

**PLANNED UNIT DEVELOPMENT****Date: August 6, 2009****DEVELOPMENT NAME**

Valenti Southeast Realty

**LOCATION**

3215 Airport Boulevard  
(South side of Airport Boulevard, extending to the North side of Airport Boulevard Service Road, 670'± West of Bel Air Boulevard).

**CITY COUNCIL  
DISTRICT**

Council District 5

**PRESENT  
ZONING DISTRICT**

B-3, Community Business

**CONTEMPLATED USE**

Planned Unit Development Approval to allow reduced front yard setbacks for a restaurant.

**TIME SCHEDULE  
FOR DEVELOPMENT**

Immediately

**ENGINEERING****COMMENTS**

Must comply with all storm water and flood control ordinances. Any work performed in the right of way will require a right of way permit. Drainage from any new dumpster pads cannot discharge to storm sewer; must have connection to sanitary sewer.

**TRAFFIC ENGINEERING****COMMENTS**

Driveway number, size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards. Handicap ramps and crosswalks will be the responsibility of the developer, with their location and design to be coordinated with Traffic Engineering. The crosswalk on the northeast side of the development should be relocated to the northwest side of the development where there are existing pedestrian signals. Handicap ramps should meet current ADA requirement and City of Mobile standards. Crosswalks and any other proposed striping changes should meet MUTCD standards.

**URBAN FORESTRY****COMMENTS**

Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

**FIRE DEPARTMENT****COMMENTS**

All projects must comply with the requirements of the 2003 International Fire Code, including Appendices B through D, as adopted by the City of Mobile, and the 2003 International Existing Building Code, as appropriate.

**REMARKS**

The applicant is seeking Planned Unit Development Approval to allow a reduced front yard setback for a restaurant.

The subject site came under review in February 2008 for Planned Unit Development and Sidewalk Waiver Request. Both were approved but construction of the restaurant was not begun and the PUD expired. The applicant again proposes to construct the restaurant, hence this application.

The site is currently a parking lot with approximately 103 spaces, and is bounded on all sides by properties generally associated with either the Colonial Bel Air Mall or Springdale Mall developments, in a B-3 district. The site is immediately bounded by Airport Boulevard to the North, a service road to the South, and mall entrance roads to the East and West.

Planned Unit Development review, stated in Section 64-5. of the Zoning Ordinance, examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating excess traffic along minor residential streets in residential districts outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD. PUD approval is site plan specific, thus any changes to the site plan must be approved by the Planning Commission.

The applicant is requesting a reduction in the required 25-foot building setback along the Southern boundary to accommodate the proposed 6,270 square foot restaurant. The main portion of the building will be approximately 13 feet from the Southern property boundary; however, the enclosure for the dumpsters will be only 6 feet from the Southern property boundary. The site plan submitted is almost identical to the site plan submitted with the previous PUD for this site, but with some minor changes incorporating conditions of approval of the previous PUD. It now indicates a 25-foot minimum building setback line from all street frontages, except for where the building and dumpster enclosure would encroach into the normal setback. As per the 2008 approval of the Sidewalk Waiver, pedestrian crosswalks are proposed across Airport Boulevard at the Northwest corner of the property, and across the North entrance to the mall service road on the East side of the property. The relocation of the existing Airport Boulevard cross walk at the Northeast corner to the Northwest corner of the site is in agreement with the Traffic Engineering Comments. Handicap ramps should meet current ADA requirement and City of Mobile standards. Crosswalks and any other proposed striping changes should meet MUTCD standards.

It also appears that some of the curbing associated with the parking area may cross the property boundary on the South and West sides. Any new construction must take place fully within the property, with the exception of the proposed pedestrian cross walks.

The site plan depicts the building and 63 parking spaces. Interior circulation appears to meet minimum dimensional requirements as it relates to the width of access aisles, but the layout of the parking does not appear to provide much accommodation for vehicular turning radii, thus setting the stage for congestion and accidents on the site during parking maneuvering and circulation – thus the site *may* be being overbuilt. The congestion caused by the site design may result in back-ups into adjacent roads. The number of parking spaces, however, *does* meet the minimum quantity requirements for the restaurant. Bumper stops are indicated in the West side parking stalls to prevent parked vehicles protruding into the right-of-way adjacent to the stalls.

As previously mentioned, the site has frontage on Airport Boulevard, Airport Boulevard Service Road, and two mall entrance drives, all of which are public streets with adequate rights-of-way. Since Airport Boulevard is a proposed major street, and as the site is entirely bounded by streets, access management for the site is a concern. Traffic counts from June 2009 indicate that approximately 28,748 vehicles per day passed the site along Airport Boulevard. The site plan submitted with the application indicates the two existing curb cuts onto the Airport Boulevard Service Road are to be closed and replaced with two new curb cuts onto the same aligning with internal traffic aisles, as per the 2008 PUD approval conditions. Staff believes that two curb-cuts are appropriate for the site, and that the location of the curb-cuts along the Southern boundary would be most appropriate for the site due to the high traffic volumes along Airport Boulevard as well as the entrance and exit roads for the mall property. Therefore the site should be limited to a maximum of two curb-cuts along the Southern boundary of the property. The size, design and location of the new curb-cuts are to be approved by Traffic Engineering and designed in conformance with AASHTO standards. The existing curb cuts which are removed should be landscaped to match adjacent right-of-way.

Full compliance with the landscaping and tree planting requirements would be required and a landscaping and tree planting plan with the application indicates full compliance with both is proposed.

The geographic area defined by the city of Mobile and its planning jurisdiction, including this site, may contain Federally-listed threatened or endangered species as well as protected non-game species. Development of the site must be undertaken in compliance with all local, state and Federal regulations regarding endangered, threatened or otherwise protected species.

### **RECOMMENDATION**

Planned Unit Development: Based upon the preceding, this application is recommended for Approval, subject to the following conditions:

- 1) the site is limited to the two proposed curb cuts to the Airport Boulevard Service Road;
- 2) subject to the Traffic Engineering Comments: *(Driveway number, size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards. Handicap ramps and crosswalks will be the responsibility of the developer, with their location and design to be coordinated with Traffic Engineering. The crosswalk on the northeast side of the development should be relocated to the northwest side of the development where there are existing pedestrian signals. Handicap ramps should meet*

*current ADA requirement and City of Mobile standards. Crosswalks and any other proposed striping changes should meet MUTCD standards).*

- 3) existing curb cuts are to be closed with curbing and landscaping to match adjacent right-of-way;
- 4) the site is denied any curb cuts onto Airport Boulevard;
- 5) full compliance with the landscaping and tree planting requirements; and
- 6) subject to the Engineering Comments: *(Must comply with all storm water and flood control ordinances. Any work performed in the right of way will require a right of way permit. Drainage from any new dumpster pads cannot discharge to storm sewer; must have connection to sanitary sewer).*

*Revised for the September 3<sup>rd</sup> meeting:*

*This application was heldover from the August 6<sup>th</sup> meeting due to questions relating to Traffic Engineering. As no representative from Traffic Engineering was in attendance, there was no one to answer the questions.*

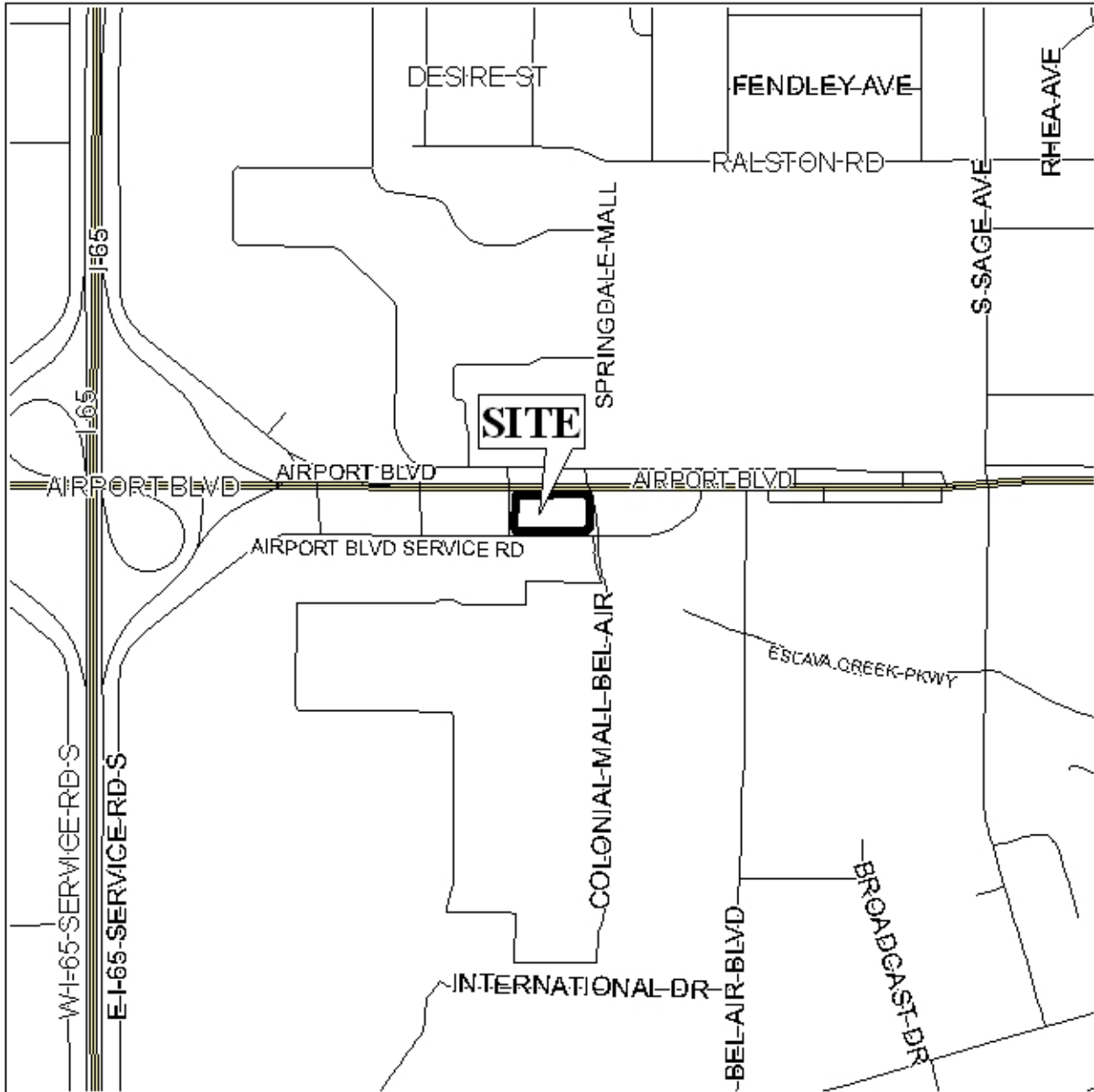
*There has been no new information or revised site plan submitted concerning this application which would change the previous review and recommendation.*

### **RECOMMENDATION**

*Planned Unit Development: Based upon the preceding, this application is recommended for Approval, subject to the following conditions:*

- 1) *the site is limited to the two proposed curb cuts to the Airport Boulevard Service Road;*
- 2) *subject to the Traffic Engineering Comments: (Driveway number, size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards. Handicap ramps and crosswalks will be the responsibility of the developer, with their location and design to be coordinated with Traffic Engineering. The crosswalk on the northeast side of the development should be relocated to the northwest side of the development where there are existing pedestrian signals. Handicap ramps should meet current ADA requirement and City of Mobile standards. Crosswalks and any other proposed striping changes should meet MUTCD standards).*
- 3) *existing curb cuts are to be closed with curbing and landscaping to match adjacent right-of-way;*
- 4) *the site is denied any curb cuts onto Airport Boulevard;*
- 5) *full compliance with the landscaping and tree planting requirements; and*
- 6) *subject to the Engineering Comments: (Must comply with all storm water and flood control ordinances. Any work performed in the right of way will require a right of way permit. Drainage from any new dumpster pads cannot discharge to storm sewer; must have connection to sanitary sewer).*

## LOCATOR MAP



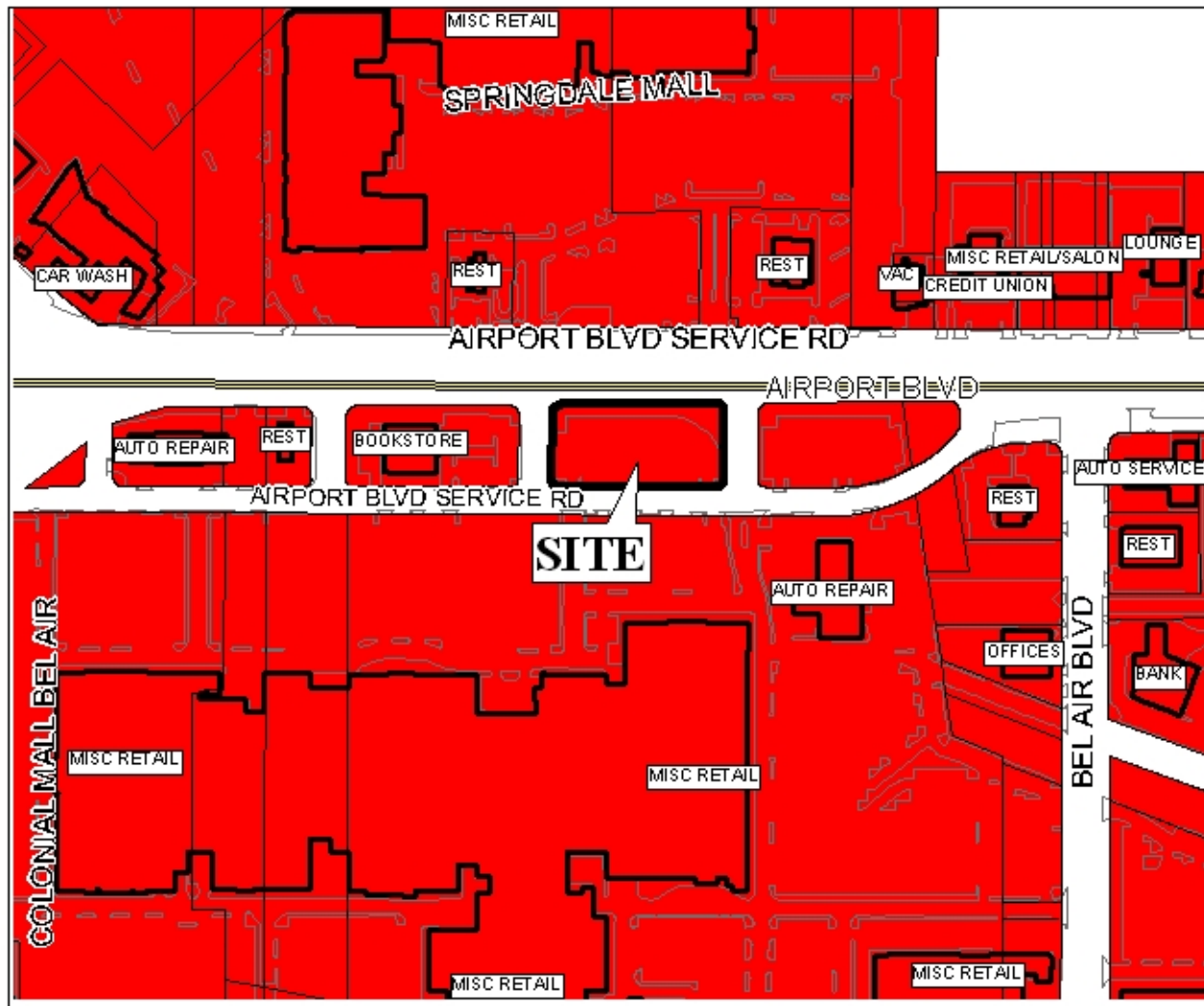
APPLICATION NUMBER 2 DATE September 3, 2009

APPLICANT Valenti Southeast Realty

REQUEST Planned Unit Development

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NTS

# PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The site is surrounded by miscellaneous commercial land use.

APPLICATION NUMBER 2 DATE September 3, 2009

APPLICANT Valenti Southeast Realty

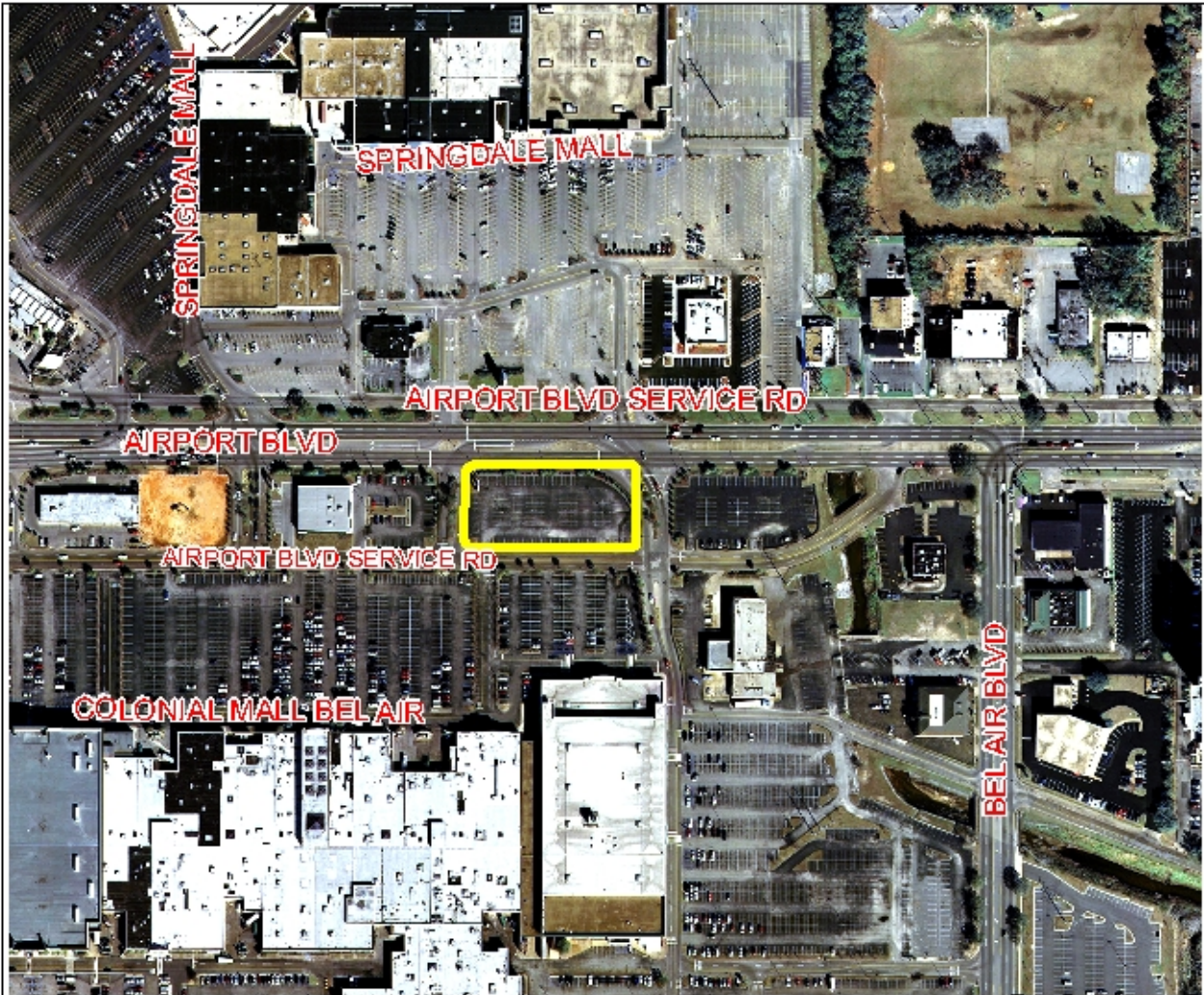
REQUEST Planned Unit Development

LEGEND

R-1	R-2	R-3	R-4	R-B	H-B	T-B	B-1	LB-2	B-2	B-3	B-4	B-5	I-1	I-2	NTS
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## PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



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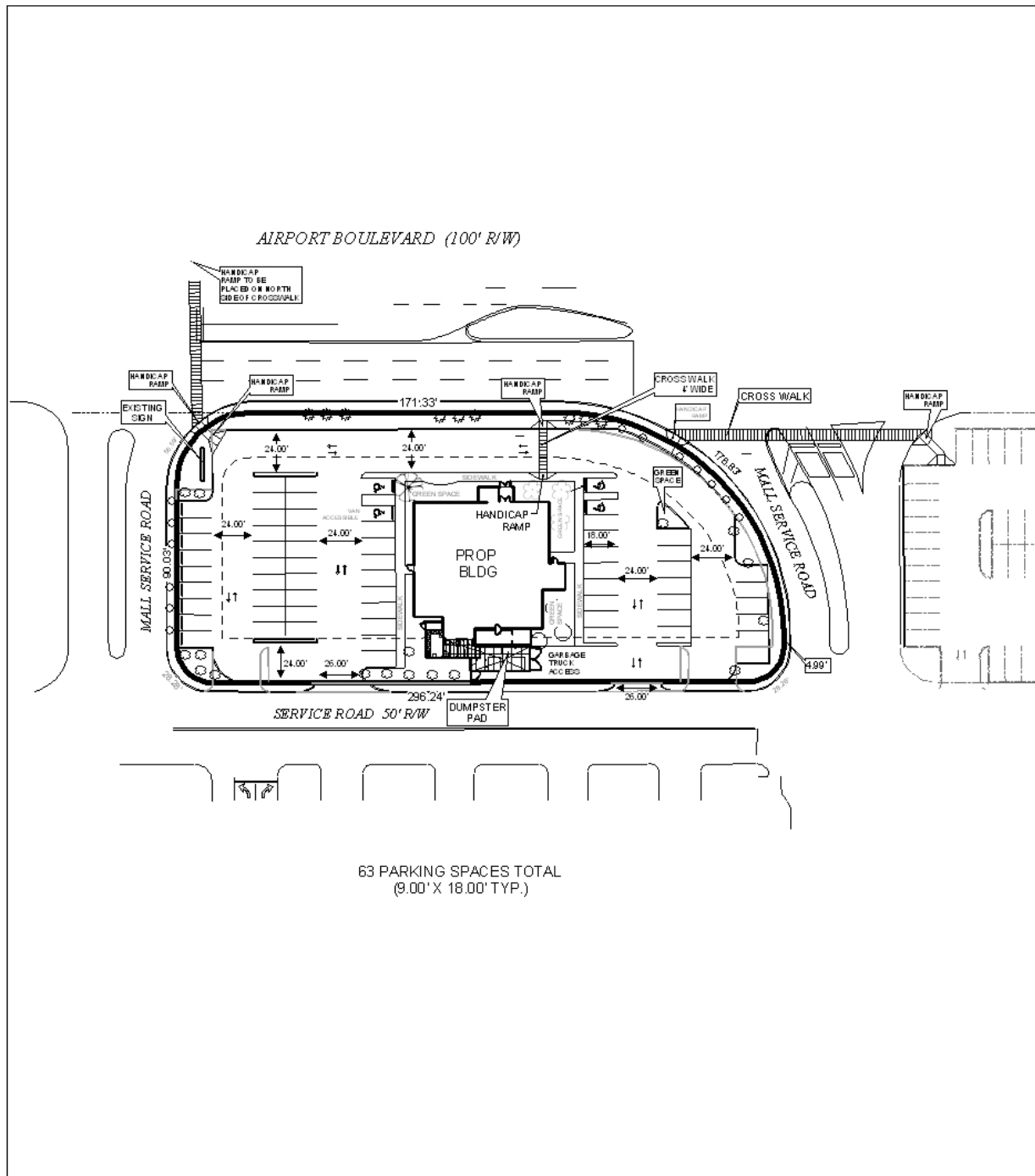
APPLICANT Valenti Southeast Realty

REQUEST Planned Unit Development



NTS

# SITE PLAN



The site plan illustrates the proposed building, parking, green spaces, dumpster pad, and garbage truck access.

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 APPLICANT Valenti Southeast Realty  
 REQUEST Planned Unit Development



NTS