel.**TIME SCHEDULE
FOR DEVELOPMENT**

No time frame provided.

ENGINEERING**COMMENTS**

The applicant is responsible for verifying if the site contains wetlands. The site can be checked against the National Wetlands Inventory on the COM web site Environmental Viewer. If the site is included on the NWI, it is the applicant's responsibility to confirm or deny the existence of regulatory wetlands. If wetlands are present, no work allowed without a Corps of Engineers permit. Must comply with all storm water and flood control ordinances. Any work performed in the right of way will require a right of way permit.

TRAFFIC ENGINEERING**COMMENTS**

Driveway number, size, location, and design to be approved by Traffic Engineering and ALDOT and conform to AASHTO standards.

URBAN FORESTRY**COMMENTS**

Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

FIRE DEPARTMENT
COMMENTS

No comments.

REMARKS

The applicant is requesting Planned Unit Development approval to allow multiple buildings on a single commercial building site, and Subdivision approval to create one legal lot of record from a metes and bounds parcel.

The existing site consists of a convenience store with associated gas pumps that the applicant proposes to demolish and construct a larger (3,720 square foot) convenience store with six pump islands under a 63-foot by 105-foot (6,615 square foot) canopy, and a drive-thru car wash facility.

Planned Unit Development review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating excess traffic along minor residential streets in residential districts outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD. PUD approval is site plan specific, thus any changes to the site plan must be approved by the Planning Commission.

Subdivision review examines the site with regard to promoting orderly development, protecting general health, safety and welfare, and ensuring that development is correlated with adjacent developments and public utilities and services, and to ensure that the subdivision meets the minimum standards set forth in the Subdivision Regulations for lot size, road frontage, lot configuration, etc.

The site fronts onto Airport Boulevard, a major street, with a right-of-way illustrated as 155-feet, and General Bullard a minor street, with a right-of-way of 50-feet. Both streets meet the required right-of-way as required by the Major Street Component of the Comprehensive Plan. Because of the location of the facility, access management is a concern. The development should be limited to one curb-cut onto the Airport Boulevard and one curb cut to General Bullard, with the size, design and location to be approved by Traffic Engineering, and conform to AASHTO standards. Additionally, since General Bullard is a minor street a note on the Final Plat stating refueling transports be denied access to this street beyond the development.

Regarding the PUD, it appears that the proposed improvements to the site will generally meet the minimum standards of the Zoning Ordinance. There are, however, a few items of concern. First, the westernmost curb cut along Airport Boulevard should be removed as to it may cause problems with traffic circulation (exiting onto Airport Boulevard too close to an intersection). The site plan should be reviewed and revised to ensure that when refueling occurs it does not curtail or reduce the capacity of the circulation on site. Secondly, what avenues of travel will the transport refueler take, it is recommended that the designer consider revising the plan to better accommodate the maneuverings of the refueler in accordance with the daily circulation of the typical customer.

Regarding the proposed car wash, the run-off from the car-wash operation must be directed to the sanitary sewer, and an oil separator must be provided.

The site is not adjacent to residential uses; therefore not subjected to buffer requirements. In order to further minimize the impact of the site on nearby residential uses, any on-premise lighting must comply with Sections 64-4.A.2. and 64-6.A.3.c. of the Zoning Ordinance; specifically lighting must not shine directly into adjacent residential properties or into traffic, and parking areas must be illuminated during their operation at night, if applicable.

RECOMMENDATION

Planned Unit Development: Based upon the preceding, this application is recommended holdover subject to the following conditions: 1) revision of the site plan to correct any directional conflicts, due to the refueling circulation; 2) direction of any car washing run-off to an oil separator prior to discharge into the sanitary sewer system; 3) compliance with the lighting requirements of Sections 64-4.A.2. and 64-6.A.3.c. of the Zoning Ordinance; 3) provision of a revised PUD site plan prior to the signing of the final plat; 4) completion of the Subdivision process; and 5) full compliance with all other municipal codes and ordinances.

Subdivision: Based upon the preceding, this application is recommended for holdover subject to the following condition: 1) placement of a note on the final plat stating that the development is limited to one curb-cut onto Airport Boulevard and one curb cut to General Bullard, with the size, design and location to be approved Traffic Engineering, and to comply with AASHTO standards; and 2) full compliance with all other municipal codes and ordinances.

Revised for the July 5th, 2007 meeting:

The application was heldover from the June 7th meeting to allow the applicant to submit a revised site plan addressing the concerns of circulation and the number of curb cuts to the site.

The applicant submitted revised site plan illustrating the reduction of the southern curb cut along General Bullard to entrance only.

There are concerns. The need for an additional entrance only curb cut along General Bullard is unwarranted and unjustified. Allowing this one way entrance in close proximity to Airport Boulevard may hinder the flow of traffic onto General Bullard as well onto the site.

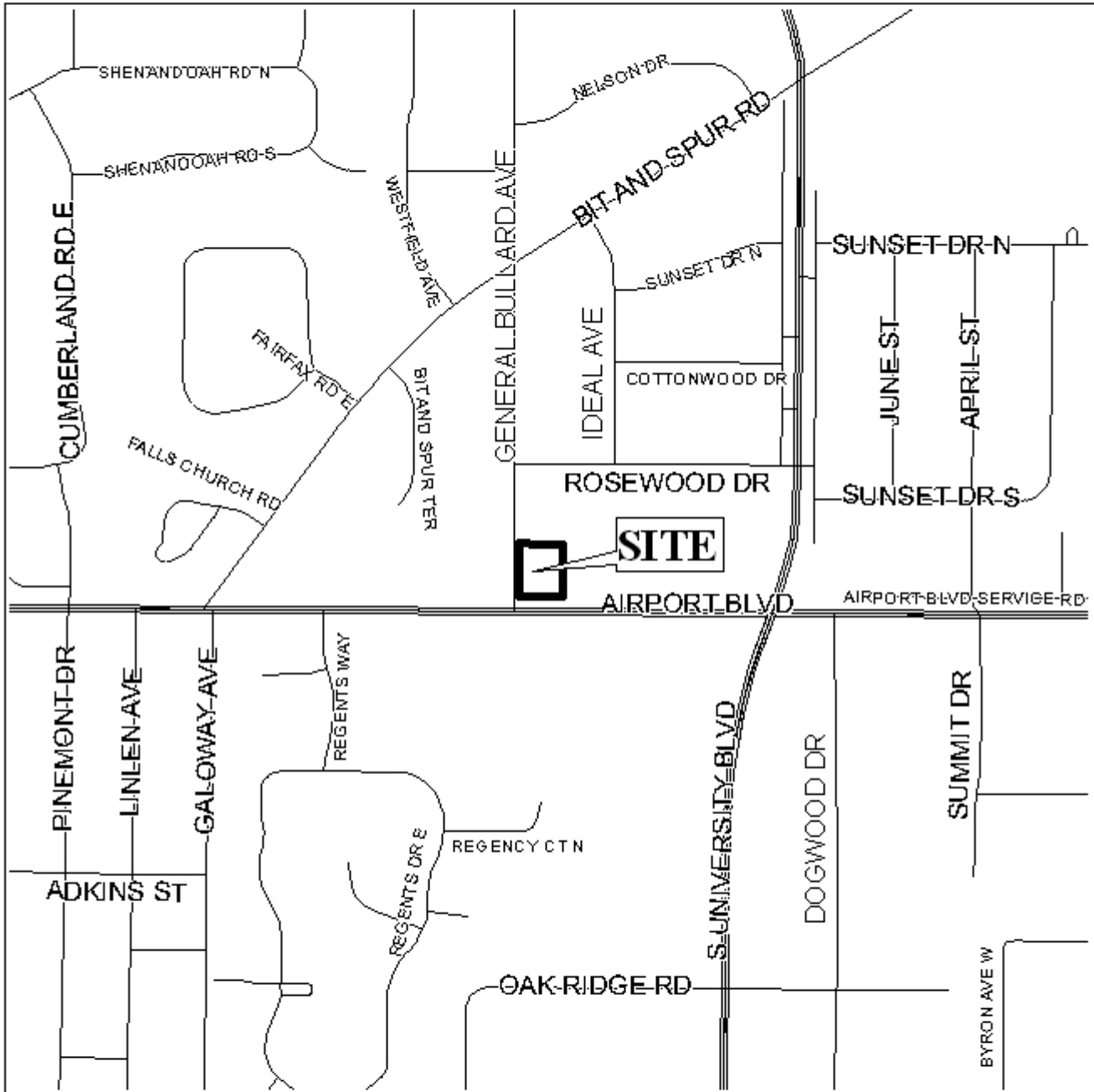
RECOMMENDATION

Planned Unit Development: The PUD request is recommended for Approval, subject to the following conditions: 1) the submission of the revised PUD plan illustrating the removal of the southern curb cut along General Bullard; 2) direction of any car washing run-off to an oil separator prior to discharge into the sanitary sewer system; 3) compliance with the lighting requirements of Sections 64-4.A.2. and 64-6.A.3.c. of the Zoning Ordinance; 3) provision of a revised PUD site plan prior to the signing of the final plat; 4)

completion of the Subdivision process; and 5) full compliance with all other municipal codes and ordinances.

Subdivision: *1) placement of a note on the final plat stating that the development is limited to two curb-cuts onto Airport Boulevard and one curb cut to General Bullard (located at the North half of the site), with the size, design and location to be approved Traffic Engineering, and to comply with AASHTO standards; and 2) full compliance with all other municipal codes and ordinances.*

LOCATOR MAP



APPLICATION NUMBER 3 and 4 DATE July 5, 2007

APPLICANT Myers at General Bullard Subdivision

REQUEST Subdivision, Planned Unit Development



NTS

PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



This site is surround by commercial landuse.
Residential landuse is located to the north of the site.

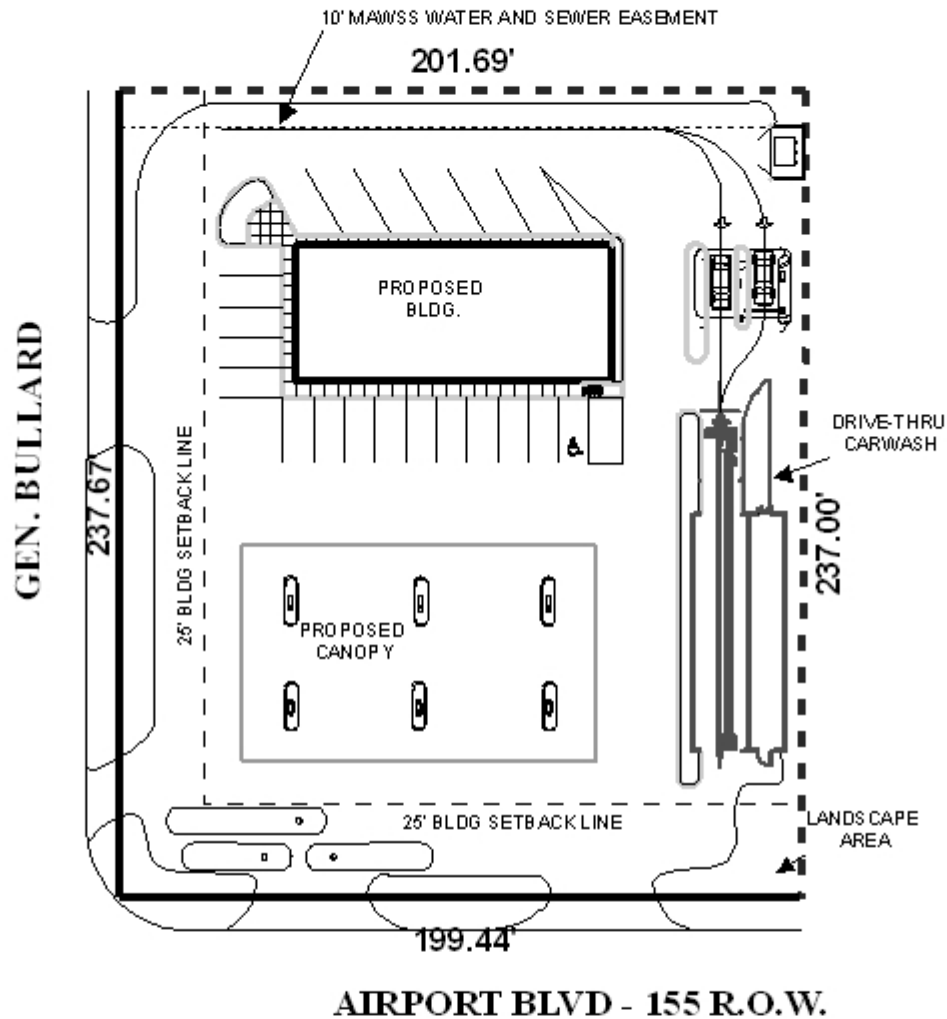
APPLICATION NUMBER 3 and 4 DATE July 5, 2007

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LEGEND NTS

SITE PLAN



This site is located on the Northeast corner of Airport Boulevard and General Bullard. The plan illustrates the proposed buildings, drive-thru carwash and proposed curb cuts.

APPLICATION NUMBER 3 and 4 DATE July 5, 2007
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NTS