

**PLANNING APPROVAL,
PLANNED UNIT DEVELOPMENT &
SUBDIVISION STAFF REPORT****Date: September 18, 2014**

<u>NAME</u>	Smith Memorial AME Church
<u>SUBDIVISION NAME</u>	Smith Memorial AME Church
<u>LOCATION</u>	6501 Felhorn Road North (Southwest corner of Felhorn Road North and Roslyn Drive West)
<u>CITY COUNCIL DISTRICT</u>	District 4
<u>PRESENT ZONING</u>	R-1, Single-Family Residential District
<u>AREA OF PROPERTY</u>	1 Lot / 1.0± Acre
<u>CONTEMPLATED USE</u>	Planning Approval to allow the expansion of an existing church in an R-1, Single-Family Residential District, Planned Unit Development approval to allow a reduced side yard setback, and Subdivision approval to create one legal lot of record.
<u>TIME SCHEDULE FOR DEVELOPMENT</u>	None given..

ENGINEERING**COMMENTS**

Subdivision: The following comments should be addressed prior to submitting the FINAL PLAT for acceptance and signature by the City Engineer:

- A. Provide all of the required information on the SUBDIVISION PLAT (i.e. signature blocks, signatures, certification statements, written legal description, required notes, legend, scale, bearings and distances) that is required by the current Alabama State Board of Licensure for Professional Engineers and Land Surveyors.
- B. Add a note to the SUBDIVISION PLAT stating that a Land Disturbance permit will be required for any land disturbing activity in accordance with the of the Storm Water Management and Flood Control Ordinance (Mobile City Code, Chapter 17 , Ordinance #65-007 & #65-045), latest edition. Storm water detention will be required for any future addition(s) and/or land disturbing activity.

- C. Add a note to the Plat stating that the approval of all applicable federal, state, and local agencies (including all storm water runoff, wetland and floodplain requirements) will be required prior to the issuance of a Land Disturbance permit.
- D. Revision of the plat to label each lot with its size in acres and square feet, or the furnishing of a table on the Plat providing the same information.
- E. Indicate the existing and proposed property lines of Lot 12, Block 37 Hillsdale Heights, 2nd Addition to clarify the 30' wide strip of property on the south side of this proposed subdivision.
- F. Add a signature block for the Owner, Notary Public, Planning Commission, Traffic Engineer, and City Engineer.
- G. Provide the Surveyor's, Owner's (notarized), Planning Commission, and Traffic Engineering signatures.
- H. Provide a copy of the Final Plat along with the original when submitting for City Engineer signature.

Planned Unit Development: ADD THE FOLLOWING NOTES TO THE PUD SITE PLAN:

1. Any work performed in the existing ROW (right-of-way) such as driveways, sidewalks, utility connections, grading, drainage, irrigation, or landscaping will require a ROW permit from the City of Mobile Engineering Department (208-6070) and must comply with the City of Mobile Right-of-Way Construction and Administration Ordinance (Mobile City Code, Chapter 57, Article VIII).
2. A Land Disturbance Permit application shall be submitted for any proposed land disturbing activity with the property. A complete set of construction plans including, but not limited to, drainage, utilities, grading, storm water detention systems, paving, and all above ground structures, will need to be included with the Land Disturbance permit. This Permit must be submitted, approved, and issued prior to beginning any of the construction work.
3. Any and all proposed land disturbing activity within the property will need to be submitted for review and be in conformance with the Storm Water Management and Flood Control Ordinance (Mobile City Code, Chapter 17, Ordinance #65-007 & #65-045); the City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control.

The proposed development must comply with all Engineering Department Policy Letters

TRAFFIC ENGINEERING

COMMENTS

Site is limited to the existing curb cuts on Felhorn Road N and Roslyn Drive W, with size, location and design to be approved by Traffic Engineering and conform to AASHTO standards. In the future when the parking lot is resurfaced, the parking lot layout should be reconfigured to provide as many conforming parking spaces as possible on this site.

URBAN FORESTRY**COMMENTS**

Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

FIRE DEPARTMENT**COMMENTS**

All projects within the City of Mobile Fire Jurisdiction must comply with the requirements of the 2009 International Fire Code, as adopted by the City of Mobile.

REMARKS

The applicant is requesting Planning Approval to allow the expansion of an existing church in an R-1, Single-Family Residential District, Planned Unit Development approval to allow a reduced side yard setback, and Subdivision approval to create one legal lot. Churches require Planning Approval when located in R-1 districts.

The review required for Planning Approval examines the applicant's location and site plan with regard to transportation, parking and access, public utilities and facilities, traffic congestion and hazard, and to determine if the proposal is in harmony with the orderly and appropriate development of the district.

Planned Unit Development review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating excess traffic along minor residential streets in residential districts outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD.

It is very important to note that the Planning Approval and PUD review are site plan specific; therefore *any* future changes to the overall site plan must be submitted for PUD review. Additionally, if the site plan is changed from what is approved by the Planning Commission, a new application for Planning Approval will be required.

The applicant proposes to construct a new kitchen addition onto the existing church building thus requiring Planning Approval. The addition is proposed to encroach into the 25' side street yard setback, thus requiring a PUD, and all contiguous properties owned by the applicant are proposed to be incorporated into one lot, thus the one-lot Subdivision.

The church received Planning Approval in 1994 for a new sanctuary addition and associated off-site parking across Roslyn Drive. The sanctuary addition was constructed, but the off-site parking area was never developed. Since then, the church has acquired additional properties adjacent to the South of the site and expanded the parking area onto one of those properties without any review process and created a nonconforming parking situation. In 1996, a Front

Yard Setback Variance was requested to allow construction of a canopy within 5' of the front property line, but the request was denied by the Board of Zoning Adjustment.

The narrative supplied indicates that the church has a 400-person seating capacity which would require 100 on-site parking spaces. However, only 75 are provided, and the site has no room to provide expanded parking facilities. Some new spaces could possibly be obtained by a re-configuration of the existing parking lot, but no such attempt was made for the PUD and Planning Approval applications. Even with a more-compliant parking plan, the site would still not provide space to construct the proposed kitchen expansion to meet the required 25' side yard setback along Rosalyn Avenue. The same situation would arise attempting to build to the front toward Felhorn Drive. The proposed addition is indicated to be 25' by 26'-5" and indicated to extend to within 12'-4" of the Rosalyn Avenue property line, thus the setback relief of the PUD request.

A comparison of the PUD and Planning Approval site plans with the Subdivision survey indicates a discrepancy in both the front and side street yard setbacks for the existing church building. The site plans indicate the sanctuary portion right at the 25' front setback and the East wing at a scaled 38'± front setback, with the Rosalyn Avenue (East end) of the remaining portion at a 37'-4" setback off the Rosalyn Avenue property line. However, the survey indicates a 22' front setback for the sanctuary and a 36'-11" front setback for the East wing, with a 36' setback for the remaining portion off Rosalyn Avenue. The narrative provided does not give a specific requested setback off the Rosalyn Avenue right-of-way. Since PUD's and Planning Approvals are site plan specific, and since staff cannot determine the actual setback requested due to the discrepancies, it is recommended that these applications be heldover to the meeting of September 18th to allow the discrepancies to be cleared.

Also, in verbal conversations with the applicant's professionals, it has been indicated that the retaining wall and parking area encroachments onto adjacent properties along the West and South property lines have been corrected as is indicated on the site plans but not on the survey for the preliminary plat. However, no permits were obtained for such. In the event the encroachments have been corrected, the plat should be revised to indicate such.

RECOMMENDATION

Subdivision: Based upon the preceding, this request is recommended for holdover to the September 18th meeting, with revisions due no later than September 3rd, to allow the applicant to address the following items:

- 1) revision of the plat as necessary so that existing front and side street setbacks can be corrected, and proposed setbacks for the kitchen addition can be shown, to match those of the Planned Unit Development and Planning Approval site plans;
- 2) revision of the plat to indicate the correction of the parking area and retaining wall encroachments onto adjacent properties to the West and South, if such have been corrected.

Planned Unit Development: Based upon the preceding, this request is recommended for holdover to the September 18th meeting, with revisions due no later than September 3rd, to allow the applicant to address the following items:

- 1) revision of the site plan as necessary so that existing and proposed front and side street setbacks will match those of the Subdivision survey;
- 2) revision of the narrative to explain precisely what side street setback is requested.

Planning Approval: Based upon the preceding, this request is recommended for holdover to the September 18th meeting, with revision due no later than September 3rd, to allow the applicant to address the following items:

- 1) revision of the site plan as necessary so that existing and proposed front and side street setbacks will match those of the Subdivision survey;
- 2) revision of the narrative to explain precisely what side street setback is requested.

Revised for the September 18th meeting:

These applications were heldover from the August 21st meeting to allow the applicant to make revisions as listed above. Such revisions were submitted to allow further review.

Most of the discrepancies between the survey and the site plans have been cleared, most importantly relating to the actual location of the existing building on the site and the proposed kitchen addition. However, it should be noted that on the Planning Approval and PUD site plans, the location of the building has not been adjusted to scale, and the revised dimensions are noted as not being to scale.

The proposed kitchen would be for fellowship activities associated with the existing congregation. As parking requirements for churches are based upon sanctuary seating and not upon square footage, and no increase in sanctuary seating is proposed, there would be no increase in the number of required on site parking spaces.

The kitchen addition is proposed to be 11'-0" off the East property line along Roslyn Avenue, and would contain approximately 660 square feet. The end of the building where the addition is proposed is currently 36' off the Roslyn Avenue property line and, if maintaining the required 25' setback off Roslyn Avenue, only an 11' extension would be allowed. That would mean that, in order to have the 660 square feet proposed, the addition would have to be 60' long stretching North and South. As the existing building is only about 3' from the parking lot sidewalk to the South, the major part of the addition would have to extend to the North toward Felhorn Road North and come within approximately 5' of the front property line, creating a much greater setback encroachment than proposed along Roslyn Avenue. To build to the South of the existing building would be impracticable due to the existing nonconforming parking lot and would diminish the already insufficient number of parking spaces.

In light of the fact that the kitchen would not increase the number of required parking spaces on site, and the fact that there is not sufficient area elsewhere to build the addition, approval of the reduced side yard setback along Roslyn Avenue would seem in order.

As previously mentioned, the church received Planning Approval in 1994 for the sanctuary expansion and for an off-site parking area across Roslyn Avenue. The sanctuary expansion was constructed, but the off-site parking area was never developed. The revised site plan submitted as a condition of that approval also indicates expanded parking onto the adjacent lot to the South and such was developed some time between 1997 and 2002, according to aerial photographs of the site. But 2002 aerials also indicate a further expansion to the South beyond that of the 1994 Planning Approval, and without additional Planning Approval, and encompassing the current site area. Needless to say, certain aspects of the approved site plan were not adhered to with the over-all expansion of the site to its current configuration, thus the current non-compliant parking layout has many access and maneuvering problems

It should be noted that the stated sanctuary seating was 204 in the 1994 Planning Approval, and the site plan was approved for 39 on-site parking spaces, with 20 spaces in the proposed off-site parking area. The unapproved expanded parking area provides approximately 34 spaces. But the seating capacity is now stated to be 400 persons which would require 100 on-site parking spaces. As 75 are provided, the applicant requests relief from the required 100 on-site parking spaces. As many of those provided are noncompliant in size and there are three large Live Oak trees (60", 40" and 36") within the parking lot, the re-configuration of the parking area and traffic flow to compliant standards could be impracticable; however, the previous sanctuary expansion was based upon the provision of compliant parking areas. As per the Traffic Engineering comments, if the parking lot is resurfaced in the future, the parking lot layout should be reconfigured to provide as many conforming parking spaces as possible on this site. It should be noted that the Subdivision plat indicates the three trees, but the site plans omit the 40" Live Oak tree.

Pertaining to the parking area, the applicant asks relief from other site compliance aspects, such as providing wheel stops and parking lot lighting requirements. However, the provision of bumper stops should be required at least on perimeter nose-in parking spaces where there is adjoining residential property. Regarding lighting, as the site has 75 parking spaces and is adjacent to residential zoning, the Zoning Ordinance requires that parking areas with more than 10 spaces and that are used at night must be illuminated. Any existing and any new lighting must comply with the Zoning Ordinance requirements, including the provision that "lighting facilities used to illuminate signs, parking areas, or for other purposes shall be so arranged that the source of light does not shine directly into adjacent residence properties or into traffic." Since the site has more than 25 parking spaces and is adjacent to residential zoning, the applicant should bring the site into lighting compliance and submit information regarding new parking area lighting fixtures (including photometrics), indicate the locations on the site plan, and submit information regarding how any existing site lighting will be modified to comply with the requirements of Sections 64-4.A.2. and 64-6.A.3.c. and 64-6.A.8. of the Zoning Ordinance.

It should be noted that the site must be brought into compliance, after-the-fact, with the storm water detention requirements of the City of Mobile. Because of this, other modifications to the

parking area may be possible, such as bringing the parking space size and circulation into compliance.

An existing exposed dumpster is indicated on the site plans, and the applicant asks for relief from the dumpster enclosure requirements of the Zoning Ordinance. However, the newly-enacted Litter Ordinance will require dumpster compliance, although that portion of the ordinance is deferred until March, 1, 2015. This will require site compliance for existing dumpsters as well as new ones, and as no hardship is shown to be associated with the requirements, the dumpster should be brought into compliance with Section 64-4.D.9. of the Zoning Ordinance as well as the Litter Ordinance. Also, as the expansion is for a kitchen facility, the dumpster should be connected to the sanitary sewer.

The site is abutted by R-1, Single-Family Residential zoning; therefore, a 6' high wooden privacy fence should be provided in those areas along the property line.

The applicant also asks for relief from the frontage tree planting requirements of the Zoning Ordinance. One overstory heritage tree is required for every 30 feet of linear street frontage. In this case, six trees would be required along Felhorn Drive North, one for the corner radius, and seven along Roslyn Avenue. There is already one Live Oak tree within the 25' setback along each of those streets. The applicant has not indicated a hardship associated with the planting of frontage trees, and sufficient area exists along Felhorn Drive North and the corner radius to provide frontage trees since there are no overhead power lines in those areas. Overhead power lines are present along Roslyn Avenue; therefore only Live Oaks may be planted within the first 15' of the property line. However, due to the limited area between the proposed kitchen addition and the Roslyn Avenue property line and the lack of area to provide further planting areas within the 25' setback along Roslyn Avenue, modified tree planting requirements should be coordinated with Urban Forestry.

There is a discrepancy between the site plans and the Subdivision plat as to the size of the largest Live Oak tree located within the rear parking area. The site plans indicate 80" in diameter while the plat indicates 60" in diameter. The size should be verified and revised on whichever is erroneous. Regardless, no perimeter tree plantings would be required since sufficient tree credits are present even if this tree is 60" in diameter along with the 40" Live Oak in the rear parking area shown on the survey. No parking understory trees are indicated, and four would be required. However, once the size of the Live Oak tree in question is determined, this may count as some credit toward the parking tree requirement. If not, parking trees should be coordinated with Urban Forestry.

The site has frontage on Felhorn Drive North and Roslyn Avenue, both minor streets with compliant 50' rights-of-way and a 25' corner radius; therefore, no dedication would be required. As on the preliminary plat, the Final Plat should indicate the 25' minimum building setback line along both street frontages and blocked around the limits of the proposed kitchen addition.

As a means of access management, a note should be required on the Final Plat stating that the lot is limited to the existing curb cuts along Felhorn Drive North and Roslyn Avenue, with the

size, location and design to be approved by Traffic Engineering and conform to AASHTO standards.

As on the preliminary plat, the Final Plat should retain the lot size in both square feet and acres, or a table should be furnished on the Final Plat providing the same information.

A discrepancy exists between the Subdivision plat and the site plans with regard to the paved area along the South and West portions of the parking area. The survey indicates an encroachment onto the adjacent properties but this is not indicated on the site plans. Verification should be submitted prior to signing the Final Plat that this encroachment has been corrected.

As the site adjoins residential sites, a note should be required on the Final Plat stating that a buffer, in compliance with Section V.A.8. of the Subdivision Regulations, must be provided where the lot adjoins residentially developed property.

The geographic area defined by the city of Mobile and its planning jurisdiction, including this site, may contain Federally-listed threatened or endangered species as well as protected non-game species. Development of the site must be undertaken in compliance with all local, state and Federal regulations regarding endangered, threatened or otherwise protected species.

Finally, since the original sanctuary seating for the 1994 Planning Approval was stated as 204, and the current application states the seating to be 400, the applicant should verify that the increased capacity is not in violation of any Building or Fire codes.

RECOMMENDATION:

Subdivision: *Based on the preceding, this application is recommended for tentative approval, subject to the following conditions:*

- 1) retention of the 25' minimum building setback line along Felhorn Drive North and Roslyn Avenue, blocked around the area of the proposed kitchen addition;*
- 2) placement of a note on the Final Plat stating that the lot is limited to the existing curb cuts along Felhorn Drive North and Roslyn Avenue, with the size, location and design to be approved by Traffic Engineering and conform to AASHTO standards;*
- 3) retention of the lot size in both square feet and acres, or the furnishing of a table on the Final Plat providing the same information;*
- 4) verification that the parking lot pavement encroachment onto adjacent properties along the South and West sides of the parking area have been corrected and revision of the plat to indicate such prior to signing the Final Plat;*
- 5) placement of a note on the Final Plat stating that a buffer, in compliance with Section V.A.8. of the Subdivision Regulations, must be provided where the lot adjoins residentially developed property;*
- 6) placement of a note on the Final Plat stating that development of the site must be undertaken in compliance with all local, state and Federal regulations regarding endangered, threatened or otherwise protected species;*

- 7) verification and revision of, if necessary, the size of the Live Oak tree on the Western portion of the main parking area (60" or 80");
- 8) subject to the Engineering comments: (The following comments should be addressed prior to submitting the FINAL PLAT for acceptance and signature by the City Engineer:
 - A. Provide all of the required information on the SUBDIVISION PLAT (i.e. signature blocks, signatures, certification statements, written legal description, required notes, legend, scale, bearings and distances) that is required by the current Alabama State Board of Licensure for Professional Engineers and Land Surveyors.
 - B. Add a note to the SUBDIVISION PLAT stating that a Land Disturbance permit will be required for any land disturbing activity in accordance with the of the Storm Water Management and Flood Control Ordinance (Mobile City Code, Chapter 17 , Ordinance #65-007 & #65-045), latest edition. Storm water detention will be required for any future addition(s) and/or land disturbing activity.
 - C. Add a note to the Plat stating that the approval of all applicable federal, state, and local agencies (including all storm water runoff, wetland and floodplain requirements) will be required prior to the issuance of a Land Disturbance permit.
 - D. Revision of the plat to label each lot with its size in acres and square feet, or the furnishing of a table on the Plat providing the same information.
 - E. Indicate the existing and proposed property lines of Lot 12, Block 37 Hillsdale Heights, 2nd Addition to clarify the 30' wide strip of property on the south side of this proposed subdivision.
 - F. Add a signature block for the Owner, Notary Public, Planning Commission, Traffic Engineer, and City Engineer.
 - G. Provide the Surveyor's, Owner's (notarized), Planning Commission, and Traffic Engineering signatures.
 - H. Provide a copy of the Final Plat along with the original when submitting for City Engineer signature.);
- 9) subject to the Traffic Engineering comments: (Site is limited to the existing curb cuts on Felhorn Road N and Roslyn Drive W, with size, location and design to be approved by Traffic Engineering and conform to AASHTO standards. In the future when the parking lot is resurfaced, the parking lot layout should be reconfigured to provide as many conforming parking spaces as possible on this site.);
- 10) subject to the Urban Forestry comments: (Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64);
- 11) subject to the Fire Department comments: (All projects within the City of Mobile Fire Jurisdiction must comply with the requirements of the 2009 International Fire Code, as adopted by the City of Mobile.); and
- 12) submission and approval of three (3) copies of revised Planning Approval and PUD site plans to the Planning Division indicating compliance with all conditions of approval for those applications prior to signing the Final Plat.

Planned Unit Development: Based on the preceding, this application is recommended for approval, subject to the following conditions:

- 1) revision of the site plan to locate the building to scale to match the Subdivision plat;
- 2) revision of the site plan to show stormwater detention facilities for all impervious areas added since 1984;

- 3) *if removal of asphalt is necessary for stormwater compliance, then the parking area should be re-striped to provide fully compliant parking spaces and access aisles;*
- 4) *verification that the 400-person seating capacity is compliant with the Building and Fire code requirements;*
- 5) *revision of the site plan to show the 40" Live Oak tree within the parking area as indicated on the Subdivision plat;*
- 6) *verification and revision of, if necessary, the size of the Live Oak tree on the Western portion of the main parking area (60" or 80");*
- 7) *revision of the site plan to provide bumper stops in the nose-in parking stalls along the perimeter where there is adjoining residential property;*
- 8) *revision of the site plan to bring the parking lot lighting into compliance with the requirements of Sections 64-4.A.2., 64-6.A.3.c and 64-6.A.8. of the Zoning Ordinance, to include a photometric plan, if re-striping of the parking area is required;*
- 9) *revision of the site plan to bring the dumpster into compliance with Section 64-4.D.9. of the Zoning Ordinance and the Litter Ordinance;*
- 10) *revision of the site plan to provide a 6' high wooden privacy fence along the interior perimeter of the site where there is abutting residential zoning;*
- 11) *revision of the site plan to provide frontage and parking trees, as much as practicable, to be coordinated with Urban Forestry;*
- 12) *retention of the 25' building setback line along the street frontages blocked around the proposed kitchen addition;*
- 13) *placement of a note on the site plan stating that the site is limited to the existing curb cuts along Felhorn Drive North and Roslyn Avenue, with the size, location and design to be approved by Traffic Engineering and conform to AASHTO standards;*
- 14) *revision of the site plan to label the site with its size in both square feet and acres, or the furnishing of a table on the site plan providing the same information;*
- 15) *subject to the Engineering comments: (ADD THE FOLLOWING NOTES TO THE PUD SITE PLAN: 1. Any work performed in the existing ROW (right-of-way) such as driveways, sidewalks, utility connections, grading, drainage, irrigation, or landscaping will require a ROW permit from the City of Mobile Engineering Department (208-6070) and must comply with the City of Mobile Right-of-Way Construction and Administration Ordinance (Mobile City Code, Chapter 57, Article VIII). 2. A Land Disturbance Permit application shall be submitted for any proposed land disturbing activity with the property. A complete set of construction plans including, but not limited to, drainage, utilities, grading, storm water detention systems, paving, and all above ground structures, will need to be included with the Land Disturbance permit. This Permit must be submitted, approved, and issued prior to beginning any of the construction work. 3. Any and all proposed land disturbing activity within the property will need to be submitted for review and be in conformance with the Storm Water Management and Flood Control Ordinance (Mobile City Code, Chapter 17, Ordinance #65-007 & #65-045); the City of Mobile, Alabama Flood Plain Management Plan (1984); and, the Rules For Erosion and Sedimentation Control and Storm Water Runoff Control. The proposed development must comply with all Engineering Department Policy Letters);*
- 13) *subject to the Traffic Engineering comments: (Site is limited to the existing curb cuts on Felhorn Road N and Roslyn Drive W, with size, location and design to be approved by Traffic Engineering and conform to AASHTO standards. In the future when the parking*

lot is resurfaced, the parking lot layout should be reconfigured to provide as many conforming parking spaces as possible on this site.);

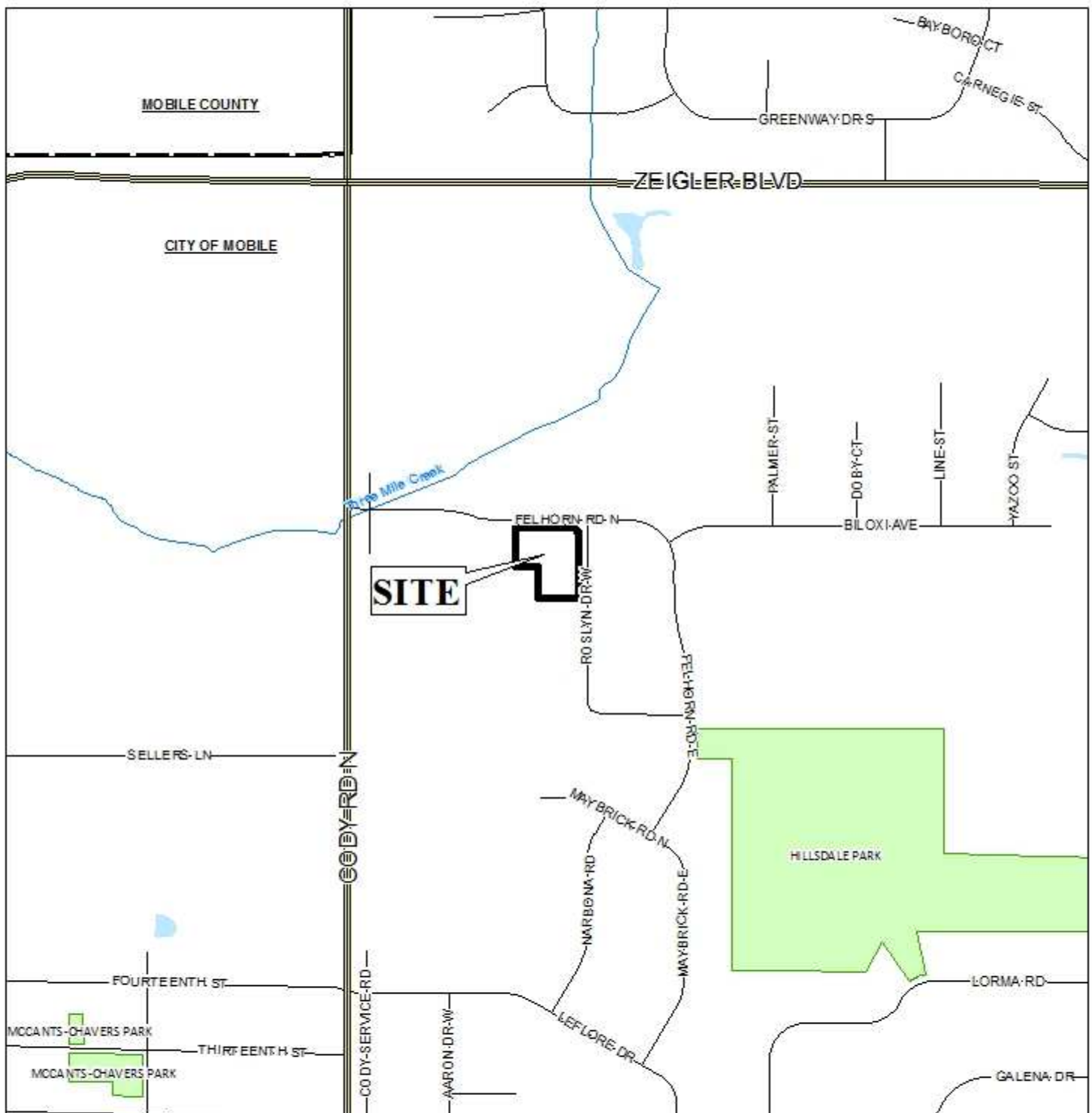
- 14) subject to the Urban Forestry comments: (Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64);*
- 15) subject to the Fire Department comments: (All projects within the City of Mobile Fire Jurisdiction must comply with the requirements of the 2009 International Fire Code, as adopted by the City of Mobile.);*
- 16) full compliance with all other municipal codes and ordinances, including applicable Building and Fire codes; and*
- 17) submission and approval of three (3) copies of a revised site plan to the Planning Division indicating compliance with all conditions of approval prior to signing the Final Plat.*

Planning Approval: *Based on the preceding, this application is recommended for approval, subject to the following conditions:*

- 1) revision of the site plan to locate the building to scale to match the Subdivision plat;*
- 2) revision of the site plan to show stormwater detention facilities for all impervious areas added since 1984;*
- 3) if removal of asphalt is necessary for stormwater compliance, then the parking area should be re-striped to provide fully compliant parking spaces and access aisles;*
- 4) verification that the 400-person seating capacity is compliant with the Building and Fire code requirements;*
- 5) revision of the site plan to show the 40" Live Oak tree within the parking area as indicated on the Subdivision plat;*
- 6) verification and revision of, if necessary, the size of the Live Oak tree on the Western portion of the main parking area (60" or 80");*
- 7) revision of the site plan to provide bumper stops in the nose-in parking stalls along the perimeter where there is adjoining residential property;*
- 8) revision of the site plan to bring the parking lot lighting into compliance with the requirements of Sections 64-4.A.2., 64-6.A.3.c and 64-6.A.8. of the Zoning Ordinance, to include a photometric plan, if re-striping of the parking area is required;*
- 9) revision of the site plan to bring the dumpster into compliance with Section 64-4.D.9. of the Zoning Ordinance and the Litter Ordinance;*
- 10) revision of the site plan to provide a 6' high wooden privacy fence along the interior perimeter of the site where there is abutting residential zoning;*
- 11) revision of the site plan to provide frontage and parking trees, as much as practicable, to be coordinated with Urban Forestry;*
- 12) retention of the 25' building setback line along the street frontages blocked around the proposed kitchen addition;*
- 13) placement of a note on the site plan stating that the site is limited to the existing curb cuts along Felhorn Drive North and Roslyn Avenue, with the size, location and design to be approved by Traffic Engineering and conform to AASHTO standards;*
- 14) revision of the site plan to label the site with its size in both square feet and acres, or the furnishing of a table on the site plan providing the same information;*

- 15) *subject to the Traffic Engineering comments: (Site is limited to the existing curb cuts on Felhorn Road N and Roslyn Drive W, with size, location and design to be approved by Traffic Engineering and conform to AASHTO standards. In the future when the parking lot is resurfaced, the parking lot layout should be reconfigured to provide as many conforming parking spaces as possible on this site.);*
- 16) *subject to the Urban Forestry comments: (Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64);*
- 17) *subject to the Fire Department comments: (All projects within the City of Mobile Fire Jurisdiction must comply with the requirements of the 2009 International Fire Code, as adopted by the City of Mobile.);*
- 18) *full compliance with all other municipal codes and ordinances, including applicable Building and Fire codes; and*
- 19) *submission and approval of three (3) copies of a revised site plan to the Planning Division indicating compliance with all conditions of approval prior to signing the Final Plat.*

LOCATOR MAP



APPLICATION NUMBER 2 DATE September 18, 2014

APPLICANT Smith Memorial AME Church Subdivision

REQUEST Subdivision, PUD, Planning Approval



PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The site is surrounded by single family residential units.

APPLICATION NUMBER 2 DATE September 18, 2014

APPLICANT Smith Memorial AME Church Subdivision

REQUEST Subdivision, PUD, Planning Approval

R-A	R-3	T-B	B-2	B-5	MUN	SD-WH	T5.1
R-1	R-B	B-1	B-3	I-1	OPEN	T3	T5.2
R-2	H-B	LB-2	B-4	I-2	SD	T4	T6



PLANNING COMMISSION VICINITY MAP - EXISTING ZONING

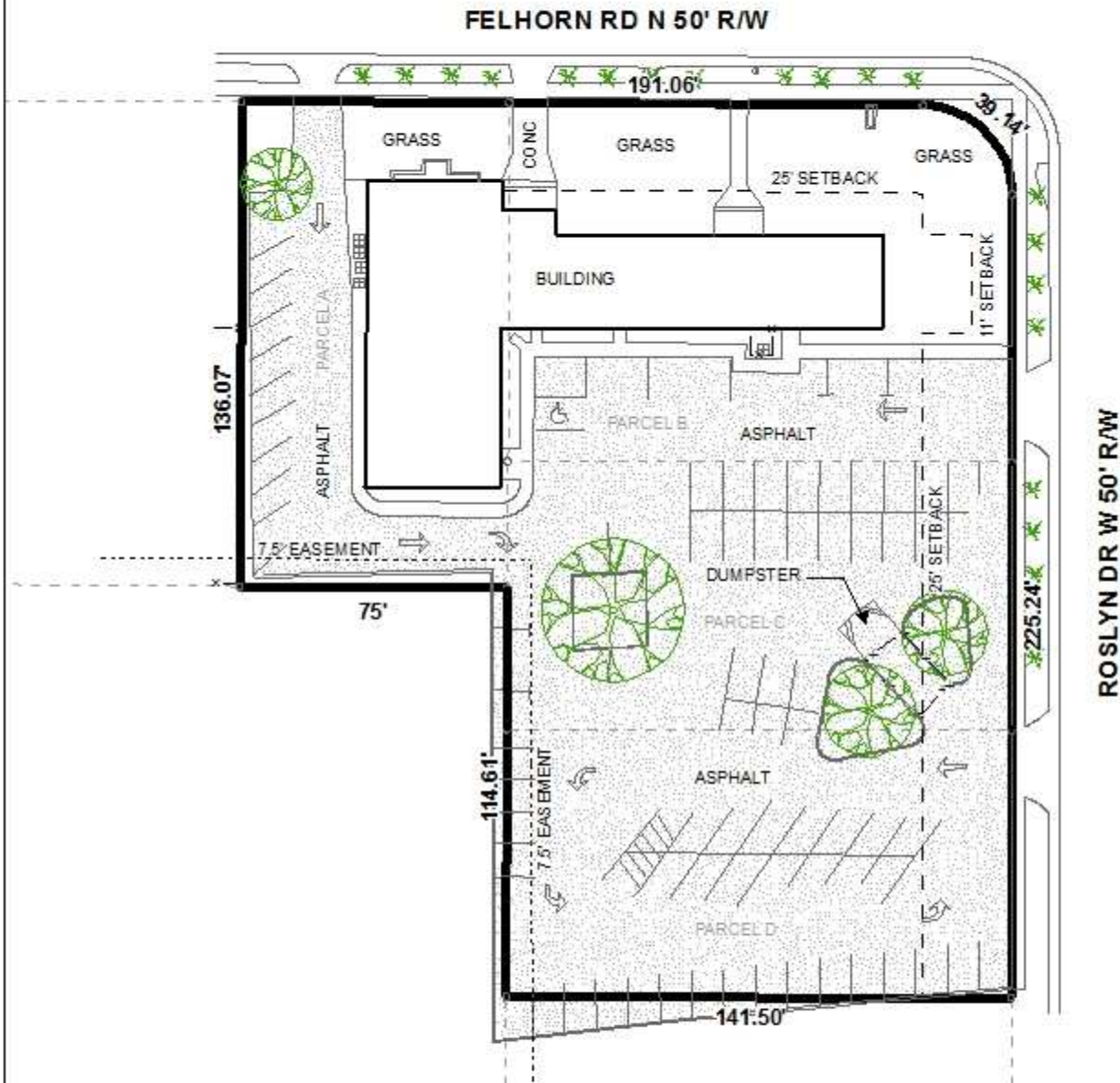


The site is surrounded by single family residential units.

APPLICATION NUMBER 2 DATE September 18, 2014
 APPLICANT Smith Memorial AME Church Subdivision
 REQUEST Subdivision, PUD, Planning Approval



EXISTING SITE PLAN

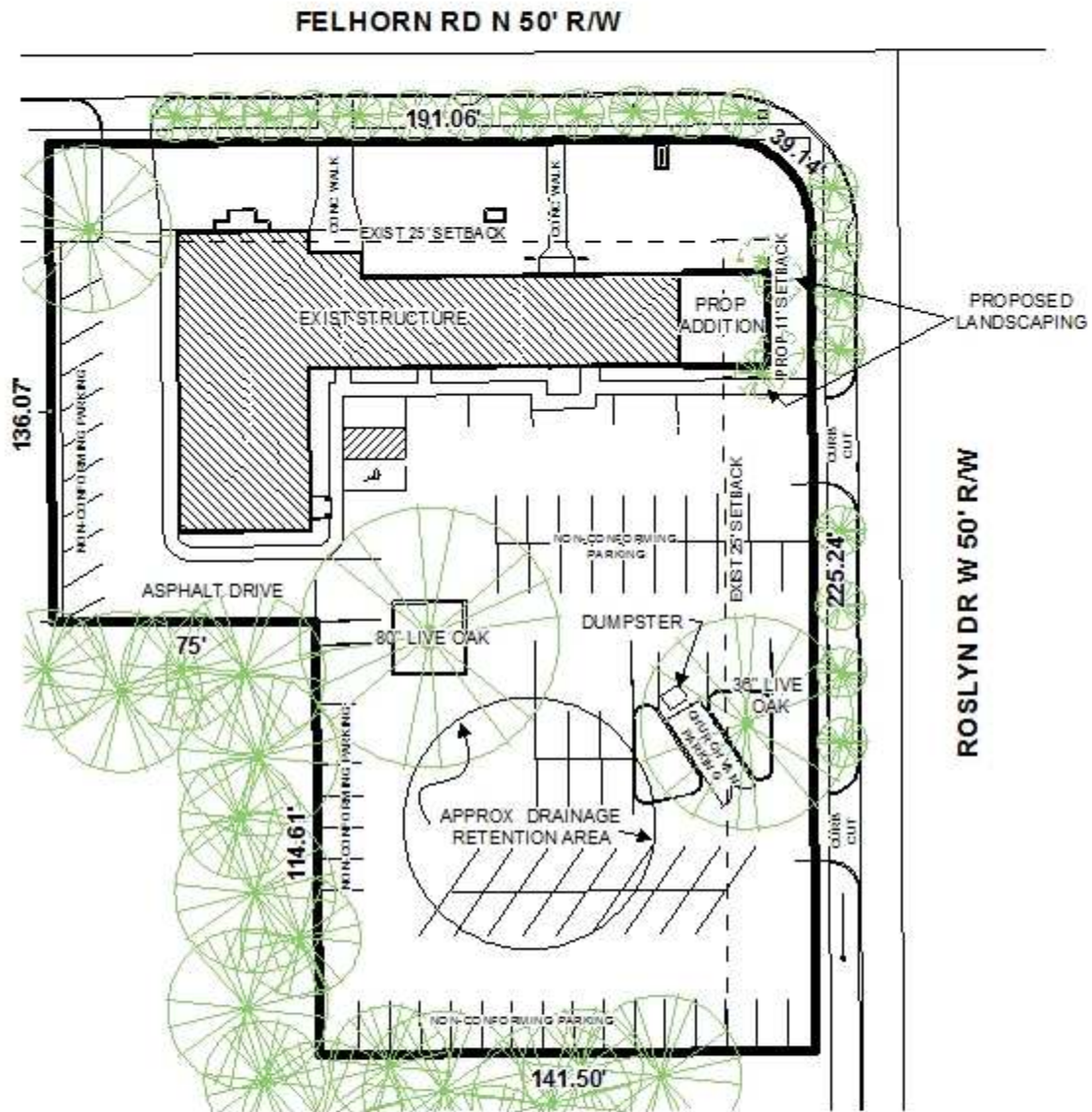


The site plan illustrates the existing building, parking, easement, and trees.

APPLICATION NUMBER 2 DATE September 18, 2014
 APPLICANT Smith Memorial AME Church Subdivision
 REQUEST Subdivision, PUD, Planning Approval



PROPOSED SITE PLAN



The site plan illustrates the existing building, proposed building addition, existing and requested setback, parking, and trees.

APPLICATION NUMBER 2 DATE September 18, 2014

APPLICANT Smith Memorial AME Church Subdivision

REQUEST Subdivision, PUD, Planning Approval

