

COTTAGE OAKS SUBDIVISION

Engineering Comments: Must comply with all storm water and flood control ordinances. Any work performed in the right of way will require a right of way permit.

Traffic Engineering Comments: Driveway number, sizes, location and design to be approved by Traffic Engineering and conform to AASHTO standards.

Urban Forestry Comments: Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

Fire Department Comments: No comments.

The plat illustrates the proposed 0.7 acre \pm , 3 lot subdivision which is located on the Southeast corner of Brookfield Drive North and Brookfield Drive West, and is in Council District 4. The applicant states that the subdivision is served by both public water and sanitary sewer.

The purpose of this application is to subdivide one lot into three lots. This site was approved for a two-lot subdivision by the Planning Commission at its November 17, 2005 meeting, although the original request was for a three-lot subdivision. The approval from 2005 has since expired.

The site fronts onto Brookfield Drive North and Brookfield Drive West, both minor streets with adequate right-of-way. While each street is a minor street, access management is a concern. Proposed Lot 1 should be limited to one curb-cut either onto Brookfield Drive North or Brookfield Drive West, but not both, while Lots 2 and 3 should be limited to one curb-cut each, with the size, design and location of all curb-cuts to be approved by Traffic Engineering and comply with AASHTO standards.

The lots meet the minimum size requirements of the Subdivision Regulations and are labeled with their size in square feet. The size of Lot 1, however, is incorrect and should be corrected for the final plat, if approved.

The plat depicts a 25-foot minimum building setback line from Brookfield Drive North, but only a 20-foot setback line from Brookfield Drive West. The Subdivision Regulations require that the setback from all streets be 25-feet: a 20-foot sideyard setback is allowed on corner lots by the Zoning Ordinance in Single-Family Residential districts. If the applicant wishes to request a 20-foot setback, an application for Planned Unit Development should be provided and the Subdivision application should be heldover to coincide with the PUD application.

Based on the preceding, the plat is recommended for Tentative Approval, subject to the following conditions:

- 1) placement of a note on the final plat stating that Lot 1 is limited to one curb-cut either onto Brookfield Drive North and Brookfield Drive West, and that Lots 2 and 3 are limited to one-curb cut each, with the size, design and location of all curb-cuts to be approved by Traffic Engineering and conform to AASHTO standards;
- 2) correction of the label regarding the size of Lot 1 in square feet;
- 3) revision of the plat to show a 25-foot minimum building setback from all street frontages; and
- 4) full compliance with all municipal codes and ordinances.

Revised for the May 17, 2007 meeting:

As a result of commentary at the April 19th meeting of the Planning Commission, the applicant was directed to revise the plat to reflect a two lot subdivision instead of the originally proposed three lot subdivision.

The revised plat depicts a two lot subdivision with a 25-foot minimum building setback line along Brookfield Drive North, and a 20-foot minimum building setback line along Brookfield Drive West.

Proposed Lot 1 should be limited to one curb-cut either onto Brookfield Drive North or Brookfield Drive West, but not both, while Lot 2 should be limited to one curb-cut, with the size, design and location of all curb-cuts to be approved by Traffic Engineering and comply with AASHTO standards.

The Subdivision Regulations require that the setback from all streets be 25-feet: a 20-foot side yard setback is allowed on corner lots by the Zoning Ordinance in Single-Family Residential districts. The Planning Commission may waive the 25-foot setback, required by Section V.D.9. of the Subdivision Regulations, however, any residence built on Lot 1 would be required to face Brookfield Drive North.

Based upon the revised plat, the application is recommended for Tentative Approval, subject to the following conditions:

- 1) placement of a note on the final plat stating that Lot 1 is limited to one curb-cut either onto Brookfield Drive North and Brookfield Drive West, and that Lot 2 is limited to one-curb cut, with the size, design and location of all curb-cuts to be approved by Traffic Engineering and conform to AASHTO standards;*
- 2) revision of the plat to show a 25-foot minimum building setback from all street frontages; and*
- 3) full compliance with all municipal codes and ordinances.*

LOCATOR MAP



APPLICATION NUMBER 1 DATE May 17, 2007

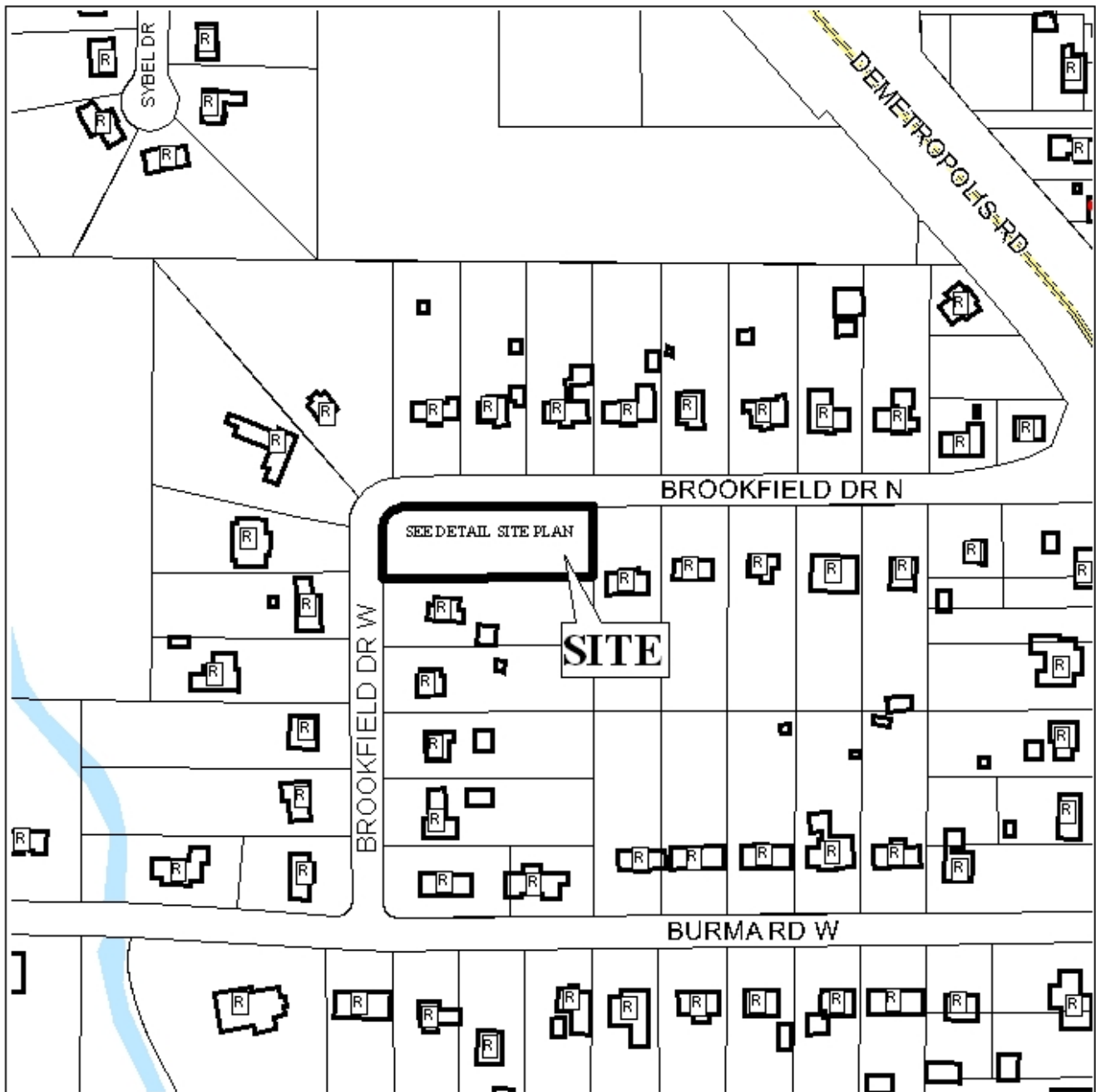
APPLICANT Cottage Oaks Subdivision

REQUEST Subdivision



NTS

COTTAGE OAKS SUBDIVISION



APPLICATION NUMBER 1 DATE May 17, 2007

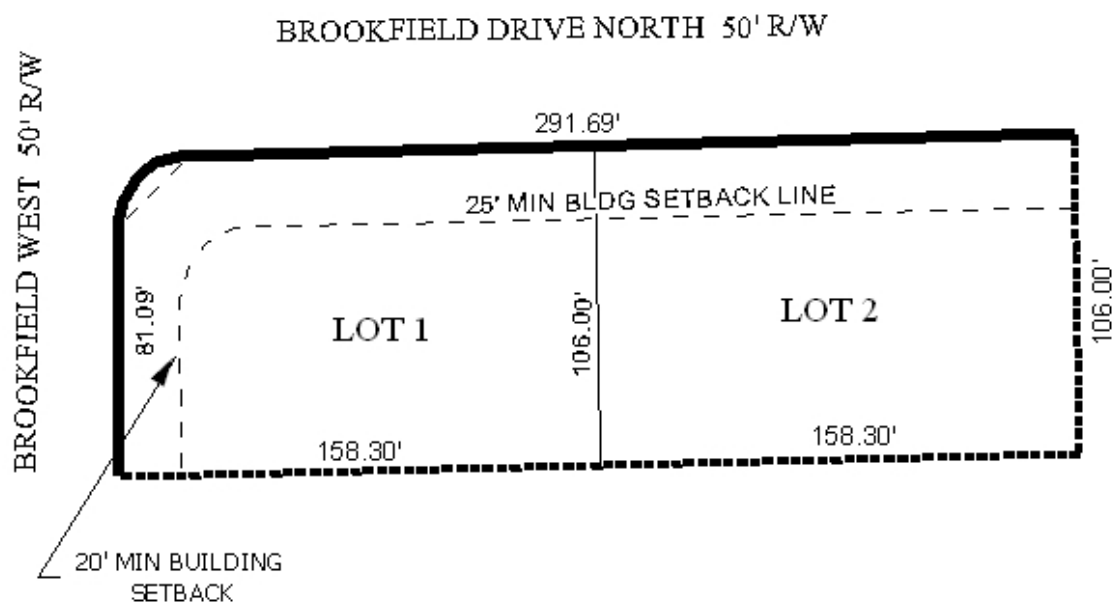
LEGEND

R-1	R-2	R-3	R-A	R-B	H-B	B-1	LB-2	B-2	B-3	B-4	B-5	I-1	I-2



NTS

DETAIL SITE PLAN



APPLICATION NUMBER 1 DATE May 17, 2007

APPLICANT Cottage Oaks Subdivision

REQUEST Subdivision



NTS