

GRANDVIEW APARTMENTS SUBDIVISION

Engineering Comments: Site is located in a VE Zone, therefore; per FEMA Regulations, no fill nor grading allowed without a CLOMR. It is the responsibility of the applicant to look up the site in the City of Mobile (COM) GIS system and verify if NWI wetlands are depicted on the site. If the COM GIS show wetlands on the site, it is the responsibility of the applicant to confirm or deny the existence of wetlands on-site. If wetlands are present, they should be depicted on plans and/or plat, and no work/disturbance can be performed without a permit from the Corps of Engineers. Must comply with all storm water and flood control ordinances. Cannot concentrate storm water runoff to an adjacent property without a release agreement or a private drainage easement. Must provide detention for any impervious area added since 1984 in excess of 4,000 square feet. Any work performed in the right of way will require a right of way permit.

Fire-Rescue Department Comments: Subdivision layout and design must comply with the requirements of the 2003 International Fire Code, including Appendices B through D, as adopted by the City of Mobile. Commercial buildings and sites within the City of Mobile Fire Jurisdiction must comply with the requirements of the 2003 International Fire Code, including Appendices B through D, as adopted by the City of Mobile, and the 2003 International Existing Building Code, as appropriate.

Traffic Engineering Comments: Driveway number, size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards. Final approval for all driveways, proposed and existing will be given upon submittal of final plans.

Urban Forestry Comments: Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

MAWSS Comments: None Received.

The plat illustrates the proposed 16.1 acre \pm , 2 lot subdivision which is located at 6151 Marina Drive South (East side of Marina Drive South at the mouth of Dog River). The applicant states that the subdivision is served by city water and sanitary facilities .

The purpose of this application is to create 2 lots from an existing metes and bounds parcel.

The site fronts onto Marina Drive South, which serves as a service road for Dauphin Island Parkway and was part of the "new" bridge project. The right-of-way width at this location is in compliance with requirements.

Access management is a concern for multiple reasons. First, the plat appears to propose access to Bay Road North for Lot 2. As the overall site was annexed as an apartment complex, and the development on Lot 2 is still multi-family, access to the substandard, minor residential street would not be appropriate. In fact, based on aerial photographs, the existing development appears to have never had access to Bay Road.

The site is subject to storm related surges and was heavily damaged in Hurricane Katrina. Based upon that damage, a number of the buildings were demolished. However, a number of multi-family buildings remain. While the request before the Commission is simply a subdivision, the Commission should consider the appropriateness of the multi-family access to Bay Road North.

As indicated above, proposed Lot 2 is currently developed with multiple buildings, therefore a Planned Unit Development Application should accompany this request. Additionally, as the existing buildings are not located on the plat, it is not possible to determine if there are any setback issues as relate to the proposed lot line.

Given its location on Mobile Bay, the site would be considered environmentally sensitive; therefore, the approval of all applicable federal, state and local agencies would be required prior to the issuance of any permits or land disturbance activities. The minimum finished floor elevation should be indicated for each lot, and the flood zone(s) should be indicated on the plat, if approved.

The site may contain federally listed endangered or threatened species, such as the gopher tortoise, or protected non-game species. Development of the site must be undertaken in compliance with all local, state and Federal regulations regarding endangered, threatened or otherwise protected flora and fauna.

Finally, if approved the plat should be revised to label each lot with its size in square feet.

Based on the preceding, the plat is recommended for holdover until June 19 to allow the applicant to address the following:

1. revision of the plat to provide Lot 2 access to Marina Drive South;
2. provision of documentation that the proposed lot line will not create a setback violation;
3. submission of an accompanying PUD application

Revised for the June 19, 2008 meeting

One of the reasons recommended for holdover was to allow the plat to be revised to provide both lots access to Marina Drive S. The applicant submitted a statement indicating that a barricade would be erected between Lots 1 and 2 to prohibit vehicular access between the lots; and that Lot 1 is to have access via Bay Road N and Lot 2 is to be accessed via Marina Drive S. While it appears that the lot numbers have changed, the issue of access remains a concern as stated in the original report – one of the lots is to be accessed from Bay Road N exclusively. In fact, the lot that is to be accessed via Bay Road N currently has several multi-family buildings. Bay Road N is a substandard gravel road, and is not adequate access for a multi-family development.

The original development, much of which was damaged/destroyed by Hurricane Katrina, was developed as an apartment complex and was later annexed into the City of Mobile. As such, the site remains zoned R-1, Single-Family Residential. Also, since the site was developed prior to being annexed into the city, there was no PUD approval. While the plan indicates a driveway to Bay Road N, it appears that the gravel from Bay Road N is not a formal driveway, but simply extends gravel within the site to the existing parking lot. In fact, this access to Bay Road appears to be recent in that it did not exist in 2002. City of Mobile aerials from that year clearly illustrate

a dumpster where this “drive” is now located. Further, while not as clear, the 1997 aerials appear to show the same dumpster location.

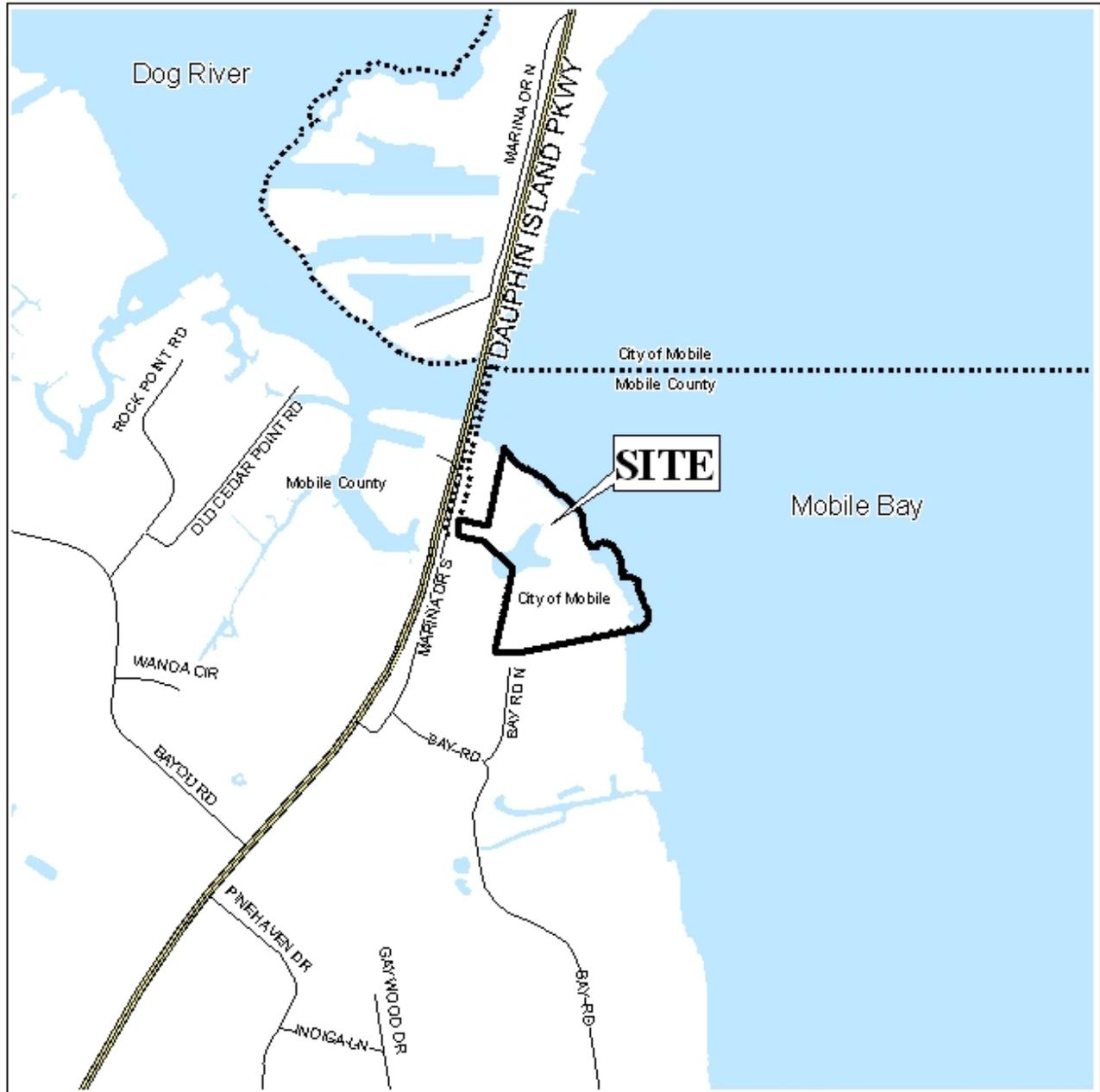
Another reason for Holdover was to allow the applicant to submit documentation that the proposed subdivision would not create a violation of the setback requirements of the Zoning Ordinance. No information was provided.

The third reason the application was heldover was to allow the applicant to submit an accompanying PUD application since the development had been significantly modified since its annexation, and would be further modified by the proposed subdivision. Again, no information/application was submitted.

Based on the preceding, it is recommended that this application be denied for the following reasons:

1. Bay Road N is substandard in both width and surface material, and thus does not provide adequate access;
2. The subdivision as proposed would substantially modify a nonconforming multi-building development without benefit of Planned Unit Development Approval, thereby creating a violation of the Zoning Ordinance.

LOCATOR MAP



APPLICATION NUMBER 2 DATE June 19, 2008

APPLICANT Grandview Apartments Subdivision

REQUEST Subdivision



NTS

GRANDVIEW APARTMENTS SUBDIVISION



APPLICATION NUMBER 2 DATE June 19, 2008



NTS