

**PLANNING APPROVAL &
PLANNED UNIT DEVELOPMENT
STAFF REPORT**

Date: April 6, 2006

NAME Alter Scrap Processing

LOCATION 101 Hardwood Lane
(East side of Hardwood Lane [private street], 540'± East of
North Craft Highway)

**CITY COUNCIL
DISTRICT** District 2

PRESENT ZONING I-2, Heavy Industry

AREA OF PROPERTY 47.9± Acres

CONTEMPLATED USE Planning Approval to allow a scrap metal processing plant
with automobile shredding in an I-2, Heavy Industry
district;
Planned Unit Development Approval to allow multiple
buildings on a single building site.

**TIME SCHEDULE
FOR DEVELOPMENT** Immediate

**ENGINEERING
COMMENTS**

The majority of the site is located in a Special Flood Hazard area and/or wetlands. The minimum finished floor elevation for structures is 12 feet above MSL. In addition, the majority of the site is wetlands. All construction and scrap metal processing activities should comply with FEMA and wetlands regulations. Development must comply with all stormwater and flood control ordinances. Any work performed in the right of way will require a right of way permit.

**TRAFFIC ENGINEERING
COMMENTS**

Driveway number, size, location, and design to be approved by Traffic Engineering and conform to AASHTO standards.

**URBAN FORESTRY
COMMENTS**

Property to be developed in compliance with state and local laws that pertain to tree preservation and protection on both city and private properties (State Act 61-929 and City Code Chapters 57 and 64).

FIRE DEPARTMENT
COMMENTS

All commercial projects shall comply with the 2003 International Fire Code and Appendices B through G, as adopted by the City of Mobile on July 6, 2004. All One- or Two-Family residential developments shall comply with Appendices B, C, and D of the 2003 International Fire Code, as adopted by the City of Mobile on July 6, 2004.

REMARKS

The applicant is seeking Planning Approval to allow a scrap metal processing plant, specifically to include the shredding of automobiles in an I-2, Heavy Industry District, and Planned Unit Development Approval to allow multiple buildings on a single building site. Planning Approval is required to allow the shredding of automobiles.

The review required for Planning Approval examines the applicant's location and site plan with regard to transportation, parking and access, public utilities and facilities, traffic congestion and hazard, and to determine if the proposal is in harmony with the orderly and appropriate development of the district.

Planned Unit Development (PUD) review examines the site with regard to its location to ensure that it is generally compatible with neighboring uses; that adequate access is provided without generating excess traffic along minor residential streets in residential districts outside the PUD; and that natural features of the site are taken into consideration. PUD review also examines the design of the development to provide for adequate circulation within the development; to ensure adequate access for emergency vehicles; and to consider and provide for protection from adverse effects of adjacent properties as well as provide protection of adjacent properties from adverse effects from the PUD.

It is very important to note that both the Planning Approval and the Planned Unit Development reviews are site plan specific; therefore any future changes (parking, structure expansion, etc.) must be resubmitted for Planning Approval and Planned Unit Development review, and be approved through the planning process.

As the reviews for Planning Approval and Planned Unit Development approval are similar, the analysis/remarks for both applications will be consolidated.

Alter Trading Corporation ("Alter") proposes to establish a full service, state-of-the-art scrap metal recycling facility within the City of Mobile. These applications if approved, would allow "Alter" to use this facility to buy, sell, process, and transport ferrous and non-ferrous metal products; and would require the site be brought into conformance with all requirements of the Zoning Ordinance.

The development encompasses existing paved, unpaved areas and several existing buildings with the capability of future expansion to include office buildings, maintenance buildings, and the ability to provide the delivery of material via truck, water and rail.

The 47.9± acre site is bounded to the north by a shipyard and marine services company, and to the south by a major steel company and property owned by the City of Chickasaw. West of the site is the access point from Hardwood Lane, while East of the site is a tributary of Mobile River.

Access to the site is via a paved driveway to Hardwood Lane, a private street from U. S Highway 43 to the site. Natural vegetative buffers occur on all sides of the property.

The location of the site on the Mobile River and along a railroad is an appropriate siting for an industrial use such as a recycling center; and the prior use of the site as a wood and timber products facility (sawmill and storage of raw wood products) would appear to be a compatible industrial use.

The automobile shredding process begins elsewhere, where the automobiles are dismantled to recover reusable parts. The stripped-down vehicles are shipped to the automobile shredding site either in bulk form or crushed where large shredders are used to turn the automobile into smaller pieces, which are then sold back to the iron, steel and nonferrous metal industries. After the shredders have recovered the metals from a vehicle, the residue (25-percent) is landfilled.

The applicant states the facility will employ approximately 70-75 employees. The shredder will only be operated from 6:00AM-6:00PM, Monday through Saturday. Maintenance will take place during the evenings, but the plant will not be in operation after dark.

The site has approximately 2,000 feet of water frontage on Mobile River and has an existing rail spur on site. The site plan illustrates future expansion, which includes the construction of a bulkhead along the canal from the Mobile River and a future rail spur to allow transportation of products by barge and rail.

The parking requirement for a development of this type requires one space per 300 square feet of gross floor area for office use and one space per three employees for manufacturing or warehouse use. Using the employee-parking requirement of one parking space per three employees, the site would be required to provide 27 spaces (2 spaces for office use and 25 spaces for manufacturing/warehouse). The site provides 25 unpaved parking spaces; it should be noted that unpaved parking is allowed in I-2 districts. As proposed the site plan does not illustrate the required number of spaces; however, the site has ample space to meet this requirement. Moreover, since future expansion is illustrated on the site plan, conformance with the parking requirements would be required at the time of expansion.

The site has naturally occurring vegetative buffers on the North, West and South sides. While tree and landscaping requirements might normally be placed on new non-residential construction or a substantial expansion undergoing Planning Approval and PUD review, since this application could be considered new development, a waiver of the tree and landscaping would not be appropriate. Therefore, full compliance with the tree and landscaping requirements of the Zoning Ordinance should be required as part of the Planning Approval and PUD review. Additionally, compliance with the buffering requirements of the Zoning Ordinance should be required from adjacent residential uses, if existing vegetation is not sufficient.

Setbacks for the existing and proposed structures comply with the requirements of the Zoning Ordinance. All future construction must comply with the setback requirements outlined within the Zoning Ordinance, unless amendments are made to the PUD to allow deviation from the

requirements. Otherwise, the presence of multiple buildings on the site, as proposed, is not anticipated to negatively impact the area.

Founded in 1898, Alter Trading Corporation, a privately owned company is one of the largest scrap metal processors in the country with offices and processing plants across the central United States. The applicant submitted a letter from a nationwide environmental service company (CPI Environmental Services, Inc.) concerning the noise issue. The company states that after studying various issues with the scrap metal recycling industry for more than 12 years, including automobile shredders a common issue was noise and its impact on surrounding properties. After conducting several noise surveys around automobile shredding facilities industry wide, the findings were, in general, the older the shredding plant the more noise. In addition, the source of power to the shredding plant, e.g., diesel engine-driven versus electrical motor, will have an influence on the noise levels.

CPI Environmental Services, Inc. stated that during a recent noise survey around an existing shredding facility they measured noise levels throughout the operational period during nighttime hours. Disregarding the background wind noise, the company states the observed sound levels due to equipment were less than 65 dB at a distance of one-quarter mile. By comparison, a business office background is generally at 70 dB and conversational speech is at 60 dB. Additionally, information was provided that at two other Alter automobile shredding facilities in Iowa the decibel level at the site did not exceed 77 dB; however, this level was measured at an older model shredder.

The majority of the site is located in an area subject to storm-surge related flooding, as well as within the FEMA identified 100 and 500-year floodplains for Mobile. Furthermore, the proximity to tributaries of Mobile Bay and illustration that the site contains wetlands indicate that the area may be environmentally sensitive; therefore, the approval of all applicable federal, state and local agencies would be required prior to the issuance of any permits.

The site plan does not indicate the location of a stormwater detention basin, which may be required for the site due to new construction. A stormwater detention basin, sized to meet the requirements of the City's Storm Water Drainage Ordinance, must be indicated on the site plan if it is required.

Finally, there is no indication on the site plan as to the proposed location of a dumpster or any other waste storage facility to deal with the residue by-product of the automobile shredding process. If the applicant utilizes a dumpster or some other type of storage facility, the location of the storage area for the by-product must be indicated on the site plan, and the location and required screening must comply Section IV.D.9. Of the Zoning Ordinance, as well as with all other applicable regulations concerning the disposal of the unrecoverable waste.

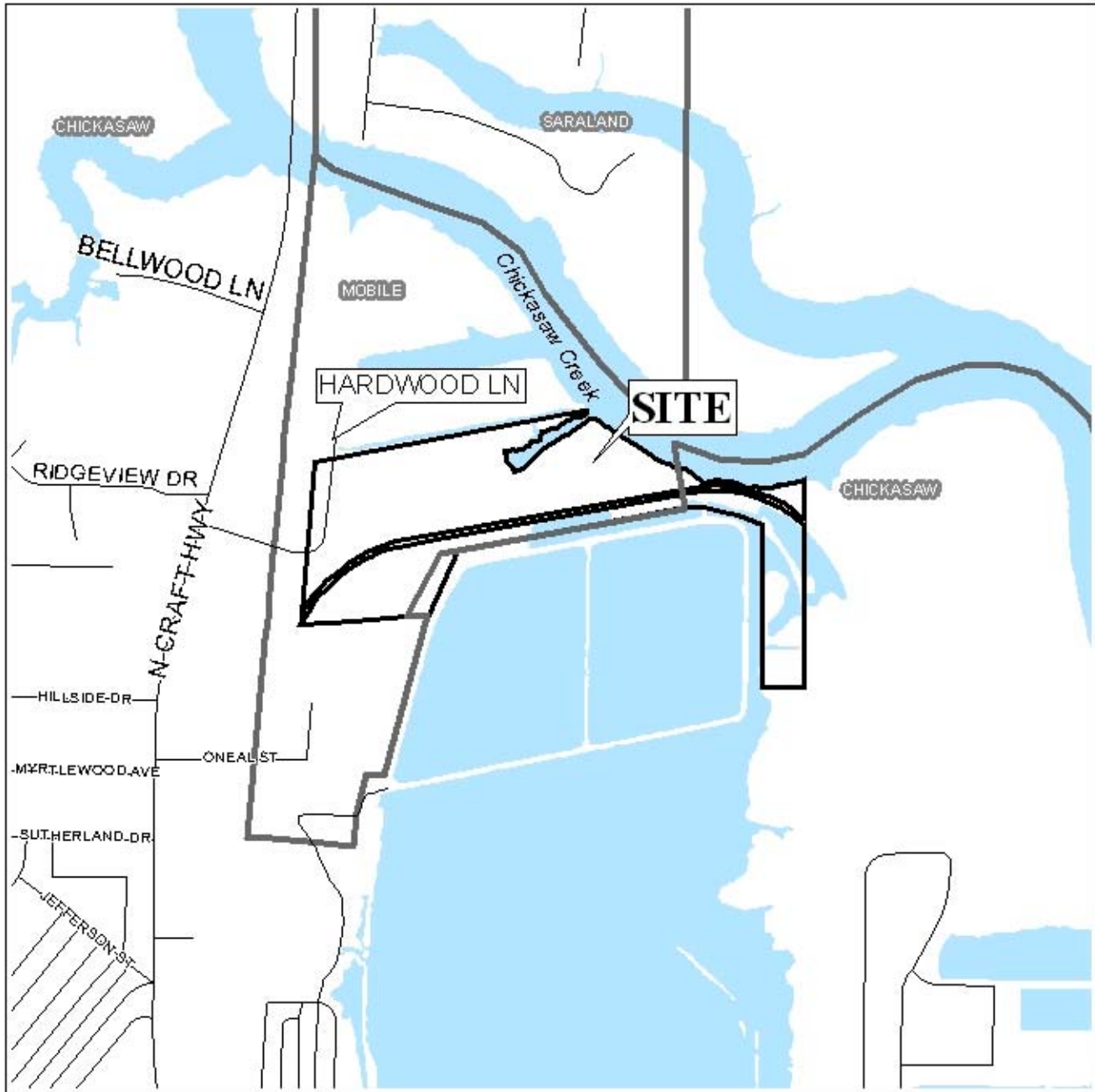
RECOMMENDATION

Planning Approval: Based upon the preceding, this request is recommended for Approval, subject to the following conditions: 1) depiction of a stormwater detention basin, if required; 2) illustration of the location of any dumpster or waste storage area on the site plan; 3) compliance with the buffering requirements of the Zoning Ordinance from adjacent residential uses, with in-fill plantings if the existing vegetation is not

sufficient; 4) approval of all applicable federal, state and local agencies prior to the issuance of any permits, and; 5) submission of a disposal plan for the unrecoverable waste associated with the automobile shredding process.

Planned Unit Development: Based upon the preceding, this request is recommended for Approval, subject to the following conditions: 1) depiction of a stormwater detention basin, if required; 2) illustration of the location of any dumpster or waste storage area on the site plan; 3) compliance with the buffering requirements of the Zoning Ordinance from adjacent residential uses, with in-fill plantings if the existing vegetation is not sufficient; 4) approval of all applicable federal, state and local agencies prior to the issuance of any permits, and; 5) submission of a disposal plan for the unrecoverable waste associated with the automobile shredding process

LOCATOR MAP



APPLICATION NUMBER 1 & 2 DATE April 6, 2006

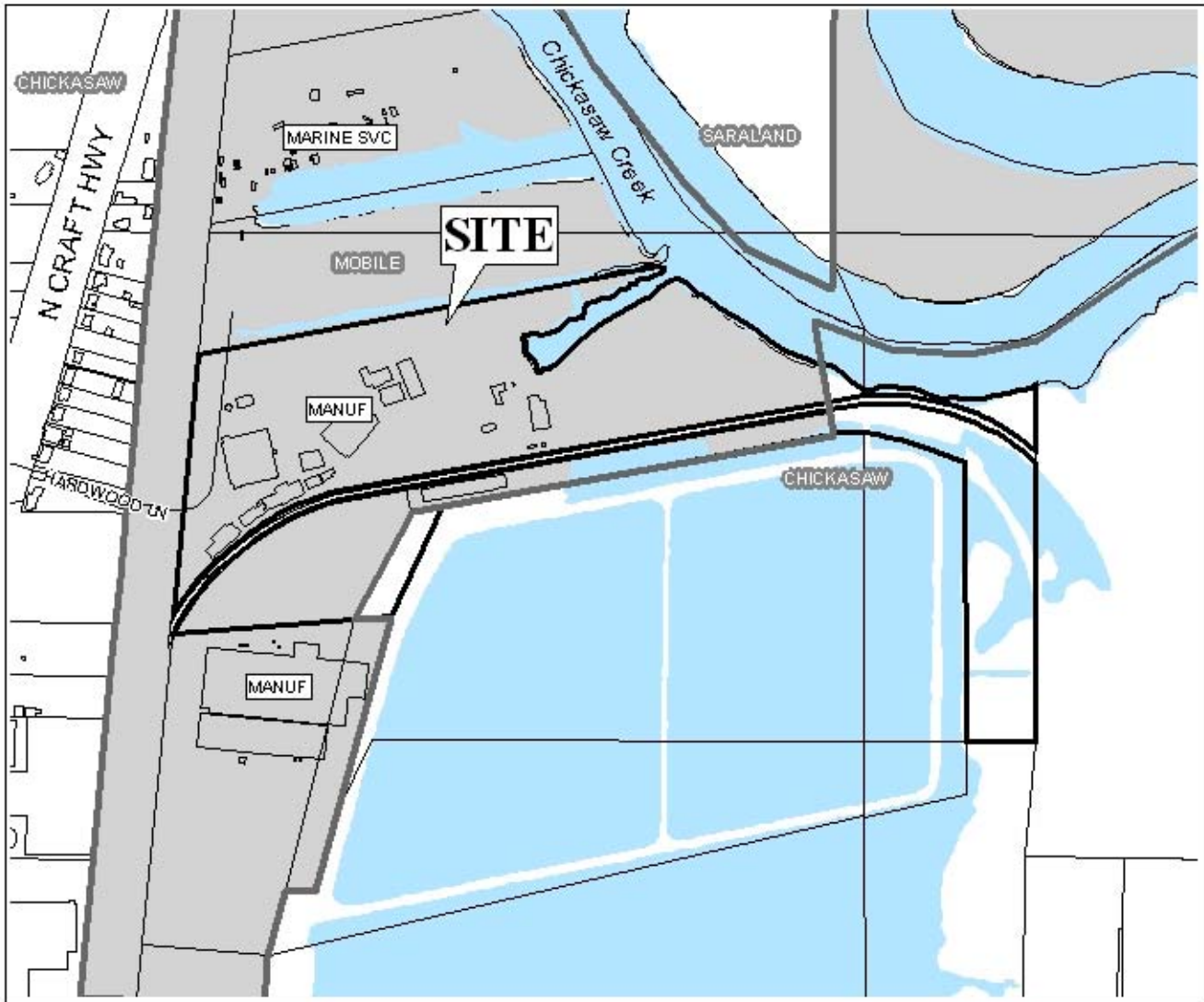
APPLICANT Alter Scrap Processing

REQUEST Planning Approval, Planned Unit Development



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PLANNING COMMISSION VICINITY MAP - EXISTING ZONING



The site is surrounded by industrial land use

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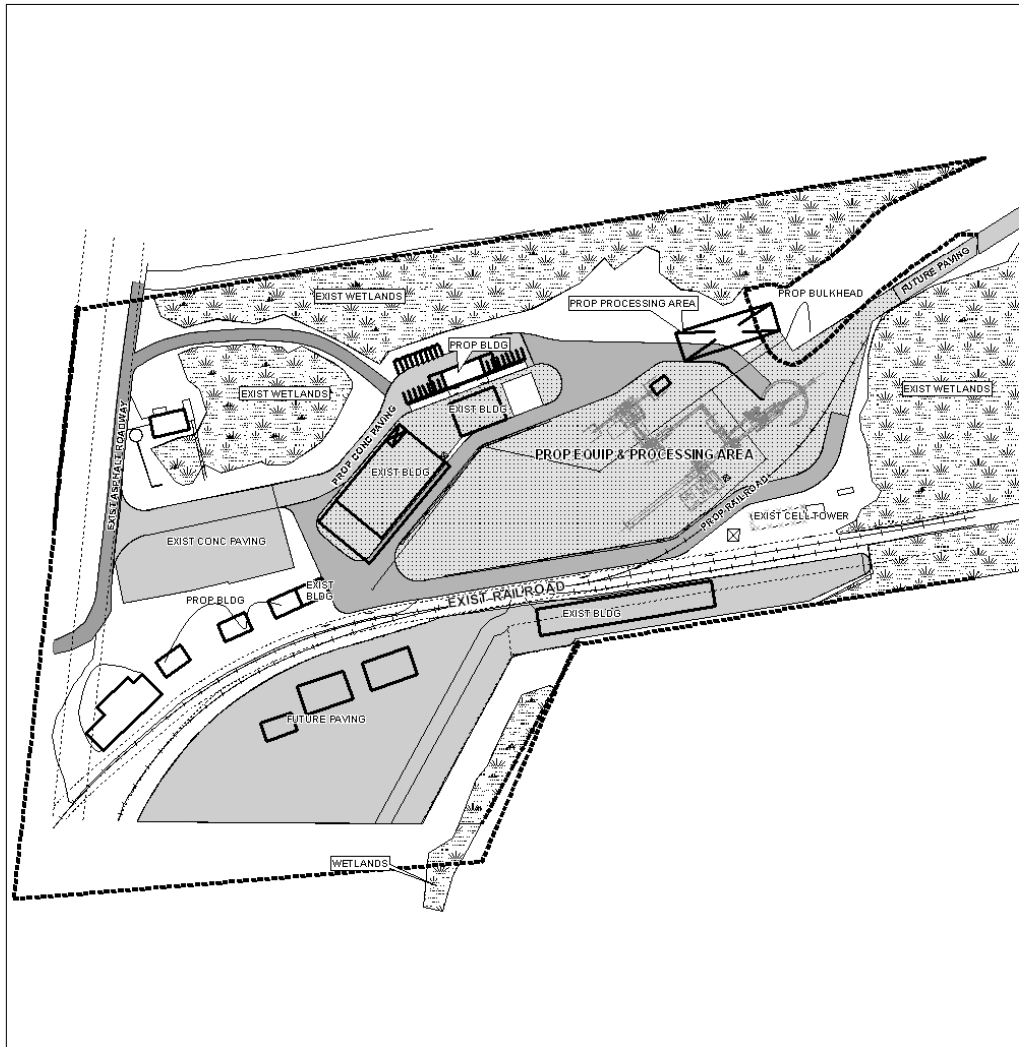
LEGEND

R-1	R-2	R-3	R-A	R-B	H-B	B-1	LB-2	B-2	B-3	B-4	B-5	I-1	I-2
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 NTS



SITE PLAN



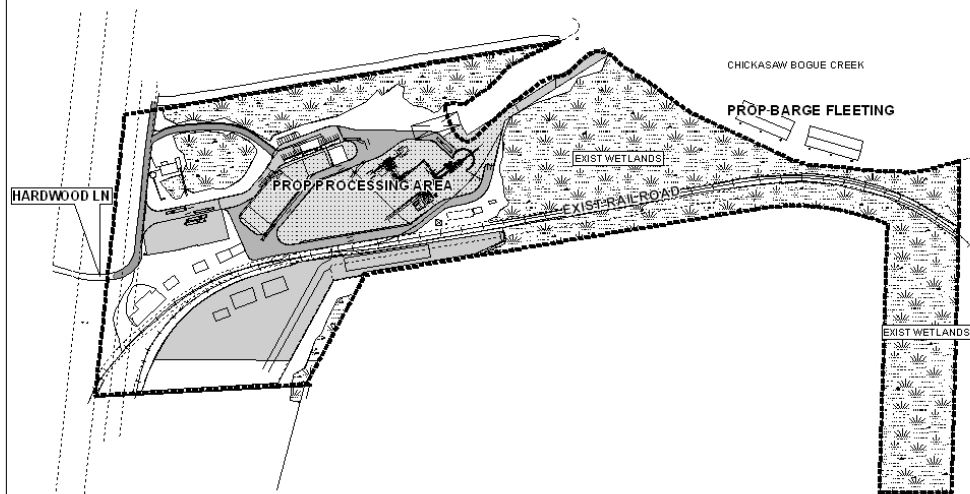
The site plan illustrates an enlarged view of the proposed development

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SITE PLAN

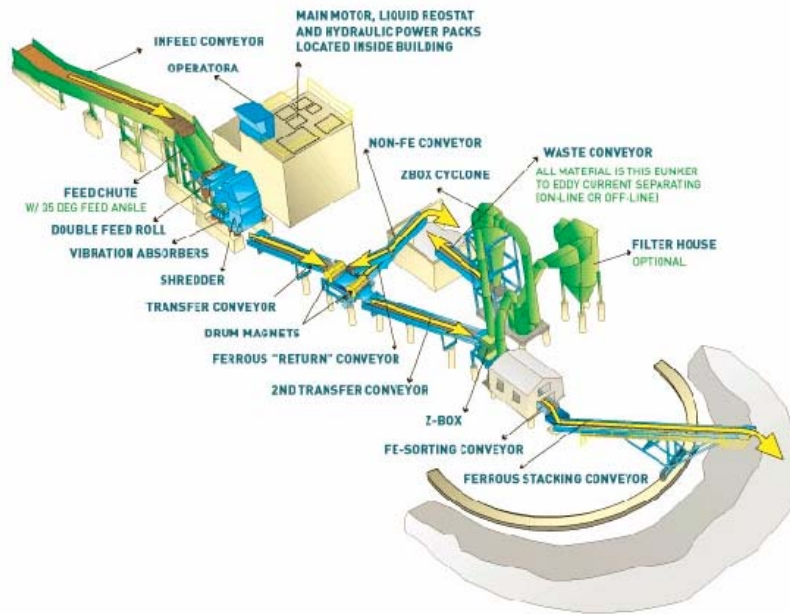
The site plan illustrates the proposed processing area and existing wetlands

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DETAIL SITE PLAN**TYPICAL SHREDDING FACILITY WITH "SMART" WATER INJECTION AT THE SHREDDER**

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